

Moors Field, Little Dunmow

Design & Access Statement

For Dandara Eastern Ltd

P23-0555_DE G001C | July 2023

Moors Field, LITTLE DUNMOW Design & Access Statement

Prepared by Pegasus Group on behalf of Dandara Eastern Ltd
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Contents

1	INTRODUCTION	08
2	PLANNING POLICY	14
3	CONTEXT	22
4	DESIGN DEVELOPMENT	36
5	DESIGN PROPOSALS	44
6	CONCLUSION	90

1

Introduction

1.1	PURPOSE OF STATEMENT	09
1.2	THE SITE	10
1.3	THE VISION	11

1 INTRODUCTION

This Design and Access Statement (DAS) has been prepared by Pegasus Design (part of Pegasus Group) on behalf of Dandara to accompany a reserved matters application for the residential development at land at Moors Field, Little Dunmow.

This statement has been prepared with reference to the Design Code dated November 2022. This statement explains how the reserved matters application adheres to the principles agreed within the Design Code.

This statement has been prepared in accordance with Article 9 of the Town and Country Planning (Development Management Procedure) (England) Order 2015, which requires certain applications to be accompanied by a Design and Access Statement.

The National Planning Practice Guidance (NPPG) states that a Design and Access Statement is a 'concise report that provides a framework for applications to explain how the proposed development is a suitable response to the Site and its setting and demonstrate that it can be adequately accessed by prospective users'

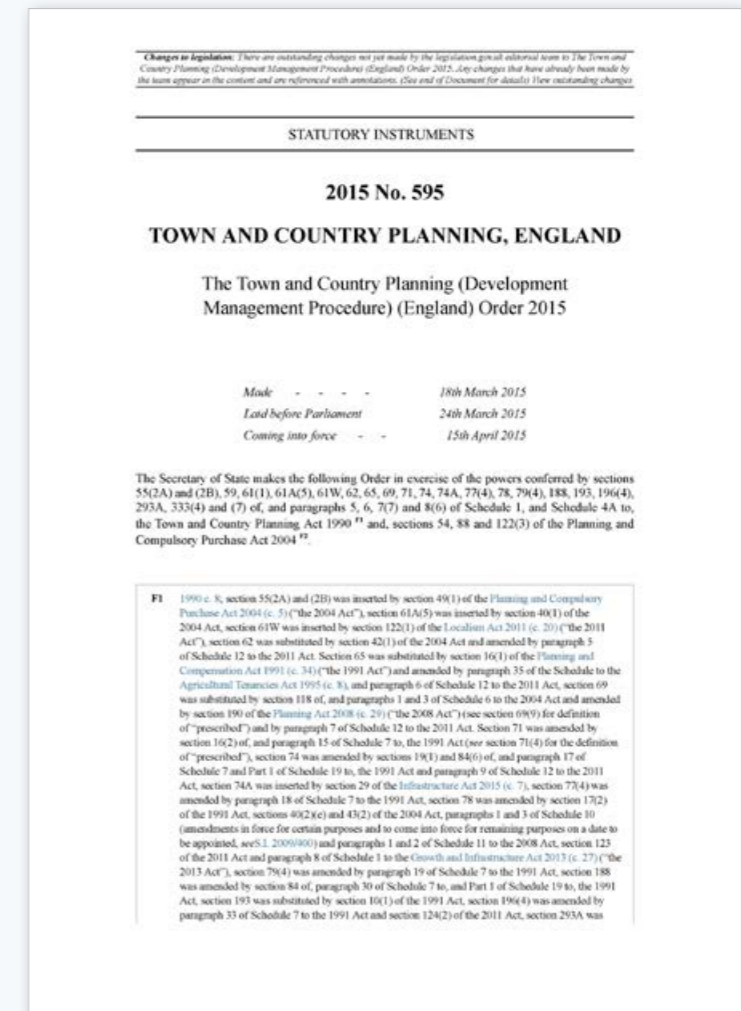
The DMPO also states the following requirements:

“(2) An application for planning permission to which this paragraph applies must, except where paragraph (4) applies, be accompanied by a statement (“a design and access statement”) about:

- a) The design principles and concepts that have been applied to the development; and
- b) How issues relating to access to the development have been dealt with.

(3) A design and access statement must:

- a) Explain the design principles and concepts that have been applied to the development;
- b) Demonstrate the steps taken to appraise the context of the development and how the design of the development takes that context into account;
- c) Explain the policy adopted as to access, and how policies relating to access in relevant local development documents have been taken into account;
- d) State what, if any, consultation has been undertaken on issues relating to access to the development and what account has been taken of the outcome of any such consultation; and
- e) Explain how specific issues which might affect access to the development have been addressed.”



TOWN AND COUNTRY PLANNING (DEVELOPMENT MANAGEMENT PROCEDURE) (ENGLAND) ORDER 2015

1.1 PURPOSE OF STATEMENT

The purpose of this Design and Access Statement is:

“...To explain how the proposed development is a suitable response to The Site and its setting, and demonstrate that it can be adequately accessed by prospective users.”
(para. 029, PPG, Reference ID: 14-029-20140306)

This document achieves this within the following sections:

SECTION 1

INTRODUCTION

Outlines the purpose of this document;

SECTION 2

PLANNING POLICY

Presentation of the key Planning Policy requirements, derived from a combination of Local Authority and National Government Policy;

SECTION 3

CONTEXT

Considers The Site and its surroundings in terms of the local physical, historical and social setting, as well as the technical and physical context;

SECTION 4

DESIGN DEVELOPMENT

Presentation of the design principles that have been derived from a combination of Government Policy and site assessment outlines key stakeholder engagement undertaken, as well as its key findings and design evolution;

SECTION 5

DESIGN PROPOSALS

Presentation of the key design proposals including the Uses, Built Form and Identity, Movement, Nature and Public Space, Homes and Buildings, Resources and Lifespan; and

SECTION 6

CONCLUSION

This Design and Access Statement has been written to respond to the Ministry of Housing, Communities and Local Government National Design Guide (NDG) ten characteristics of well-designed places.

THIS STATEMENT SHOULD BE READ IN CONJUNCTION WITH THE OUTLINE PLANNING APPLICATION AND ITS ACCOMPANYING SUPPORTING DOCUMENTS.



1.2 THE SITE

Moors Fields currently comprises one field in agricultural use and neighbouring woodland, covering an area of 14.06ha.

The site lies between the villages of Little Dunmow and Flitch Green. Access to the site is currently from three points along Station Road; in the north western corner, alongside Pound Hill Villas and alongside Harrisons Road.

The site's southern boundary is bordered by the Flitch Way, which is a multi-user path and national cycle route found within the Flitch Way Country Park. Pedestrian access is possible from the footpath which runs through the site although the registered route of the footpath is through the overgrown area and is currently inaccessible. A separate informal route has formed from the pedestrian access point along the western boundary towards the Flitch Way.

VISION

Our Vision for the development Moors Field is to create an outstanding place to live which accords with the aspirations of the Design Code. The development seeks to create a new place to live, study and play, taking inspiration from the surroundings as well as recognising new approaches to urban design and masterplanning. The vision will be achieved through:

- Meet Moors Fields local policy requirements and future development needs through the creation of a sustainable community of 160 new family and other homes, including 40% affordable housing provision, to meet future development need.
- Delivering extensive public open space covering over 60% of the site, formed of a countryside park and include community allotments and orchard space.
- Delivery of significant new tree planting both around the public open space and throughout the development, strengthening the recreational role of the Moors woodland.
- The provision of a circular walking and cycle trail connected to the existing Public Right of Way and Fitch Way National Cycle Route.

(Taken from pg. 4 of Design Code for Moors Feilds, dated November 2022)



2

Planning Policy

2.1	NATIONAL PLANNING POLICY FRAMEWORK	14
2.2	PLANNING PRACTICE GUIDANCE	15
2.3	NATIONAL DESIGN GUIDE	16
2.4	NATIONAL MODEL DESIGN CODE	17
2.5	DESIGN CODE	17
2.6	LOCAL PLANNING & DESIGN GUIDANCE	18
2.7	ADOPTED SUPPLEMENTARY PLANNING DOCUMENTS	18
2.8	PLANNING HISTORY	19

2 PLANNING POLICY

The development proposals will be formulated with due regard to the policies that make up the statutory Local Development Plan and Supplementary Planning Guidance, together with Government guidance

contained within the National Planning Policy Framework (July 2021), National Design Guide (published in 2019 and updated in January 2021) and the National Model Design Code (January 2021).

2.1 NATIONAL PLANNING POLICY FRAMEWORK

Government guidance in the form of the National Planning Policy Framework (NPPF) sets out the Government’s planning policies and how these should be applied. The NPPF states at paragraph 8 that the planning system has three interdependent key objectives, which when pursued in a mutually supportive way, can achieve sustainable development. The three key objectives are:

- A social objective;
- An economic objective; and
- An environmental objective.

There is a presumption in favour of sustainable development, as set out at paragraph 11. Section 9: Promoting Sustainable Transport (para. 104) of the NPPF points to the role that design has to play in ensuring that transport issues are considered at the earliest stages of development proposals, and the role that design can play to ensure that development maximizes opportunities for sustainable transport options.

“...patterns of movement, streets, parking and other transport considerations are integral to the design of schemes, and contribute to making high-quality places.” (para. 104(e) NPPF 2021)

The Government also continues to place a high emphasis on design and the NPPF expands on the principles of good design, to define what is expected of well-designed places. It also explains how policies and decision-making processes should support the inclusion of good design, providing detailed advice at Section 12: Achieving Well-Designed Places.

The contribution that good design makes to sustainable development is set out in paragraph 126, as follows:

“The creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities...” (para. 126, NPPF 2021)

Furthermore, a new test is being introduced in the latest edition of the NPPF, to ensure that developments are well-designed, placing an emphasis on fostering of “beautiful” places among the overarching objectives of the planning system. In paragraph 134, the NPPF states that:

“Development that is not well designed should be refused, especially where it fails to reflect local design policies and government guidance on design, taking into account any local design guidance and supplementary planning documents such as design guides and codes”. (para. 134, NPPF 2021)

The NPPF is also clear at paragraphs 127 and 128 that Development Plans should set out a clear design vision to provide certainty to applicants, and that design policies should be prepared in conjunction with local communities to reflect local aspirations.



SOCIAL

Support strong, vibrant and healthy communities.



ENVIRONMENTAL

Protecting and enhancing the natural, built and historic environment.



ECONOMIC

Build a strong, responsive and competitive economy.

Paragraph 130 of the NPPF states that with regard to design planning policy and decision making should ensure that developments;

- “a) Will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
- b) Are visually attractive as a result of good architecture, layout and appropriate and effective landscaping;
- c) Are sympathetic to the local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities);
- d) Establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit;
- e) Optimise the potential of The Site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks; and
- f) Create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users, and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.”

2.2 PLANNING PRACTICE GUIDANCE

The NPPF is accompanied by the on-line Government resource Planning Practice Guidance (PPG). The Design: Process and tools PPG provides guidance on the methods and processes available to both applicants and local authorities to ensure the delivery of well-designed and high-quality, long lasting places with considered design solutions, under the following headings:

- Planning for well-designed places;
- Making decisions about design;
- Tools for assessing and improving design quality; and
- Effective community engagement on design.

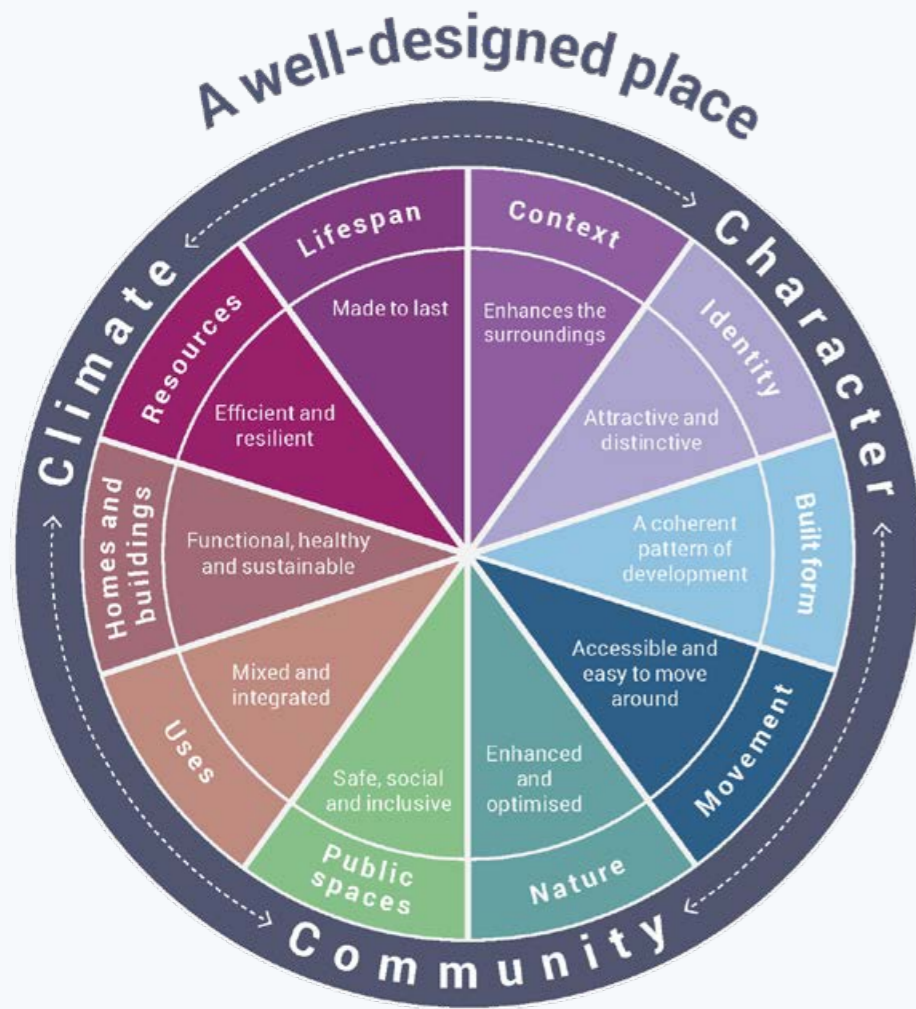
Paragraph 1 of the Design PPG reinforces the Government and NPPFs commitment to requiring the creation of well-designed places and the role that early engagement can play in this.

“Well-designed places can be achieved by taking a proactive and collaborative approach at all stages of the planning process, from policy and plan formulation through to the determination of planning applications and the post approval stage.” (para. 001, PPG, ID: 26-001-20191001, October 2019)


Ministry of Housing,
Communities &
Local Government

National Planning Policy Framework

NATIONAL PLANNING POLICY FRAMEWORK,
JULY 2021



THE TEN CHARACTERISTICS OF WELL-DESIGNED PLACES (NATIONAL DESIGN GUIDE, 2021)

2.3 NATIONAL DESIGN GUIDE

The National Design Guide (NDG) published by the Ministry of Housing, Communities and Local Government (MHCLG) in September 2019 and updated in January 2021 further reinforces the way in which the design process can be used to ensure the delivery of quality places:

“In a well-designed place, an integrated design process brings the ten characteristics together in a mutually supporting way. They interact to create an overall character of place.” (para. 13, NDG 2021)

The NDG outlines and illustrates the Government’s priorities for well-designed place in the form of ten characteristics, based on national planning policy, planning guidance and objectives for good design.

The ten characteristics contribute towards the cross-discipline themes for good design set out in the NPPF and fall under three broad aims:

- To create physical character;
- To help to nurture and sustain a sense of community; and
- To positively addresses environmental issues affecting climate.

Whilst the NPPF, PPG and NDG are the primary points of reference, there are other well-regarded design guidance documents that are still relevant to creating good design including:

- Manual for Streets 1 & 2 (Department of Transport/ Department for Communities and Local Government, 2007/2010); and
- Building for a Healthy Life (Homes England, June 2020) is the latest edition, and new name for Building for Life 12 (BfL12) written in partnership with NHS England, NHS Improvement and MHCLG.

2.4 NATIONAL MODEL DESIGN CODE

The National Model Design Code (NMDC) was published in January 2021 by the Ministry of Housing, Communities and Local Government. The purpose of this document is to:

“... provide detailed guidance on the production of design codes, guides and policies to promote successful design. It expands on the ten characteristics of good design set out in the National Design Guide, which reflects the government’s priorities and provides a common overarching framework for design.” (para. 1, National Design Code 2021)

The NMDC document draws upon the NPPF’s commitment to ensure that local planning authorities are utilising visual tools, such as design codes and guides, to inform development proposals, which will consequently provide a framework for creating high-quality places, with a consistent and high-quality standard of design.



NATIONAL MODEL DESIGN CODE, JANUARY 2021

2.5 DESIGN CODE

The Moors Fields Design Code (dated November 2022) sets out detailed principles for the reserved matters application to follow for the Moors Field site. The document aims to create a successful, landscape led design, which focuses on the Building for a Healthy Life criteria and responds to climate change.

The design code states the following:

- The outline approval plan shows that the main land use on the site will be residential development of up to 160 dwellings, with associated public open space, surface water attenuation and structural landscaping. The development parcels illustrated opposite include roads, footpaths, private gardens and parking areas.
- The site will deliver around 8.4ha of open space, with the main open space found to the north, west and south of the site in the form of a countryside park. Green links are created within the development area to provide a landscaped setting to the proposed new homes.

- The density across the site should be varied with lower density along the interfaces with the northern open space, medium density within the centre of the site and higher density along the southern edge of the site. The average density based on 160 dwellings within 5.54ha of development area is around 30dph.
- The road layout within the scheme will ensure suitable vehicle access to all properties, and include some shared space streets to minimise vehicle speeds.
- Section 4 of this D&A Statement demonstrates how the proposed development is in full accordance with the vision and Development Framework set out within the Design Code document.

2.6 LOCAL PLANNING & DESIGN GUIDANCE

The Uttersfield Local Plan was adopted in January 2005 and set out the strategy for development for the plan period from 2000 to 2011. As such, the Local Plan is now time expired and out of date. The weight to be afforded to the Local Plan should be considered in this context.

The NPPF states at paragraph 219 that existing policies should not be considered out-of-date simply because they were adopted or made prior to the publication of this Framework. Due weight should be given to them, according to their degree of consistency with this Framework. As such, the Local Plan policies are considered in this context, with the following policies broadly relevant to this submission:

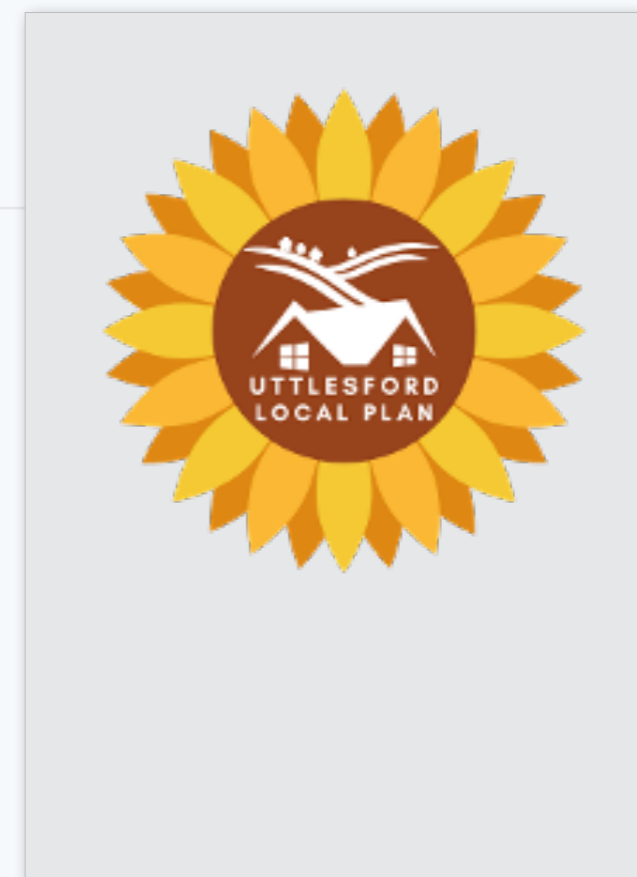
- Policy S7 – The Countryside
- Policy GEN1 – Access
- Policy GEN2 – Design
- Policy GEN6 –Infrastructure Provision to Support Development
- Policy GEN7 – Nature Conservation
- Policy GEN8 – Vehicle Parking Standards
- Policy ENV3- Open Spaces and Tree
- Policy ENV8 – Other Landscape Elements of Importance for Nature Conservation
- Policy H9- Affordable Housing
- Policy H10 – Housing Mix

2.7 ADOPTED SUPPLEMENTARY PLANNING DOCUMENTS

- Urban Place Supplement to the Essex Design Guide

Other Planning Guidance:

- Essex Design Guide
- Highways and Parking Standards
- Building for a Healthy Life



UTTLESFORD NEW LOCAL PLAN 2023



2.8 PLANNING HISTORY

As noted in Section 1, outline approval for the site was granted in March 2023 (application reference UTT/21/3596/OP) for 160 dwellings, alongside a country park office hub and other associated works. This DAS supports the Reserved Matters submission associated with this outline approval.

The immediate surrounding area has been the subject of a number of planning applications for housing development. A summary of these applications is provided below:

UTT/22/3470/OP Hybrid Planning Application

Outline application for up to 72 dwellings (Use Class C3) together with a building for use falling within Use Class E (a) or (b) or (c ii) or (c iii) or (d) or (e) or (f) or (g i) with all matters reserved, except access and structural landscaping. Full application for access and structural landscaping. Not yet determined.

UTT/21/3182/FUL

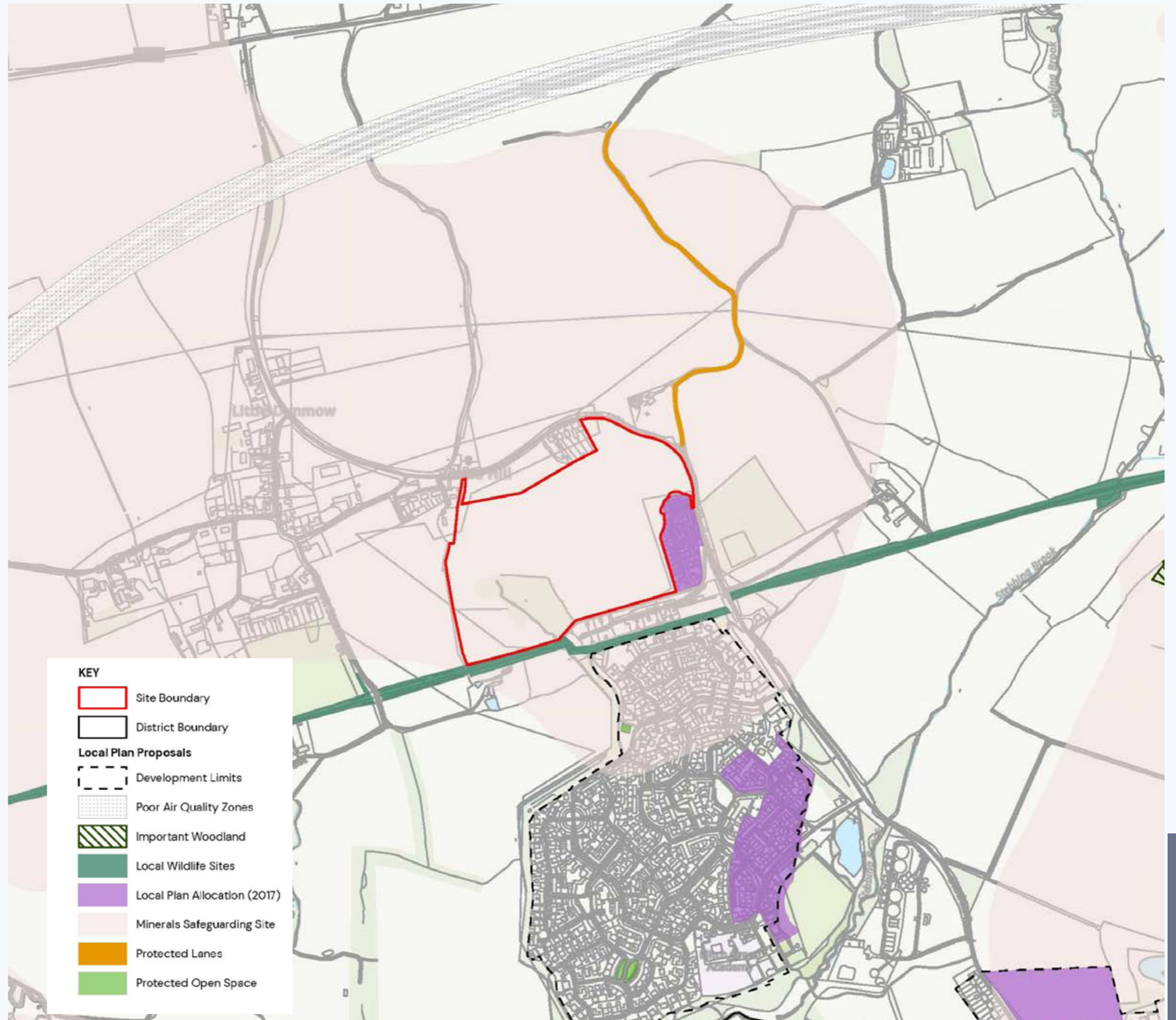
Proposed erection of 9 no. detached dwellings, provision of new access and associated landscaping and parking. Approved 22nd April 2022.

UTT/19/1789/FUL

Residential development comprising 14 dwellings (use class C3), vehicular access, public open space, sustainable drainage systems and all other associated hard/soft landscaping and infrastructure. Approved 21st May 2021.

UTT/18/2600/FUL

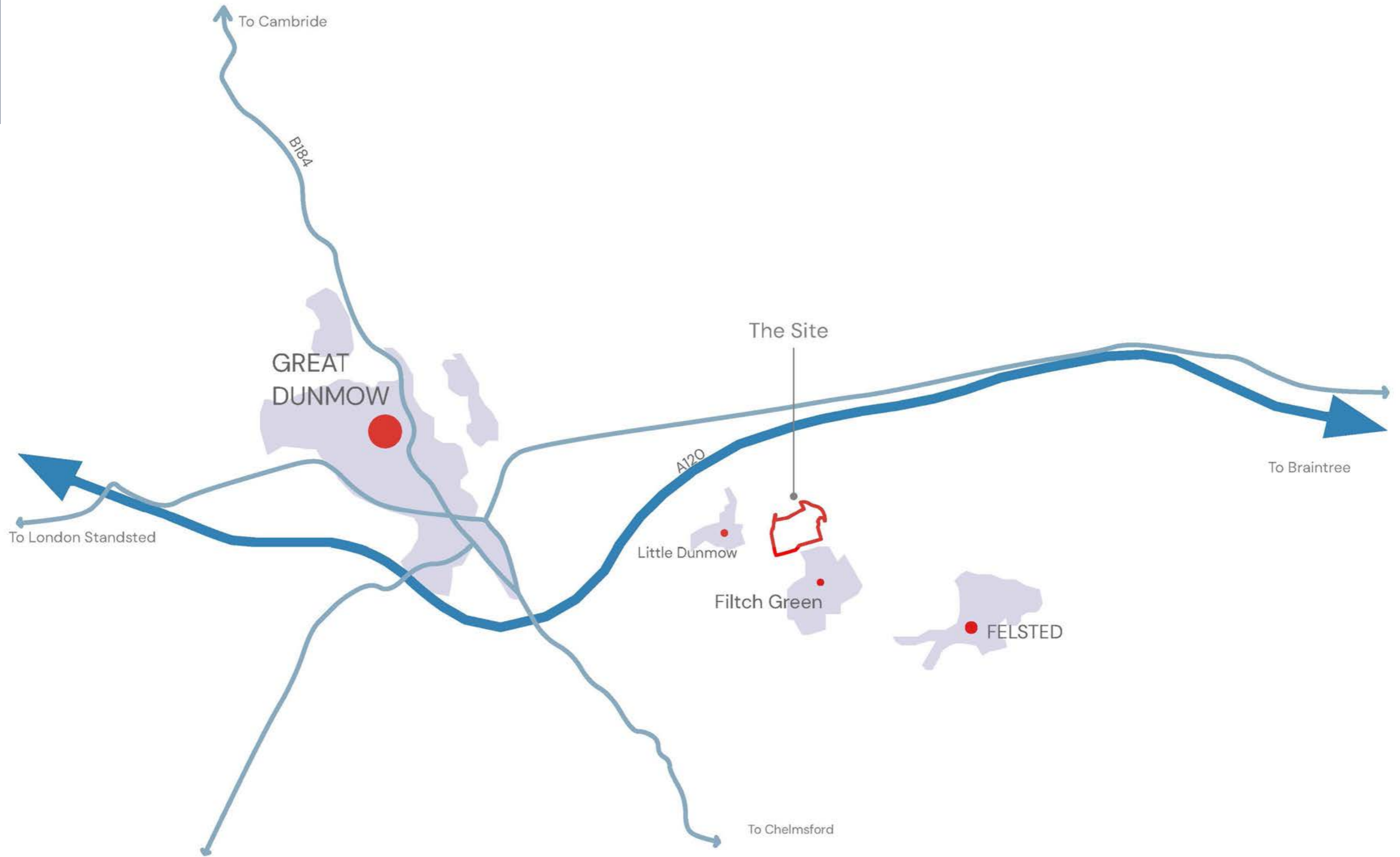
Proposed demolition of the existing 12 bungalows on the site, and replacement with 16 new fit for purpose dwellings (8x 1B2P Flats and 8x 2B4P). Approved 15th April 2019.



3

Context

3.1	WIDER CONTEXT	23
3.2	CONNECTIVITY	25
3.3	LOCAL FACILITIES	27
3.4	LOCAL CHARACTER	28
3.5	ARBORICULTURE	30
3.6	DRAINAGE	31
3.7	OVERVIEW OF THE SITES CONTEXT	32



3 CONTEXT

3.1 WIDER CONTEXT

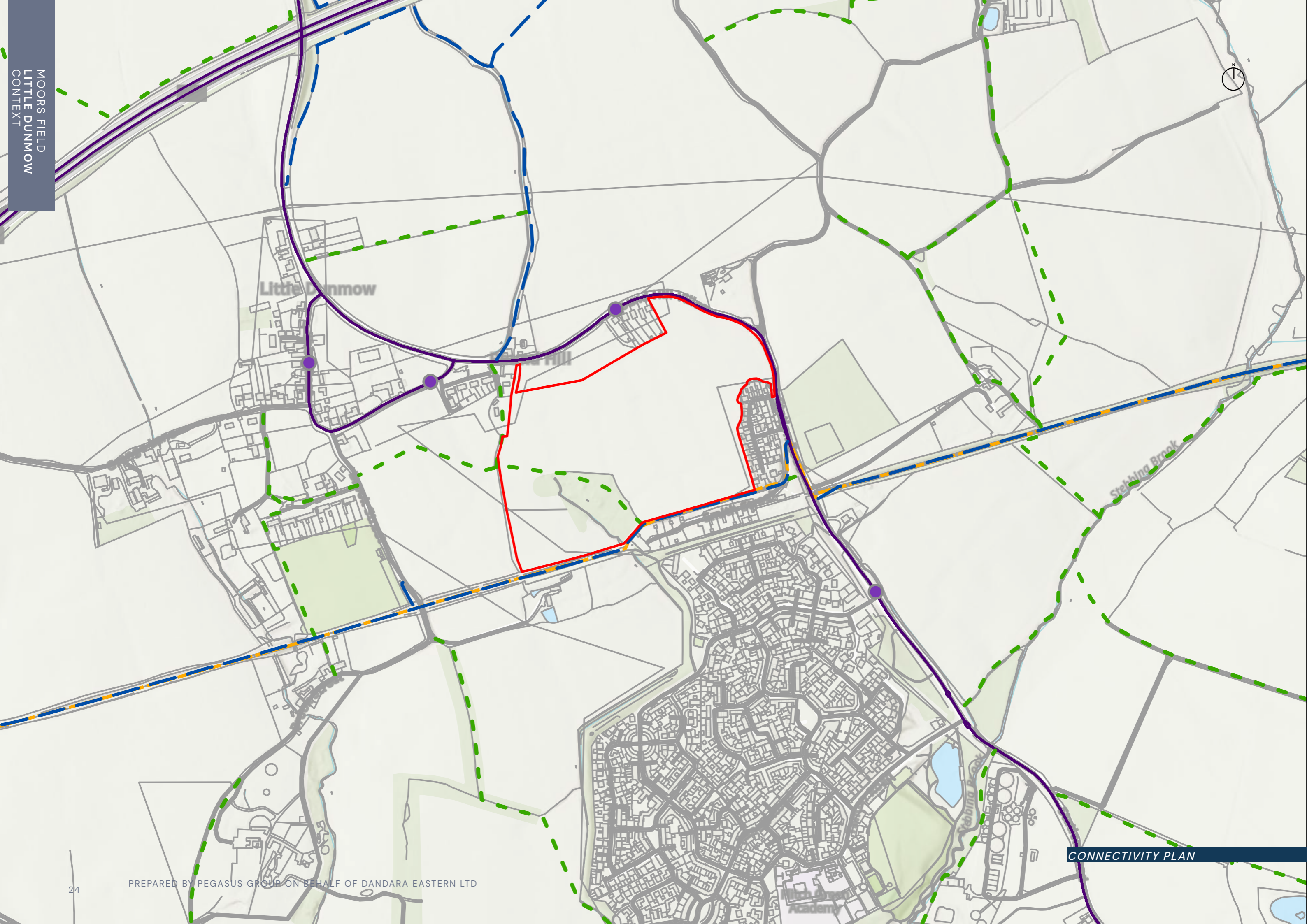
The Site, covering 14.06ha in area, is located on agricultural land, with an area of existing woodland and shrub located to the south western portion of the Site. An existing footpath runs through the Site, connecting Brook Street to Flitch Way, the latter of which runs adjacent to the Site's southern boundary.

The Site lies between the village of Little Dunmow and the residential area of Flitch Green. Further residential development lies close to the Site off Station Road, including a Taylor Wimpey scheme adjoining the Site's eastern boundary. Whilst the wider context is generally rural in character, the Site lies within 3km of the town of Great Dunmow to the west and easy access to the A120 road, providing links to the larger town of Baintree 7km to the east.

BOUNDARIES AND EDGES

The Site is of an irregular shape, formed of natural and built boundaries. The north is bound by an existing vacant piece of land and row of residential properties, beyond which is Station Road. The eastern boundary is bound by Station Road and the aforementioned Taylor Wimpey scheme further southward. The southern boundary runs adjacent to the Flitch Way, with a traveller site beyond, whilst a further agricultural field lies to the western boundary.





Little Dunmow

Stepping Brook

Little Dunmow Academy

3.2 CONNECTIVITY

STREET PATTERN AND CONNECTIVITY

The Site is well connected to the surrounding area of Little Dunmow and to the wider area, with strong links to Dunmow, Great Dunmow and Braintree. The site benefits from easy access to public transport and key highway links. The plan opposite illustrates the location of the Site within the context of the local access and movement network.

EXISTING ACCESS NETWORK

At present the Site is only accessible through a restricted access points off Station Road and through Public Right of Way entries. The Site is well served by public transport services with bus services off Station Road close to the site.

The site is also well connected to existing walking routes and there are proposals to bolster pedestrian and cycle connectivity within the Site and local area. The improved accessibility will help reduce car dependency for new and existing residents.

LOCAL BUS AND RAIL SERVICES

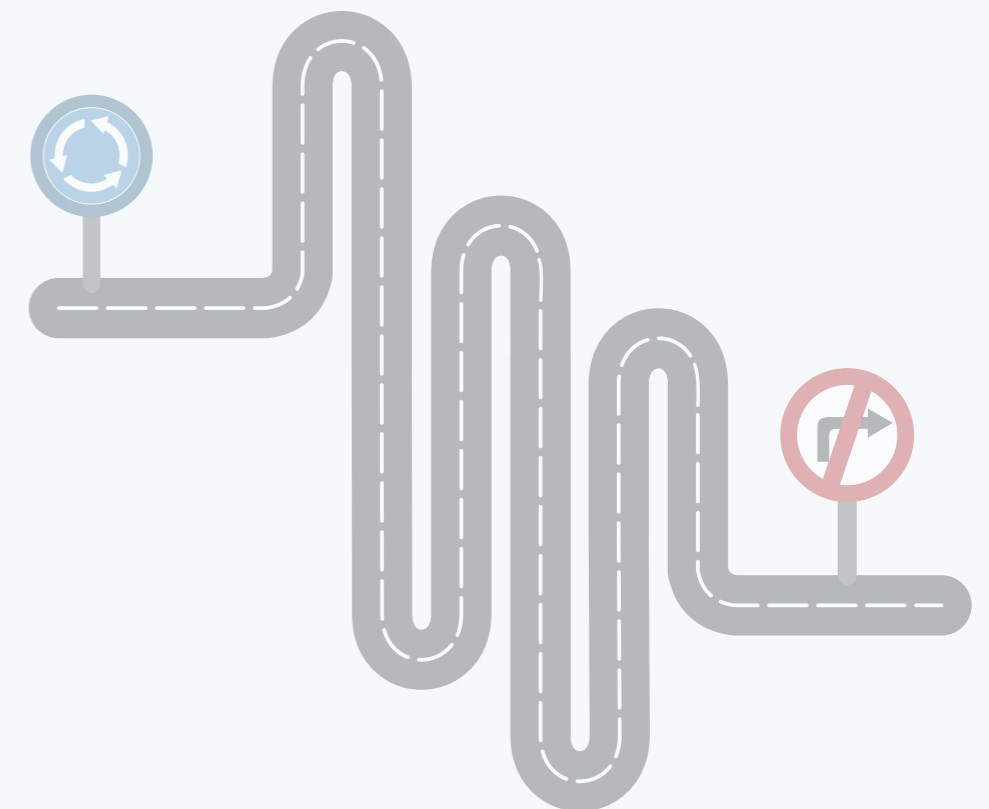
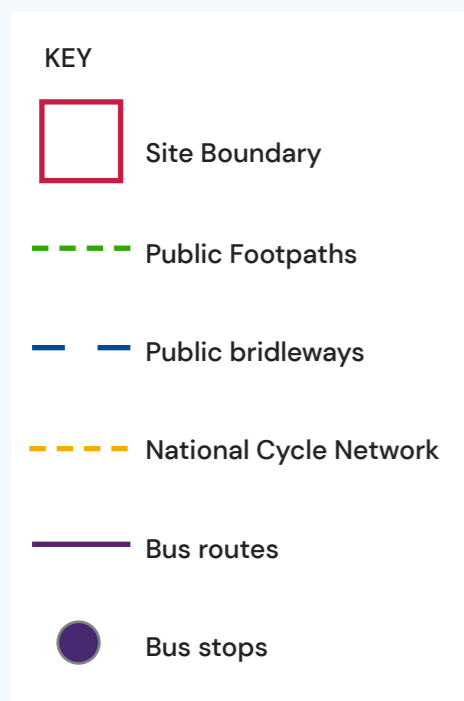
The nearest bus stops are located on Station Road. From here there are several regular services to Baintree, Stanstead Airport and Great Dunmow, amongst others. Services include the 133 Sapphire, 16, 414, 451 and 570. Thereafter, further connections provide access to the wider south east, including London.

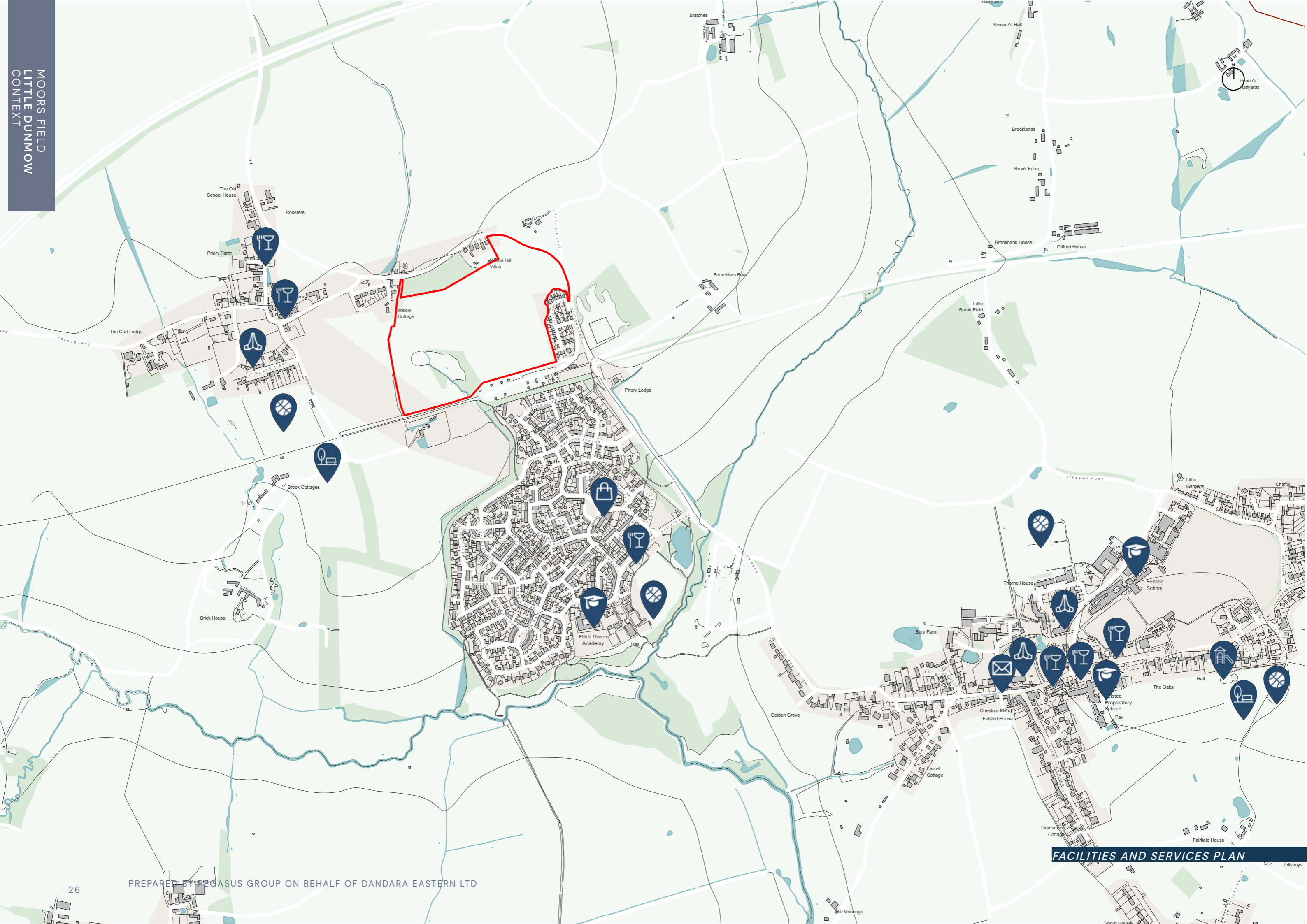
The closest railway stations to the site lie within Baintree and Stanstead Airport, roughly 10k from the Site. From these stations direct links are available to London.

WALKING AND CYCLING

An existing footpath runs through the site, connecting Brook Street to Flitch Way, the latter of which runs adjacent to the Site's southern boundary. These footpaths provide access to the wider Public Right of Way Network and link into services and amenities locally in Little Dunmow.

The Flitch Way forms part of the National Cycle Network, providing a good level of cycle infrastructure for medium and long distance journeys.





FACILITIES AND SERVICES PLAN

3.3 LOCAL FACILITIES

A key part of achieving sustainable development is to enable access for residents to a range of local facilities without placing a reliance on the use of the private car.

The site is comfortably within walking distance to various local amenities. The plan opposite outlines facilities within 800m (approx 10 minute walk) both within Little Dunmow and Flich Green, including a primary school, convenience store, public house and sports facilities.

Further afield, a range of further facilities are available within Dunmow (within 2km to south east) and Great Dunmow (3km to west).












CO-OP FOOD - FLITCH GREEN - HALLETT ROAD



THE FLITCH OF BACON - PRIVATE DINING - THE STREET

KEY:

-  Site Boundary
-  School
-  Sports facilities and Recreation
-  Place of Worship
-  Park
-  Post Office
-  Food and Drink
-  Playground
-  Shop



3.4 LOCAL CHARACTER

The National Design Guide states that well-designed new development is influenced by:

“...an appreciation and understanding of vernacular, local or regional character, including existing built form, landscape and local architectural precedents;” (para. 53, NDG 2021)

An analysis of the existing built form can help identify patterns of development and key design components. Together these character generators and design components can help to inform the design approach.

CHARACTER REFERENCES

Whilst the broader character is largely rural, the Site lies between the existing residential development within Little Dunmow to the north west and Flitch Green to the south. A range of architectural styles, detailing, materials and thereby character is evident which has informed the design of the development.

The residential development to south is formed of modern residential properties developed from the early 2000s. Little Drumond to the north west is more traditional in character, including a number of listed properties.

Using the following details within the reserved matters application will ensure that the proposed development integrates with Flitch Green and responds to the local context of the site.

Urban Form

- The main streets must have as much continuous built frontage as possible, which can be achieved through the use of drive-unders and carriage arches.
- The streets should generally be laid out with footways on each side of the carriageway where houses are found.
- Shared surface areas are appropriate for lower order streets and must be wide enough to comfortably accommodate vehicle movements and pedestrians and cyclists.
- Street trees and parking must be accommodated within a well planned streetscape.

- Development form must follow the form of the roads, with houses aligned parallel to the street.

Building Plot Form

- The development must include a mix of terraced cottages, large detached and semi-detached and houses, in accordance with the character area descriptions.
- Development must be a maximum of 2.5 storeys in height, with 2 storey dwellings the most common height.
- Houses can be set on the back of the pavement or with varying sizes of front gardens as appropriate.
- Plot forms should vary with both wide and narrow fronted plots found.
- Rear garden sizes are typical of the age and density of the development.
- Dwellings should generally have eaves facing the streets, with some gable fronted elements introduced.
- Overhanging gables at the first floor level are also appropriate.



BAYNARD AVENUE



STATION ROAD

CHARACTER REFERENCES



THE STREET



BAYNARD AVENUE

CHARACTER REFERENCES

Car Parking

- Residents and visitors parking must be planned for within the development.
- Appropriate parking solutions include allocated on street parking, on-plot parking to the side and courtyard parking to the rear of properties.
- Parking to the front of terraces should only be used where necessary.

Details and Materials

- Common walling materials should include red brick and smooth render (pastel colours)
- Some horizontal wooden cladding in black or white/cream is appropriate.
- Vertical brick detailing around windows and doors should be used, in the same colour or contrasting brick.
- Roofs should be a mix of red or brown tiles or grey slate.
- Dormer windows should have gable roofs.
- Chimneys should be set in line with walls or create a feature on the gable end of properties.
- Windows should be either timber painted or uPVC – white.
- Casement windows should be used.
- Doors should be coloured with glazed vision panels.
- Varied use of porches and canopies / threshold details should be used.
- Where properties are set back from the pavement, boundaries should be formed by a mix of brick walls, fences, hedges or planting within front gardens.

Open Spaces

- Open space should be concentrated in a large usable area.
- Smaller incidental open spaces should be found within the residential area.

The adjacent images show local examples which incorporate these design references, and have provided guidance for the proposed development.

TOPOGRAPHY PLAN








3.5 ARBORICULTURE

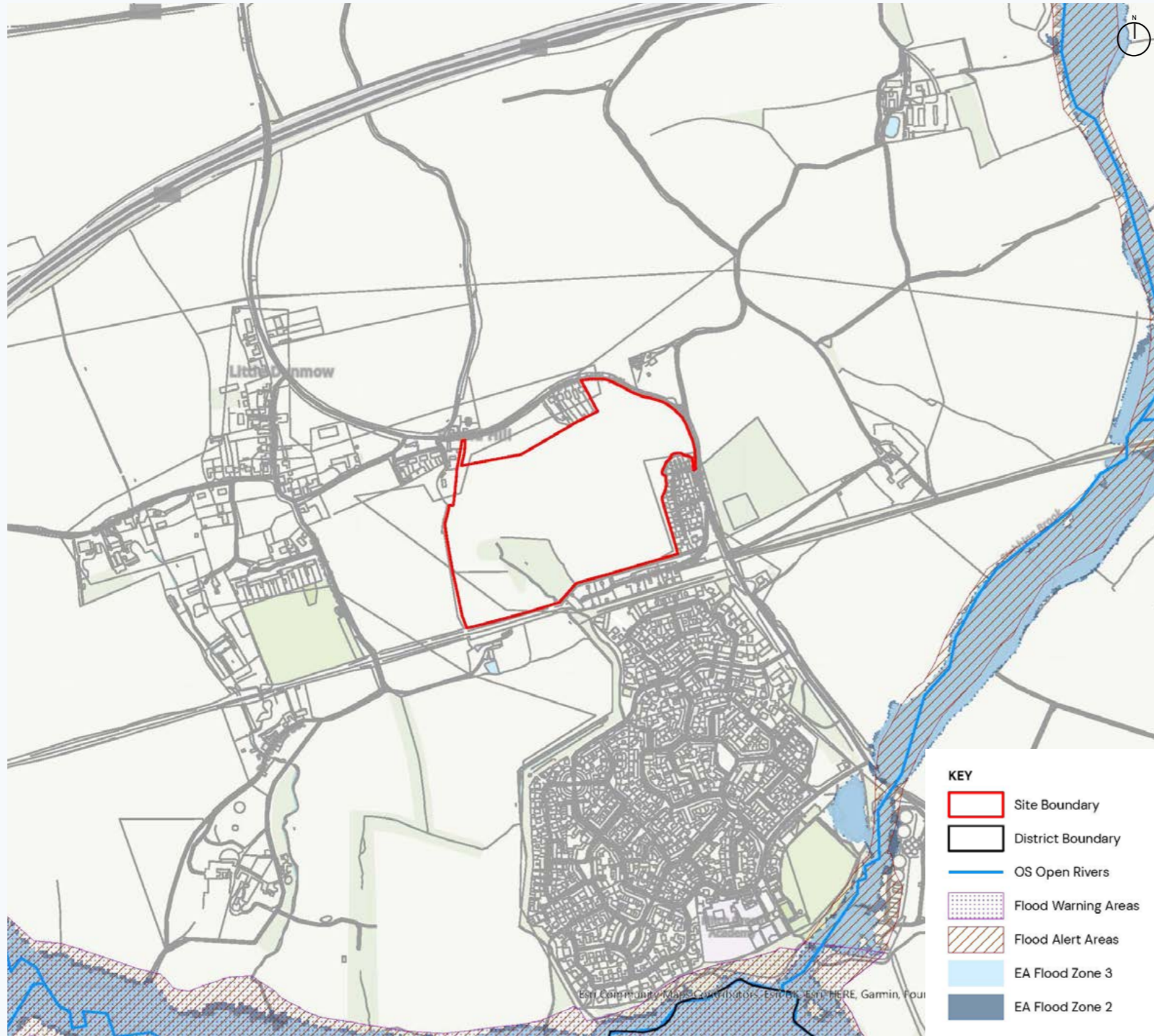
As can be seen from the adjacent plan, the survey assessed twenty seven individual trees, eight groups and six hedges, the quality and value of which are summarised below.

No trees were indicated as category A. Three groups were assessed as BS5837: 2012 'Moderate Quality' retention category B. Five groups and five hedges were assessed as 'Low Quality' retention category C and two trees were assessed as 'Unsuitable' category U.

There are also a number of trees that are identified as Root Protection Areas. All of the surveyed trees are either within or on the boundaries of the application site.

KEY

-  Category B tree, group or hedge
-  Category C tree, group or hedge
-  Category U tree, group or hedge
-  Root Protection Area (RPA)
-  Site Boundary











3.6 DRAINAGE AND FLOOD RISK

As can be seen from the adjacent plan, the Site lies wholly within Flood Zone 1 where the risk of flooding is at its lowest. The nearest flood risk area is associated with Stebbing Brook, over 500m from the Site.

OPPORTUNITIES & INFLUENCES



- KEY**
-  Potential Site Access Point
 -  Bus route
 -  Bus Stops
 -  Grade II listed building
 -  Existing public right of way
 -  Existing Woodland
 -  Trees to be retained
 -  Notable gradient change

3.7 OVERVIEW OF SITE & CONTEXT

Following an assessment of The Site and its surroundings, the key constraints and opportunities have been identified. These are outlined below and are illustrated on the adjacent opportunities and influences plan. These have helped inform the current design proposals for The Site.

CONSTRAINTS

- Existing topography of the site;
- The existing wildlife;
- Existing boundary and internal vegetation and woodland;
- Existing road network and potential points of access; and;
- Sensitive boundaries with existing residential areas and listed buildings.

OPPORTUNITIES

- Enhance existing landscape and ecology features;
- Improved connectivity for the wider area;
- Provide new connections and utilise existing pedestrian links alongside and through the application area;
- Provide Little Dunmow with affordable housing to widen local housing choice;
- Provide a range of house types and styles;
- Provide new, safe and attractive areas of public open space for both new and existing residents.

4

DESIGN DEVELOPMENT

4.1	DEVELOPMENT FRAMEWORK	36
4.2	CONCEPT DEVELOPMENT	36
4.3	OUTLINE APPROVAL FRAMEWORK	37
4.4	DESIGN PRINCIPLES	38
4.5	ILLUSTRATIVE MASTERPLAN	41

4 DESIGN DEVELOPMENT

4.1 DEVELOPMENT FRAMEWORK

In line with National and Local Government Guidance and Policy, considerable importance has been placed on achieving a high standard of design across the site. Successful urban design is dependent upon achieving an appropriate relationship between community needs, development principles, development form and a positive response to local conditions.

Plan-makers, as well as decision makers should apply a presumption in favour of sustainable development, which will mean that:

All plans should promote a sustainable pattern of development that seeks to: meet the development needs of their area; align growth and infrastructure; improve the environment; mitigate climate change (including by making effective use of land in urban areas) and adapt to its effects (Para 11(a), NPPF 2021)

The application of key urban design objectives will ensure a high-quality layout is achieved, whilst the early identification of the sites features will ensure that the proposals are sensitively assimilated into the landscape and urban fabric.

The developed layout has been based on an accurate topographical survey however it has been ensured, as part of the design process, that the proposed scheme still correlates and complies with the Design Code areas.

The Illustrative Masterplan and Design Code established the following key principles;

- The outline approval plan shows that the main land use on the site will be residential development of up to 160 dwellings, with associated public open space, surface water attenuation and structural landscaping. The development parcels illustrated opposite include roads, footpaths, private gardens and parking areas.
- The site will deliver around 8.4ha of open space, with the main open space found to the north, west and south of the site in the form of a countryside park. Green links are created within the development area to provide a landscaped setting to the proposed new homes.
- The density across the site should be varied with lower density along the interfaces with the northern open space, medium density within the centre of the site and higher density along the southern edge of the site. The average density based on 160 dwellings within 5.54ha of development area is around 30dph.
- The road layout within the scheme will ensure suitable vehicle access to all properties, and include some shared space streets to minimise vehicle speeds.

















4.2 CONCEPT DEVELOPMENT

The Site has seen multiple iterations of masterplans developed by Pegasus Group. These have been informed and revised along the course of development and the Office Hub location was revised following pre-app discussions with Little Dunmow Parish Council.

This application builds upon this history. While acknowledging the previous discussions, the masterplan has been re-evaluated from an urban design perspective so the final masterplan can deliver the full potential of the Site.

4.3 OUTLINE APPROVAL FRAMEWORK

The framework plan creates a developable area with green corridors linking the northern green space through to the natural corridor to the west, a pedestrian route is located from north to south with the blue linking the SUDS pond. There are a number of key access points for pedestrians and cyclists and the existing PRoW has been retained to enhance connectivity and encourage active travel.

-  Site boundary
-  Proposed all modes access
-  Existing access to Public Right of Way - footpath
-  Proposed pedestrian and cycle access
-  The Flitch Way / Saffron Trail (offsite)
-  Existing Public Right of Way retained
-  Existing pedestrian / cycle link to Flitch Green / Little Dumow
-  Existing trees retained
-  Existing scrub / woodland retained
-  Existing stream retained
-  Proposed development area (to include streets, residential development, an office hub and green corridors)
-  Proposed public open space (to include drainage attenuation, landscape buffers, countryside park, allotments, community orchard and children's play)
-  Proposed landscape buffer (min. 8m along boundaries with adjacent properties, min. 16m along site frontage)
-  Proposed green corridors within the development area (exact location and size to be determined at Reserved Matters stage)
-  Proposed swale corridors within the development area (exact location and size to be determined at Reserved Matters stage)
-  Proposed surface water attenuation area (exact location and size to be determined at Reserved Matters stage)



4.4 DESIGN PRINCIPLES

The principles which have been developed provide a framework by which to create a distinctive place, with a consistent and high-quality standard of design. These principles have been derived from the site assessment, in conjunction with the delivery of a high-quality development which achieves the criteria set out within the NPPF, namely:

FUNCTION & QUALITY

“...will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development” (para. 130(a), NPPF 2021)

- New development provides the opportunity to establish a distinctive identity to a place which, whilst having its own character, integrates with the surrounding built form and landscape context;
- Retention of the existing landscape features on the site where possible, notably the integration of Moors Woodland into the extensive open space and park;
- Provision of a range of house types, tenures and sizes in order to cater for choice and a variety of households;
- In-built ‘robustness’ – the ability of the development, including individual buildings, to adapt to changes such as use, lifestyle and demography over time;
- Provision of Sustainable Drainage systems to ensure that the development does not increase the risk from flooding in the area;
- New development at Moors Fields will be designed to deliver the proposed residential use and will represent value for money in terms of lifetime costs;
- Make efficient use of the site through proposing a development with an appropriate density; and
- It will be intuitive, comfortable, safe and easy for all to use irrespective of the environmental conditions identified within Section 4 of this statement.

VISUALLY ATTRACTIVE

“...are visually attractive as a result of good architecture, layout and appropriate and effective landscaping” (para. 130(b), NPPF 2021)

- Provision of a clear hierarchy of connected spaces and places, including streets, accessible by a variety of users, which consider the design of the space as well as its function as a movement corridor;
- Integration of existing and proposed landscape features will help to soften the built form, including Moors Woodland and extensive open space around the site;
- Minimise the impact of the development on the open countryside and surrounding context;
- New development will be set within a considered and attractive landscape setting; and
- Enrich the qualities of the existing place, with distinctive responses that complement the setting, respect the grain of the local area and acknowledge the established local character.

RESPONSE TO CONTEXT

“...are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities)” (para. 130(c), NPPF 2021)

- Integration of the development into the existing surrounding built form of Little Dunow and the local area whilst creating a separate residential area, particularly in relation to development block form, scale, height and massing;
- Consider how distinctive elements of the local vernacular and the best examples of local buildings relate to the space they enclose, and how this might be reflected within the proposals and can be used to inform the architecture of the proposed development;
- Consider carefully the specification of materials that respect/enhance the local vernacular;
- Respond to the existing site topography including the consideration of key views in and out of the site;
- Retention of the existing landscape features and habitats on the site; and
- Protection of existing and proposed residential amenity through appropriate separation distances and landscaping

STRONG SENSE OF PLACE

“...establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit (such as increased densities)” (para. 130(d), NPPF 2021)

- Allow the key characteristics of the local area to influence the character of the development;
- Position key spaces and focal points where movement corridors converge to encourage activity and vitality;
- Creation of a development which allows ease of movement for all types of users and provides equal employment, social, community, leisure and retail activity opportunities for all;
- Consider how open spaces will best meet the recreational needs of the local community, thereby encouraging social interaction;
- Consider how the type and positioning of enclosures and soft landscape will clearly define the ownership of the space between buildings;
- Incorporate existing and proposed landscape features into the proposals, so as to enhance the richness and attractiveness of the streetscape; and
- Consider carefully texture, colour, pattern and durability of materials and how they are used.

ACCESSIBILITY

“...optimise the potential of The Site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks” (para. 130(e), NPPF 2021)

- Integration of the proposed development into the existing movement network of footpaths, cycleways, bus routes and vehicular routes, with public transport provision located within easy walking distance of the new dwellings;
- Provision of multiple access points into the development forming part of a permeable network of streets which connects to the existing Public Right of Way network and assists in dispersing traffic (vehicular and pedestrian);
- Maximisation of the opportunities for alternative modes of transport to the car particularly walking, cycling and bus travel;
- Creation of a legible and permeable development, that is easy to navigate for all users, with a clear movement hierarchy providing easily recognisable routes, balancing the street as a space alongside its function as a movement corridor;
- Enhancement and extension of the existing public rights of way network as an integral part of the development, particularly facilitating sustainable access to the Fritch Way and Little Dunmow;
- Ensure a mix of appropriate residential tenures to further promote the economic and social success of the scheme; and
- Consider the potential for a variety of uses to be included within the development to promote its economic and social success, and to reduce the need to travel

SAFE, INCLUSIVE & ACCESSIBLE PLACES

“...create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience” (para. 130(f), NPPF 2021)

- Convenient, safe and direct access for all residents to the existing and proposed local services and facilities including schools, retail, community uses and employment opportunities;
- Provision of extensive and accessible public open spaces and recreation areas to meet the needs of the local community whilst encouraging social activity;
- Creation of a clearly defined public realm through the provision of a defined residential built area and variations in the methods of enclosure of private spaces;
- Consideration of the proposals in relation to the location of nearby buildings to the Site, gradients, and the relationship between various uses and transport infrastructure, particularly for those with disabilities; and
- Control of access to private areas, particularly rear gardens and parking courts.

4.5 ILLUSTRATIVE MASTERPLAN

The illustrative masterplan further develops the framework plan and highlights how the scheme could be delivered in further detail.

Key features include the development of multi-functional green and blue infrastructure networks with designated areas for play, rest and community functions.

The developable area has been broken into more clearly defined parcels separated by permeable routes through the site with a clear road hierarchy developed to increase legibility.

-  Site boundary
-  Proposed all modes access
-  Existing access to Public Right of Way - footpath
-  Proposed pedestrian and cycle access
-  The Flitch Way / Saffron Trail (offsite)
-  Existing Public Right of Way retained
-  Existing pedestrian / cycle link to Flitch Green / Little Dunmow
-  Proposed on-site footpath / cycle path
-  Proposed managed / semi natural open space
-  Proposed strategic planting areas
-  Existing trees retained
-  Proposed trees
-  Existing scrub / woodland retained
-  Proposed hedges
-  Proposed play area with 20m offset to homes
-  Proposed surface water attenuation area and swales
-  Existing stream retained
-  Potential landscape buffer between Flitch Green and Little Dunmow, and between the site and existing homes along Harrisons Road / Ainsworth Drive
-  Potential countryside park with new walking routes
-  Potential natural play area
-  Potential pocket park
-  Potential trim trail
-  Potential micro allotments
-  Potential community orchard
-  Potential drainage and ecological area
-  Potential new family homes
-  Potential office hub



ILLUSTRATIVE MASTERPLAN





5

DESIGN PROPOSALS

5.1	USES	49
5.2	MOVEMENT	51
5.3	BUILT FORM	53
5.4	SCALE	55
5.5	DISTINCTIVE PLACES	56
5.6	IDENTITY	59
5.7	HOMES AND BUILDINGS	63
5.8	MATERIALS	65
5.9	BOUNDARY TREATMENT	69
5.10	PUBLIC SPACES	70
5.11	PEDESTRIAN MOVEMENT	75
5.12	GREEN AND BLUE INFRASTRUCTURE	77
5.13	NATURE	83
5.14	SUSTAINABILITY	84



KEY

-  Site Boundary (14.07 Ha)
-  Location of existing drain to be retained (position to be confirmed)
-  Location of Affordable shared Ownership plot
-  Location of Affordable Rent plot (tenure blind)

PRESENTATION LAYOUT

5 DESIGN PROPOSALS

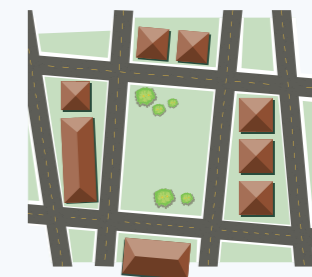
This chapter explains the principles that have been applied to particular aspects of the design, such as layout, use and amount, access and movement, appearance, scale, landscape and sustainability, and how they have responded to the principles agreed within the Design Code.



USES
Mixed and integrated



MOVEMENT
Accessible and easy to move around



BUILT FORM
A coherent pattern of development



HOMES AND BUILDINGS
Functional, healthy and sustainable



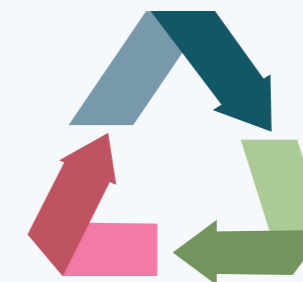
IDENTITY
Attractive and distinctive



PUBLIC SPACES
Easy to find your way around






GREEN INFRASTRUCTURE
Enhanced and optimised



SUSTAINABILITY
Resilient and made to last



KEY

-  Site Boundary
-  Location of existing drain to be retained (position to be confirmed)
-  Location of affordable plot (tenure blind)

DETAILED LAYOUT

5.1 THE DETAILED LAYOUT

The Detailed Layout has taken the core principles of the Illustrative Masterplan and developed them into a high quality and meaningful place.

The core principles of the layout are as follows:

HIGH QUALITY DESIGN

The layout seeks to deliver a well considered scheme with the proposals being sensitively integrated on the site and into the wider context, with priorities given to green and blue infrastructure as well as access routes favouring pedestrians.

CHOICE & IDENTITY

The development proposes to deliver a variety of residential dwellings through a mix of densities and typologies delivering a range and choice to meet varying needs.

HEALTHY LIVING

Walking and cycling are actively promoted through pedestrianised and landscaped routes while the layout effectively links into the wider network benefiting both existing and future residents.

COMMUNITY

The development also delivers a variety of open space typologies, community food growing, formal and informal areas of public play and an office hub within easy reach of existing and future residents ensuring maximum community benefit.

STRATEGIC GREEN OPEN SPACES

Multi-functional green open space will provide places for people and wildlife with new open spaces for recreation and play.

INTEGRATING WITH NEIGHBOURING AREAS

The development responds to local residential layouts and provides good connectivity to adjoining communities.

SUSTAINABLE DEVELOPMENT

Integration of a range of measures to reduce carbon emissions, mitigating the effects of climate change, and adaptation measures to ensure the long term resilience of the development to the effects of climate change.

WHAT THE DESIGN CODE STATES:

The Design Code states, alongside the Development Framework plan, the following size areas for the scheme;







Total Site Area:	13.96ha
Total Development Area:	5.54ha
Total Open Space:	8.42ha

DESIGN CODE COMPLIANCE:

The developed layout has been based on an accurate topographical survey however it has been ensured, as part of the design process, that the proposed scheme still correlates and complies with the Design Code areas;

Total Site Area:	13.96ha
Total Development Area:	5.54ha
Total Open Space:	8.42ha



- KEY**
-  Site Boundary
 -  Built Form
 -  LEAP
 -  Open Space
 -  Orchard
 -  Attenuation Areas

LAND USE PLAN



MIXED & INTEGRATED

ACCOMMODATION SCHEDULE					
Open Market Dwellings					
CODE	NAME	BEDS	STOREY HEIGHT	NO.	%
1BB	1BB	2	1	2	2.1
2BB	2BB	2	1	3	3.1
VYN	Vyne	2	2	16	16.7
2 Bed				22	22%
CHA	Charleston	3	2	12	12.5
FRO	Frogmore	3	2	7	7.3
GOS2	Gosford	3	2	16	16.7
3 Bed				35	36%
PEN	Penhurst	4	2	8	8.3
WOB	Woburn	4	2	16	16.7
GOO	Goodwood	4	2	5	5.2
WIN	Windsor	4	2	3	3.1
CHR	Chartwell	4	2	4	4.2
4 Bed				36	38%
KIN	Kingston	5	2	4	4.2
5 Bed				4	4.2%
TOTAL OPEN MARKET				96	60%
Affordable Dwellings					
CODE	NAME	BEDS	STOREY HEIGHT	NO.	%
1BF	1BF	1	1	12	18.8
2BF	2BF	1	1	4	6.3
1BB	1BB-M4(3)	1	1	2	3.1
2BB	2BB-M4(3)	1	1	2	3.1
1 Bed				20	31.3%
HER	Hertford	2	2	18	28.1
2 Bed				18	28.1%
PAR	Parham	3	2.5	22	34.4
3 Bed				22	34.4%
BLE	Bletchley	4	2.5	4	6.3
4 Bed				4	6.3%
TOTAL AFFORDABLE				64	40%

5.2 USE AND AMOUNT

(The Town and Country Planning (Development Management Procedure) (England) Order 2015 states that “amount” means (a) the number of proposed units for residential use). The development proposals include the following;

RESIDENTIAL – UP TO 160 DWELLINGS (CLASS C3)

The development proposes to provide 160 dwellings with 5.54ha of residential development. This equates to 33dph and allows space for a range of dwelling types suitable for people of different ages and lifestyles. This area will include access roads within the Site, private garden space, car parking areas, significant amounts of open space and landscaping. It therefore excludes major distributor roads, open spaces serving a wider area and significant landscape buffer strips.

AFFORDABLE HOUSING

40% affordable housing provision is proposed, catering for a range of affordable housing needs through the provision of apartments and 2, 3 & 4 bedroom homes. The affordable provision will be spread across the Site and be tender blind.

PUBLIC OPEN SPACE & GREEN INFRASTRUCTURE

The public open space expanding over 8.42ha will feature the retained long distance views and the enhanced riverside with extensive landscaping providing habitats and natural amenity spaces.

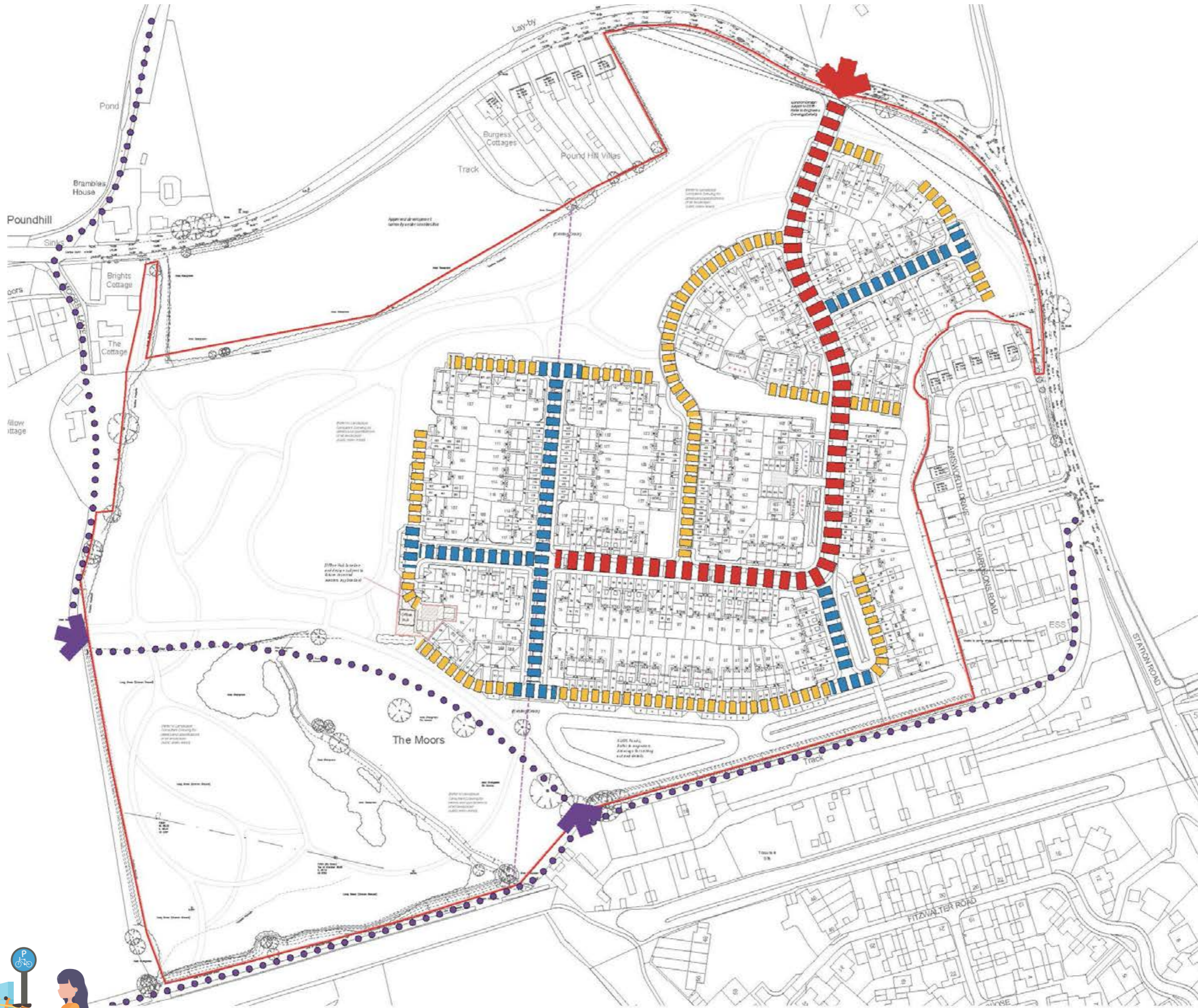
The landscape proposals will enhance the existing quality of biodiversity and seek to retain the existing trees and supplement them with new planting. Furthermore, the development will provide recreational opportunities in the form of play provision, opportunities for community foraging and cycle and pedestrian links through the following features:

- A LEAP
- Fruit trees
- Landscaped SuDS features
- A Site-wide network of footpaths and a dedicated cycle route
- Retained (FPO8) and diverted (FPO6) footpaths traversing the Site.








The proposals for the public open space will also assist in integrating the proposed development within its surroundings, visually; environmentally, through enhanced habitat corridors; and, socially through the amenities created.

ATTENUATION & DRAINAGE

Linear SUDS ponds are proposed to the south of the site, at the lowest point on site to ensure effective implementation of drainage and attenuation. These naturalistic landform and planting of swales and the southern drainage basin provide opportunities to interact with nature and be close to water, while also managing water in a sustainable and ecologically beneficial manner.



KEY

-  Site Boundary
-  Main Street
-  Residential Street
-  Private Drives
-  All Modes Access
-  Pedestrian Access
-  Existing PRoW

STREET HIERARCHY PLAN



ACCESSIBLE & EASY TO MOVE AROUND

5.3 ACCESS AND MOVEMENT

The Indicative Masterplan shows the disposition of land uses and the proposed structure for movement within the development. A well-connected movement network, accessible by all users, is proposed which helps to ensure that all areas of the development will be accessible, easy to navigate, safe and secure. The proposed access and movement strategy will focus on the delivery of the following elements which are in accordance with the objectives of national and local planning policy:

- Proposed access points;
- Proposed pedestrian and cycle movement network;
- Street hierarchy and;
- Parking strategy.

ACCESS

The adjacent plan illustrates the proposed hierarchy of streets within the development. Access into the site has been carefully considered for both pedestrian and vehicular users, with routes into the site being direct, clear and safe, whilst according with the width requirements. The main vehicular access into the site will be from Station Road, the main through movement route in the area. Wider pedestrian routes have been improved and connected into to ensure effective connectivity between the site and wider communities.

Enhanced connectivity is proposed to Flitch Watch through new site entrances and enhancements to the footway along Station Road.

WALKING CYCLING AND PUBLIC TRANSPORT

The development of a pedestrian/cycle network within The Site is seen as an integral part of the transport infrastructure for the proposed development. The following measures to provide accessibility by foot and cycle are proposed:

- Pedestrian and cycle links predominantly pass through open spaces but will also link into the proposed street network and integrate with the wider bus network both to the north and south of the site.
- Additional connections onto the Flitch Way at both the eastern and western corners of the site to promote walking and cycling are to be provided.

- Upgrades to local bus stops should be provided, to encourage their use. These upgrades have been agreed with Uttlesford District Council and form part of S106 Agreement.
- Extensive natural surveillance and numerous traffic calming measures, including raised table junctions will maximise the safety of the route network and further encourage active travel.

PARKING

One of the key elements to creating a successful development is to ensure that the building layout and landscaping are the prominent features of the development. To achieve this, both resident and visitor parking is provided within the development in accordance with Essex County Council Parking Standards.

Appropriate parking solutions include allocated on street parking, on-plot parking to the side and courtyard parking to the rear of properties – a range of car parking solutions have been incorporated into the scheme on each plot.

Parking to the front of terraces should only be used where necessary – parking to the front of certain properties has been used. This approach has been used to create a continuous built frontage, with rear parking courts within these streets representing an inefficient use of space which breaks up the frontage.

DESIGN CODE KEY POINTS

1 HIERARCHY OF STREETS

A clear, legible hierarchy of streets.







2 PARKING

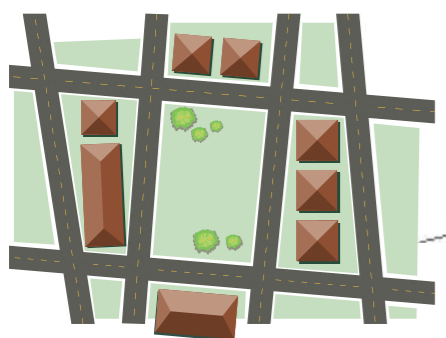
Sufficient parking

3 PEDESTRIAN CONNECTIVITY

Convenient and suitable pedestrian and cycle routes.



- KEY**
-  Site Boundary
 -  Duel Aspect Dwellings
 -  Surveillance of Public Open space
 -  Key Nodal Dwellings
 -  Key areas of Public Space
 -  Pedestrian Connections
 -  Lanes Providing Country Views



A COHERENT PATTERN OF DEVELOPMENT

LAYOUT PRINCIPLES PLAN

5.4 BUILT FORM

The design solution for the site reflects the variety in townscape form that can be seen in Little Dunmow and in particular the area surrounding the site.

The arrangement of the built form at Moorsfield creates a network of attractive street and spaces including:

The proposed layout builds upon the Illustrative Masterplan submitted alongside the outline planning application, including how buildings, routes and open spaces are provided, placed and orientated in relation to each other and buildings and spaces surrounding the development. Key buildings have been positioned at the end of vistas within the development to properly terminate views and aid with wayfinding.

LEGIBILITY

A series of dwellings are strategically positioned so that they frame key views and formalise open space, with a formal build line fronting the open space. A variety of house types and materials have been utilised along frontage to key areas to give a varied roofscape and enhance the character of the development.

STREETS FOR ALL

Streets must be considered as social spaces, rather than a place for vehicles to drive and park in. Cyclists are accommodated on the carriageway, and footpaths are included throughout the site to aid non-vehicular movement.

AMENITY GREEN SPACE

The layout ensures that all areas of amenity green space are fronted by dwellings, creating defined spaces that are overlooked creating activity and ensuring these spaces are secure. Additionally, dual aspect dwellings are positioned at key corners, with habitable rooms with windows overlooking both street frontages, ensuring street corners add life to the street scene.

DESIGN CODE KEY POINTS

1 LEGIBILITY

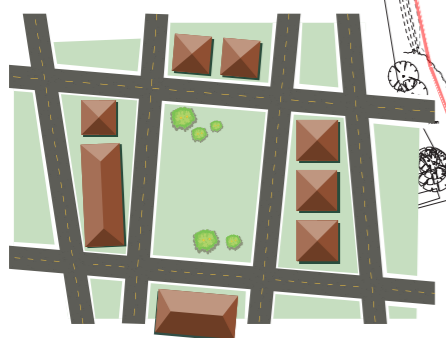
A clear, legible hierarchy of streets while providing key nodal buildings.

2 CHARACTER

A landscape led scheme which is an extension of the character and “traditional” style of Little Dunmow.




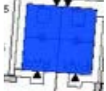
3 PEDESTRIAN CONNECTIVITY

Convenient and suitable pedestrian and cycle routes.



A COHERENT PATTERN OF DEVELOPMENT

KEY

-  Site Boundary
-  1 Storey (Bungalows and Garages)
-  2 Storey Houses
-  2.5 Storey Houses

BUILDING HEIGHTS PLAN

5.4 SCALE

DENSITY

Dwellings will be predominantly 2-storeys, which is appropriate to the setting of the development.

Within the planning layout, a number of 2.5 storey dwellings are proposed. These have been placed in appropriate locations to provide variety in the building height and roofscape, and to terminate key views, acting as wayfinding devices within the street scene.

Subtle variations in height due to the layout of the house types, accompanied by architectural details such as a mix of side and front facing gables, bay windows, canopies and varied roof forms introduce additional variety into the roofscape.

CONTINUITY & MASSING

Key development frontages, such as those overlooking areas of public open space and following the primary movement route, will be particularly prominent and critical to the appearance of the development. Particular attention has been paid to the massing and architectural style of these buildings, so that they contribute positively to the quality and character of the new development.

These frontages are designed with consideration given to the spaces they adjoin, in order to provide a cohesive approach to these prominent positions.

DESIGN CODE KEY POINTS

1 BUILDING HEIGHTS

Dwellings will be predominantly 2 storeys.

2.5 storey dwellings to be used where appropriate.

5.5 DISTINCTIVE PLACES

TOPOGRAPHY

One of the key influencing existing features of the site is its topography. Although not detrimentally steep, in construction terms, the gentle slope of the site has been considered so that gradients are integrated into the layout at ground level and views above rooftops are effective. Linear SUDS ponds are located at the south of the site, at the lowest point on site to ensure effective implementation of drainage and attenuation.

ARCHAEOLOGY AND HERITAGE

The built form of the development is set approximately 150m away from the site's western boundary and behind a significant countryside park area. This will limit the effects on the designated heritage assets Willow Cottage, Ivy House and the Church of St Mary the Virgin. New planting has been introduced into this area to further screen the development.

ECOLOGY

The Moors woodland has been well integrated into the public open space and only where necessary have shrubs and woodland been removed to reopen the PROW and allow natural surveillance.

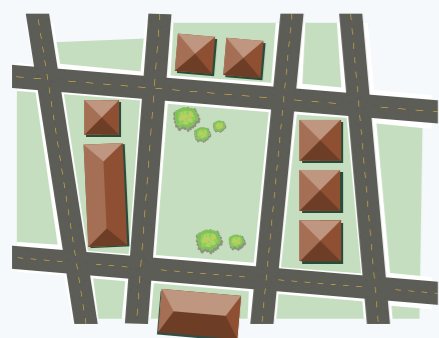
Soft landscaping within the countryside park will include habitats such as grasslands (featuring wild-flower) and large vegetative buffers to the boundaries providing a permeable site for wildlife. SUDS ponds and swales provide 'blue' fingers within the layout linking to extensive 'green' corridors throughout the site, providing a wide range of natural ecological habitats, which will also be supplemented by additional features such as bird and bat boxes.

UTILITIES

The existing foul sewer, with an associated easement, which runs through the centre of the site, has been considered within the layout to ensure it is contained under a road within the highway network or ease of future maintenance.

AIR QUALITY, NOISE AND LIGHT

An Air Quality Assessment accompanied the outline application, confirming future occupiers are not predicted to be exposed to air quality concentrations exceeding the UK AQS objectives. Therefore, it is not considered that any specific mitigation measures will be required for the resi-dential development. Transport related mitigation measures will be incorporated into the scheme (including electric vehicle charging points) to minimise the potential impact of the development on local air quality. Light considerations to be incorporated.



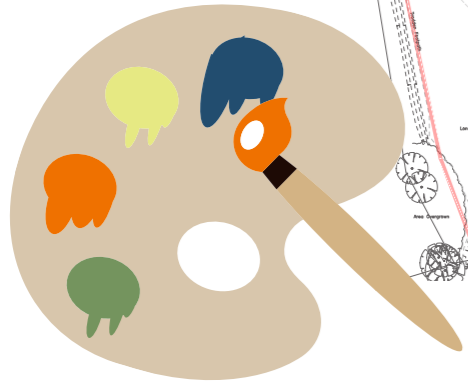
A COHERENT PATTERN OF DEVELOPMENT



LAND PARCELS SET BACK FROM WESTERN BOUNDARY TO REDUCE NEGATIVE HARM TO HERITAGE ASSETS







ENHANCEMENT OF EXISTING AREAS OF ECOLOGICAL BENEFIT



ATTRACTIVE & DISTINCTIVE

KEY

-  Site Boundary
-  Character Area 1 Landscape Edge
-  Character Area 2 Development Core
-  Character Area 3 Neighbourhood Edge

CHARACTER AREAS PLAN

5.6 IDENTITY

Character areas are a useful way of helping assimilate the design proposals within its surroundings, whilst providing a continuity of themes across the development and helping to generate a sense of place. Each character area will contain its own individual design components which aid in making it distinct from other areas. These components of character include the built form elements referred to earlier will include built form principles, and in addition consideration of changes in building height, building setbacks, landscape treatments, architectural detailing and materials.

The design code sets out points for creating distinctive places on the site, such as ensuring the layout has memorable character areas that have strong forms aiding legibility, including the following elements:

- Development must contain a mixture of building types from smaller homes to larger detached houses. Dwellings sizes should be varied within the layout of the development to create elevational interest and contrast in scale, detailing and materials.
- The density within the development must be varied to convey the transition from developed area to the edge of settlement, with building forms that respond to this change. The development should have an average density of approximately 30dph to make best use of available land.
- Good sized gardens for family houses must be included within the development, in line with the specifications set out in the Essex Design Guide.
- Open spaces must be usable and well-overlooked to provide good passive surveillance and encourage use by residents.
- Local materials and details must be used within a contemporary context to reflect the built heritage of the area. This should include linking buildings, in appropriate areas, with carriage arches, both with and without first floor accommodation above.

CHARACTER AREAS

The proposal aims to deliver a high-quality range of housing with a distinct character over the three defined character areas by utilising the following design principals;

- Variations in building materials, including stone, brick and render, will be used to respect the materials used in surrounding development and reflect its heritage.
- The density within the development is varied to convey the transition from developed area to the edge of settlement, with building forms that respond to this change. The average density of 33dph across the site broadly confirms with the density envisaged within the Design Code. 2.5 storey dwellings are predominantly located within the southern area of the development area, but also used sparingly to create dynamics in the street scene where varied height is useful along long stretches of road, for example.
- Good sized gardens for family houses are included within the development, and comply with the specifications set out in the Essex Design Guide.

The character areas themselves are positioned slightly differently to those defined within the Design Code. This is intentional and has been considered for the following reasons; 60% of the scheme is open space provision, meaning a large portion of the housing overlooks landscaped areas. We feel this would more appropriately be overlooked by the large detached dwellings of the 'Landscape Edge character area. The proposed scheme does not increase the size of this character area too much from the Design Code, but simply repositions it along the formally landscaped edges of the scheme. The Development Core and Neighbourhood Edge are largely the same positions, with minor tweaks to accommodate design considerations as part of the developed layout

The following pages describe how the character areas should be designed in such a way to help create a varied and diverse townscape.

The character areas are detailed below as follows:

- CA1: landscape Edge
- CA2: Development Core
- CA3: Neighbourhood Edge

A summary of the proposed residential character is set out on the following pages.

	Design Code Requirements	Proposed Development
1. Landscape Edge	1.24ha	1.40ha
2. Development Core	2.96ha	2.80ha
3. Neighbourhood Edge	1.34ha	1.34ha

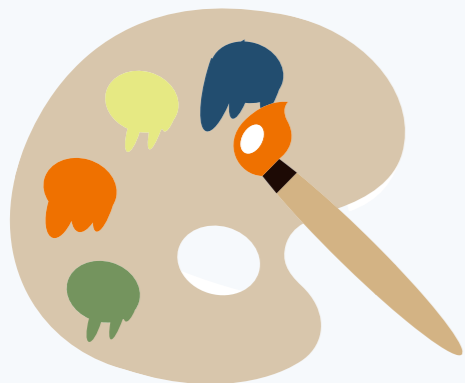
CHARACTER AREA 1 - LANDSCAPE EDGE



DESIGN CODE COMPLIANCE:

	1. LANDSCAPE EDGE
Density	21 dph
Storey Heights	2 Storeys
Dwelling Types	Detached (frequent) Semi-detached (Rare) Terraced (None) Apartments (None)
Wall Materials	Red Brick (occasional) Smooth pastel render (frequent) Horizontal wooden cladding (common)
Roof Materials	Brown Tiles Red Tiles

1.LANDSCAPE EDGE



ATTRACTIVE & DISTINCTIVE

CHARACTER AREA 2 - DEVELOPMENT CORE



2.DEVELOPMENT CORE

DESIGN CODE COMPLIANCE:

	2. DEVELOPMENT CORE
Density	30 dph
Storey Heights	2 Storeys (frequent) 2.5 Storeys (rare)
Dwelling Types	Detached (occasional) Semi-detached (frequent) Terraced (none) Apartments (minimal)
Wall Materials	Red Brick (frequent) Smooth pastel render (common) Horizontal wooden cladding (occasional)
Roof Materials	Brown Tiles Grey Slate

CHARACTER AREA 3 - NEIGHBOURHOOD EDGE



3. NEIGHBOURHOOD EDGE

DESIGN CODE COMPLIANCE:

	3. NEIGHBOURHOOD EDGE
Density	35 dph
Storey Heights	2 Storeys 2.5 Storeys
Dwelling Types	Detached (minimal) Semi-detached (frequent) Terraced (common) Apartments (none)
Wall Materials	Red Brick (frequent) Smooth pastel render (common) Horizontal wooden cladding (occasional)
Roof Materials	Brown Tiles Grey Slate



ATTRACTIVE & DISTINCTIVE

5.7 HOMES & BUILDINGS

The proposals will comprise a distinctive character and a strong sense of place, informed by important site features and the existing valued qualities of Little Dunmow.

The proposals aim to create a place that has a healthy, comfortable and safe internal and external environment.

House frontages could be carefully designed with generous windows from habitable rooms, clearly defined and attractive front doors and planting to act as buffer between the pavement and window.

Internal habitable rooms could have high levels of natural daylight and connect well to gardens and terraces.

Affordable housing will be well-integrated with a tenure blind approach so there is no discernible difference between private and affordable dwellings.

The Design Code sets out character references to be incorporated into the design proposals as part of the reserved matters submission to help to ensure that the proposed development integrates with Flitch Green and responds to the local context of the site. The requirements are detailed in bold below followed by details of how the proposed development accords with each point

URBAN FORM

- The main streets must have as much continuous built frontage as possible, which can be achieved through the use of drive-unders and carriage arches – continuous street form and frontages have been achieved through a mix of car parking to the side of properties and front driveways on certain streets where appropriate.
- The use of front parking creates a continuous built frontage, with rear parking courts within these streets representing an inefficient use of space which breaks up the frontage.
- The streets should generally be laid out with footways on each side of the carriageway where houses are found – footways have been provided on each side of the carriageway throughout the development to provide strong pedestrian connectivity.

- Shared surface areas are appropriate for lower order streets and must be wide enough to comfortably accommodate vehicle movements and pedestrians and cyclists – shared services are provided on low order streets on the periphery of the development where vehicle, pedestrian and cyclist movements can be safely accommodated.
- Street trees and parking must be accommodated within a well planned streetscape – the street scene includes extensive tree planting along key frontages and carefully positioned car parking to a well-planned streetscape.
- Development form must follow the form of the roads, with houses aligned parallel to the street – houses have been carefully aligned parallel to the streets to create attractive continuous frontages.

BUILDING/PLOT FORM

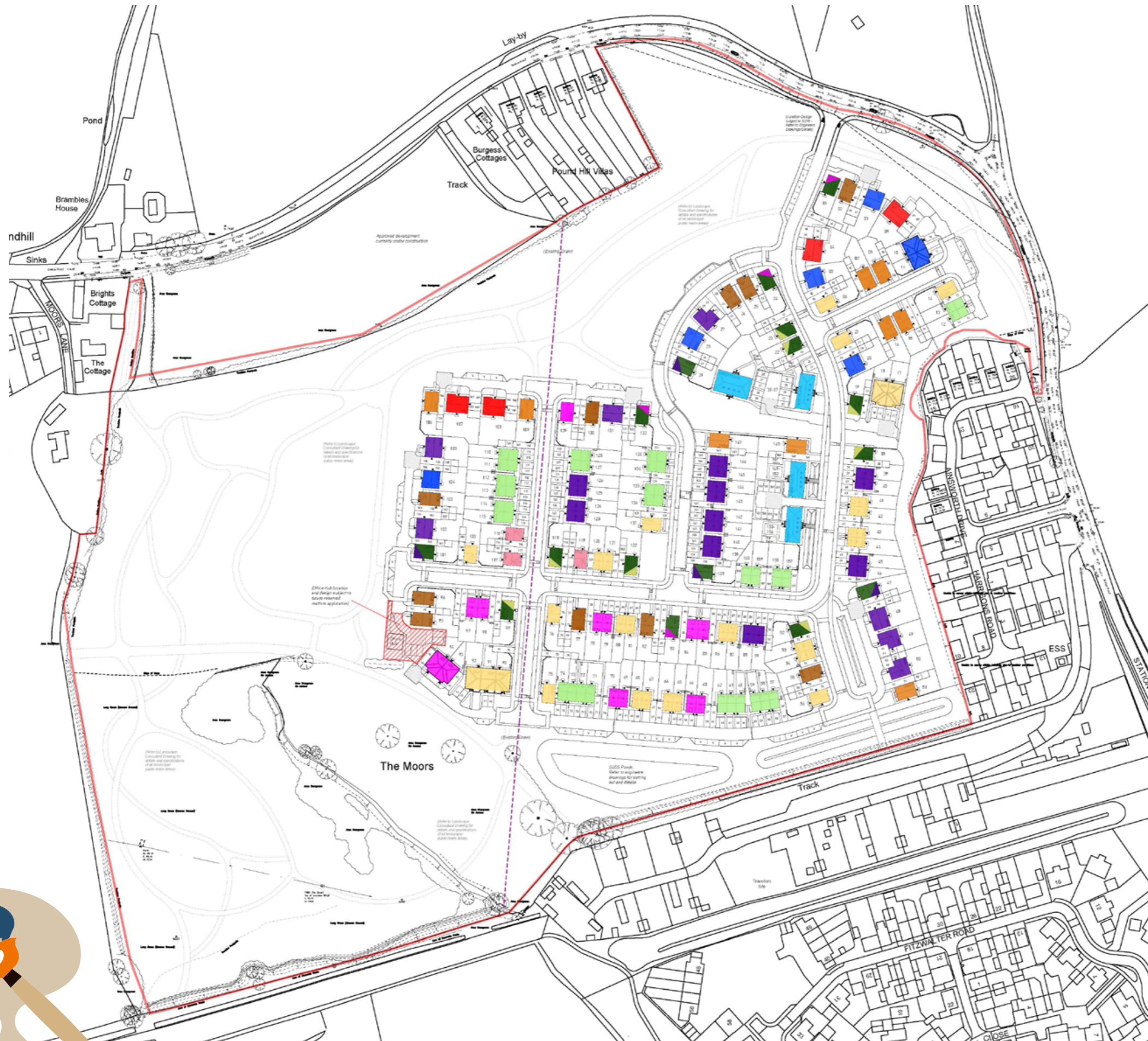
- The development must include a mix of terraced cottages, large detached and semi-detached and houses, in accordance with the character area descriptions – a wide mix of properties have been included within the design, from apartments and terraced properties to larger detached dwellings in accordance with requirements.
- Development must be a maximum of 2.5 storeys in height, with 2 storey dwellings the most common height – dwellings are 2 or 2.5 storeys in height to reflect the prevailing local character.
- Houses can be set on the back of the pavement or with varying sizes of front gardens as appropriate – varied set back distances have been incorporated to create a varied street scene.
- Plot forms should vary with both wide and narrow fronted plots found – varied plots sizes are provided for different property types including a range of frontage sizes.
- Rear garden sizes are typical of the age and density of the development. Appropriate garden spaces have been provided for each property, providing high quality private amenity space for residents. All garden sizes are policy

compliant with 1 and 2 bed types with a minimum of 50m² and 3 + bed houses at 100m².

- Dwellings should generally have eaves facing the streets, with some gable fronted elements introduced – eaves primarily face the street, with gables introduced at certain points to create an attractive and varied street scene.
- Overhanging gables at the first floor level are also appropriate – such measures have been incorporated at appropriate points in the development.

PARKING

- Appropriate parking standards have been provided for each property and is in line with design code standards. Parking standards are met with parking space size being 29m x 5.5m unless gable parking is provided on a linear drive which will be 3m wide. 1 bed dwellings will be provided 1 space and 2+ bed dwellings provided with 2 spaces.
- Visitor parking is provided at 25% and complies with the design code. 40 visitor spaces are proposed for 160 units which are accommodated within the parking bays shown on the layout along adoptable highways opposite POS areas.



KEY

-  Site Boundary (14.07 Ha)
-  Brick and Grey Roof
-  Brick and Red Roof
-  Brick and Brown Roof
-  Brick and Brown Roof With Feature Cladding
-  Render and Grey Roof
-  Render and Red Roof
-  Render and Red Roof With Feature Cladding
-  Render and Brown Roof
-  Render and Brown Roof With Feature Cladding
-  Brick and Brown Roof With Feature Cladding
-  Fully Boarded/Cladding Feature Plot with Brick Plinth and Grey Roof
-  Fully Boarded/Cladding Feature Plot with Brick Plinth and Brown Roof
-  Fully Boarded/Cladding Feature Plot with Brick Plinth and Red Roof

NOTE: Please refer to house type pack for details of material locations and specification within specific house types



ATTRACTIVE & DISTINCTIVE

MATERIALS PLAN

5.8 MATERIALS

APPEARANCE

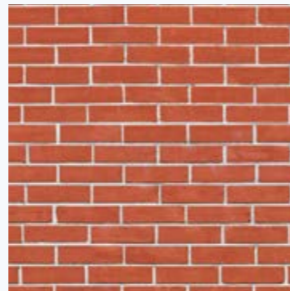
Following an assessment of Moorsfield the proposed dwellings have been designed to ensure the architectural response complements the local character, whilst creating a development with a sense of place where people will enjoy living. Across The Site, there are a mix of different house types creating a subtle variety that complements the existing town.

In order to complement the existing local vernacular, the different house types have been designed with a range of architectural detailing. This includes:

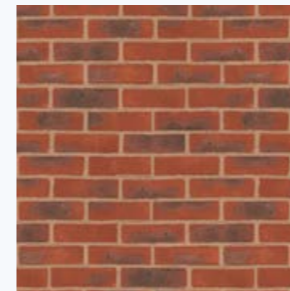
DETAILS AND MATERIALS

- Common walling materials should include red brick and smooth render (pastel colours)
- Some horizontal wooden cladding in black or white/cream is appropriate.
- Vertical brick detailing around windows and doors should be used, in the same colour or contrasting brick.
- Roofs should be a mix of red or brown tiles or grey slate.
- Dormer windows should have gable roofs.
- Chimneys should be set in line with walls or create a feature on the gable end of properties.
- Windows should be either timber painted or uPVC – white.
- Casement windows should be used.
- Doors should be coloured with glazed vision panels.
- Varied use of porches and canopies / threshold details should be used.
- Where properties are set back from the pavement, boundaries should be formed by a mix of brick walls, fences, hedges or planting within front gardens.

BRICK TYPES



Red Brick



Red Brick



Render

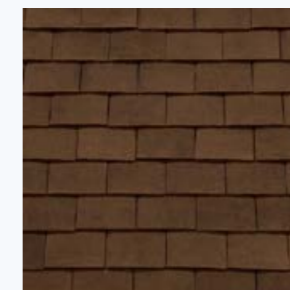
ROOF TILES



Grey Roof Tile



Red Roof Tile



Brown Roof Tile

DESIGN CODE KEY POINTS

1 CHARACTER

Landscape-led scheme.

2 ARCHITECTURE

Buildings of a traditional style using materials and boundary treatments that reflect the local vernacular.

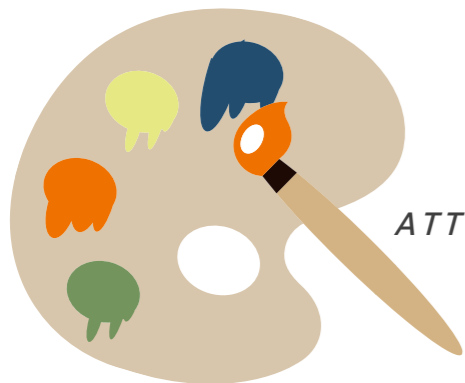
STREET SCENES



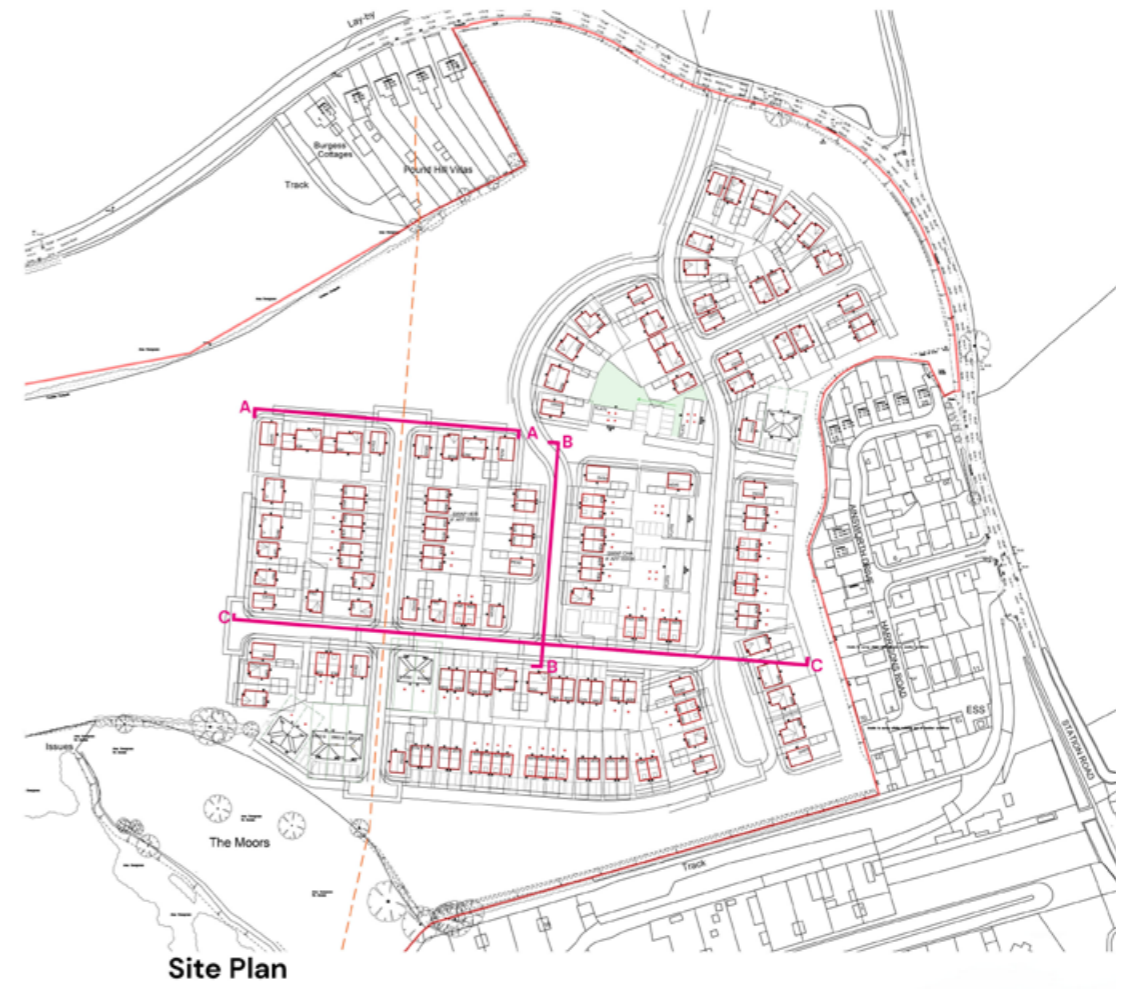
Street Scene A



Street Scene B



ATTRACTIVE & DISTINCTIVE



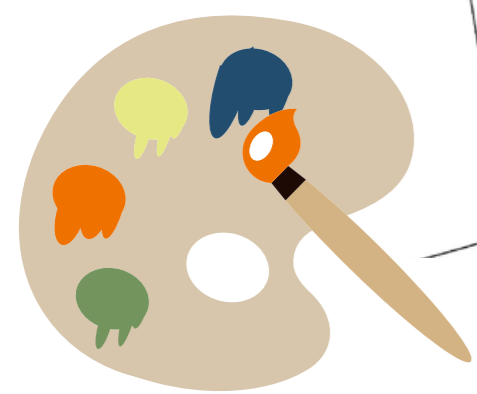
Site Plan



Street Scene C



- KEY**
- Site Boundary
 - 1.8m High Close Boarded Timber Fence
 - 1.8m High Brick Wall, Wienerberger, Ashington Red Multi or similar
 - Gated Access Point
 - Existing Boundaries to be retained and made good where required



ATTRACTIVE & DISTINCTIVE

BOUNDARY TREATMENTS PLAN

5.9 BOUNDARY TREATMENTS

Development plots will be defined by a range of boundary treatments such as fences, walls and hedgerows to create the distinction between public and private spaces. The adjacent plan demonstrates the varying types of boundary treatment proposed for the development, which should be read in conjunction with the Proposed Landscape Plan.

Rear gardens will be predominantly enclosed by 1.8m high fencing. Where exposed to the street frontage, brick walls will enclose rear gardens, to match the brick of the adjacent dwelling.

Private drives, the lowest category road, will be delineated from the wider landscape by the introduction of timber post and rail fencing with hedgerow.

The perimeter of The Site will be defined by a mix of new and existing hedgerows, additional planting and fencing where appropriate.



EXAMPLE BOUNDARY TREATMENTS TO BE USED WITHIN THE SITE

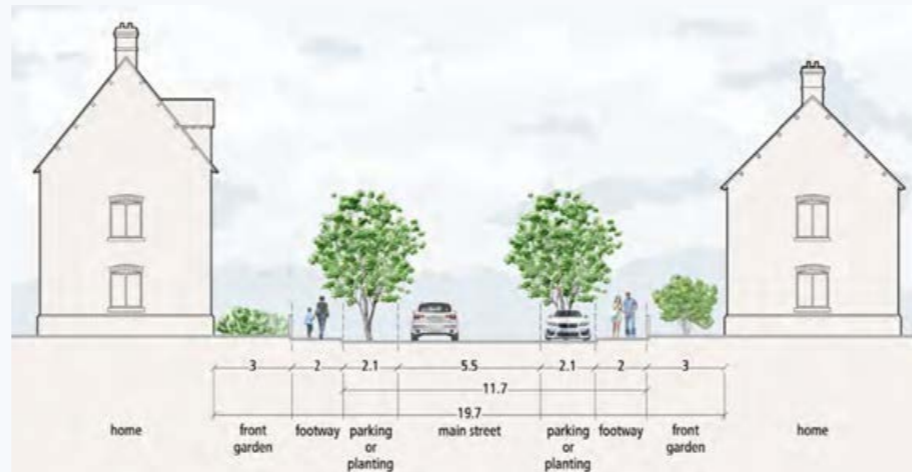
5.10 PUBLIC SPACES

The streets and green spaces are designed to form a network of places and open spaces, with distinctive character.

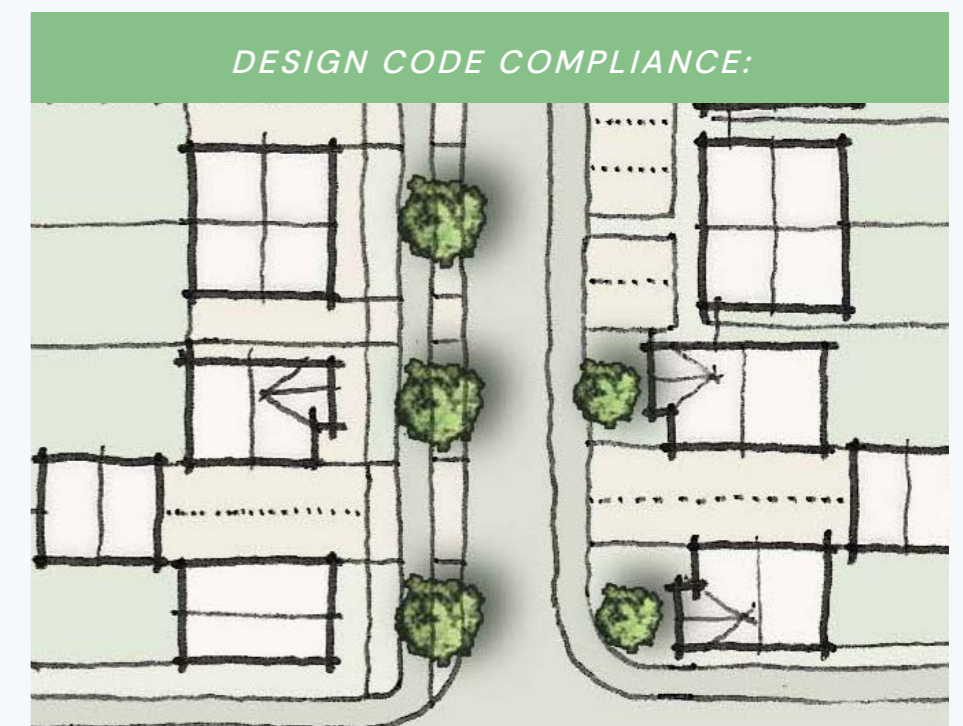
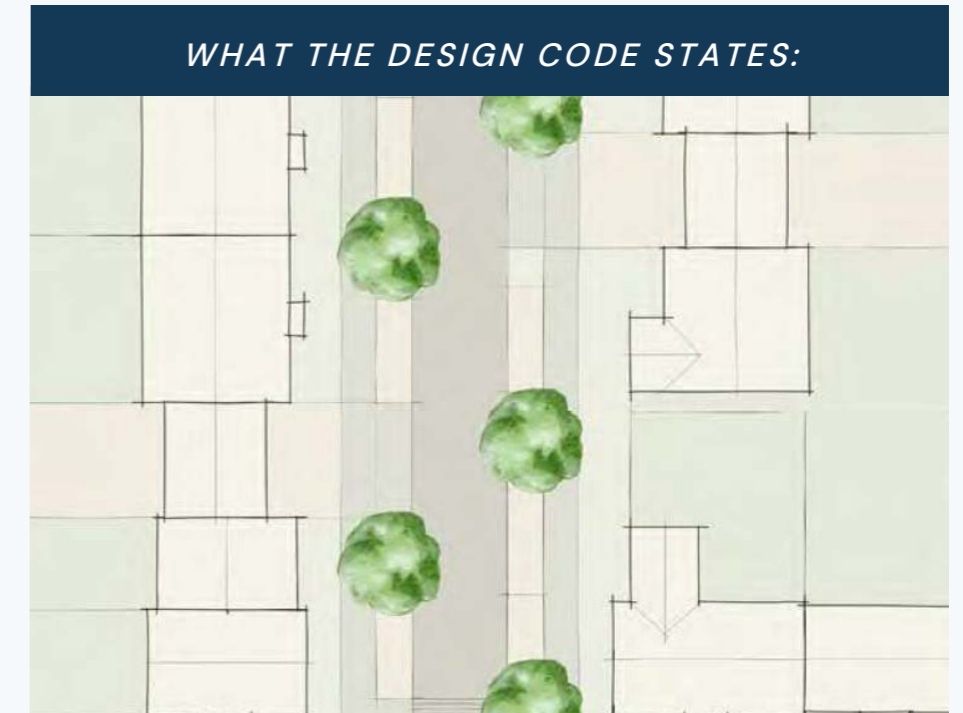
The design code sets out a hierarchy of streets to help people navigate around the site stating; "Streets should connect with each other in a logical manner to provide an appropriate movement network. Streets should be as straight as possible, whilst balancing placemaking and speed reduction measures." Legibility is central to the design proposal, which promotes the ease of navigation for both existing and future residents. The hierarchy of streets are based on the following street types: Main Street, Residential Street, Shared Surface / Private Drive.

MAIN STREET

- 5.5m carriageway with at least 2m wide footpaths on each side
- Footpaths could be separated from the carriageway by green spaces which should include swales in appropriate locations.
- Benches provided within green spaces.
- Parking to be provided to the side or rear of properties.
- On street visitor parking in marked parallel bays
- Trees provided in conjunction with parking bays or green spaces.
- Boundaries to front gardens formed of low wooden fences or brick walls.
- Set back distances should vary along this street type, with some areas having larger set back with parking and street trees and other areas having smaller set backs to give a sense of enclosure.



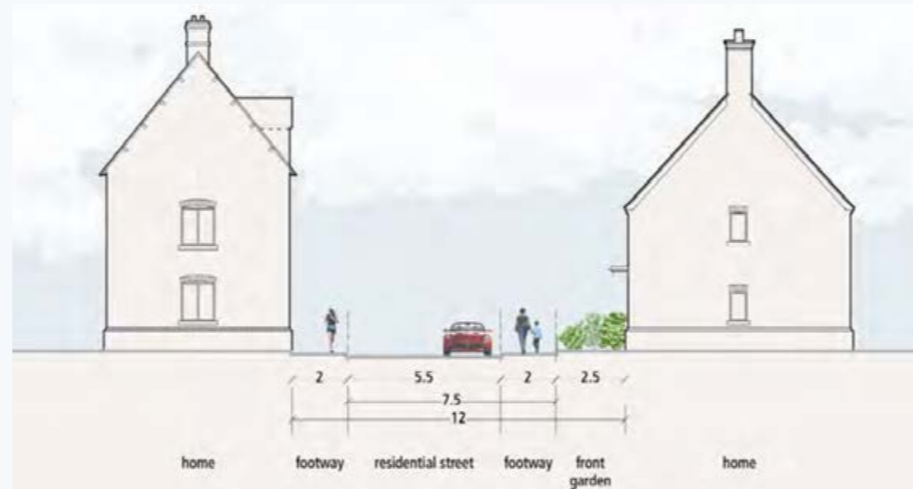
The above image is an extract from the proposed site layout showing a typical area of the Main Street within the scheme. As you can see the setting out complies with the details of this typology described within the Design Code; stepped building line, tree lined verges and hidden/gable parking, for example.



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RESIDENTIAL STREET

- 4.8 – 5.5m carriageway with 2m footpaths on at least one side;
- Footpaths could be separated from the carriageway by green spaces which should include swales in appropriate locations;
- Benches provided within green spaces;
- Parking to be provided to the side or rear of properties;
- On street visitor parking in marked parallel bays;
- Trees provided in conjunction with parking bays or green spaces;
- Boundaries to front gardens formed of low wooden fences or hedges/front garden planting;
- Set back distances should be smaller on this street type to give a sense of enclosure.



The above image is an extract from the proposed site layout showing a typical area of a Residential Street within the scheme. As you can see the setting out complies with the details of this typology described within the Design Code; Consistent building line with smaller set back distance and gable parking.

WHAT THE DESIGN CODE STATES:

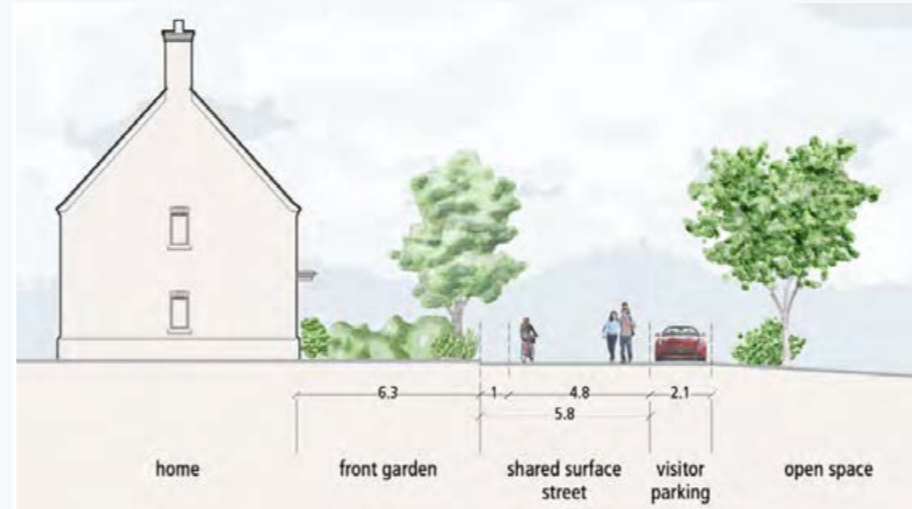


DESIGN CODE COMPLIANCE:

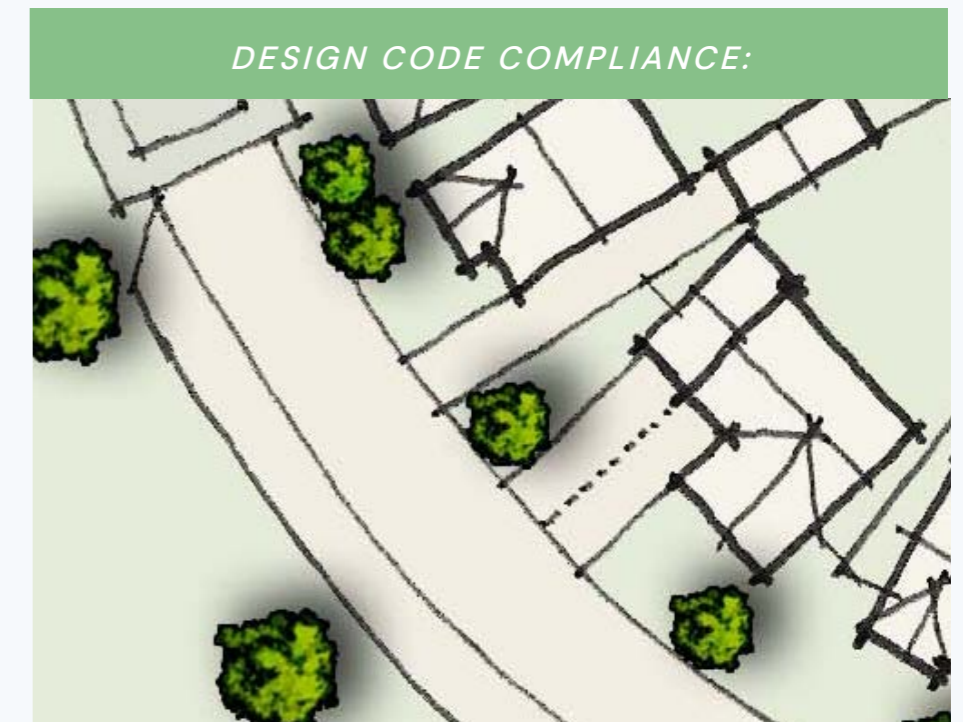
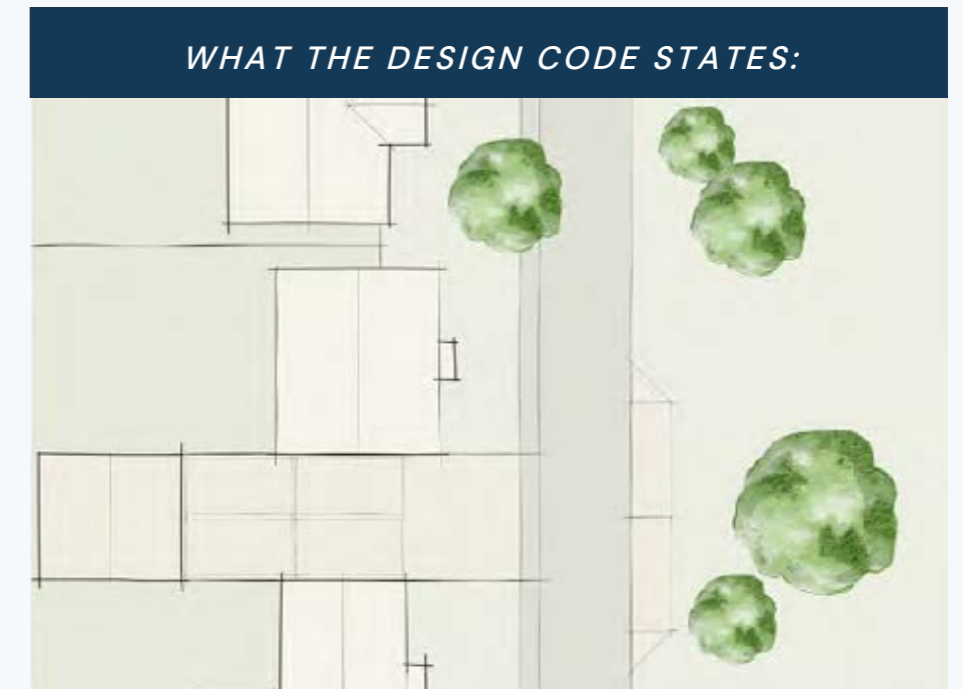


SHARED SURFACE / PRIVATE DRIVE

- 4.5 – 6m shared surface carriageway with no vertical delineation between vehicle and pedestrian areas;
- Parking to be provided to the side of properties;
- On street visitor parking in marked parallel bays;
- Trees provided in conjunction with parking bays or green spaces;
- Boundaries to front gardens formed of hedges or front garden planting;
- Set back distances should be varied on this street type, with larger set backs along the Landscape Edge of the site and smaller set backs within the Development Core and Neighbourhood Edge.



The above image is an extract from the proposed site layout showing a typical area of a shared surface within the scheme. As you can see the setting out complies with the details of this typology described within the Design Code; Varied building line but with large set backs to open space and gable parking.



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CREATING A SAFE PLACE TO LIVE

One of the design objectives of the National Planning Policy Framework (NPPF) states that developments should:

“...Create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users, and where crime and fear of crime, do not undermine the quality of life or community cohesion and resilience” (para. 130(f), NPPF 2021)

The design proposals for Moorsfield are based on an understanding of best practice guidance and reference has been made to the relevant documents including “Safer Places: The Planning System” and “Manual for Streets” as well as ACPO “New Homes” guidance.

Well-designed public lighting increases the opportunity for surveillance at night and will be integrated into future reserved matters applications.

Natural surveillance in the form of doors and windows overlooking streets, pedestrian routes and public open spaces will create activity throughout the day and evening and will be an essential element in creating a safe environment for all users, whilst discouraging criminal activity by increasing the risk of detection.

WELL DEFINED STREETS AND SPACES






- The road network within the development must have a 20mph design speed and create a strong framework of connected and well overlooked streets and spaces – the roadways have been designed for 20mph speed limits to create a safe environment for pedestrians and cyclists. Key intersections also include feature raised table junctions to reduce vehicle speeds.
- Perimeter blocks must be used to provide a clear separation between public and private spaces, with front doors overlooking the streets – the perimeter blocks overlook the streets adjacent to the public open space, providing a clear separation between these areas.
- Front gardens should be provided where appropriate to enliven the street scene and provide places for social interactions to take place – front gardens are provided where appropriate to enhance the street scene.
- Where gardens adjoin the street these must be separated from the street by 1.8m high walls, with gates to provide access for residents.
- Buildings on street corners must have windows on both elevations to provide overlooking to both streets. Bay windows could be appropriate here to provide additional interest to the side elevation – to confirm.



SECURED BY DESIGN: HOMES 2019



KEY

-  All Modes Access
-  Existing Pedestrian Access
-  Existing PRoW retained
-  Proposed pedestrian and cycles access
-  Proposed footpath

PEDESTRIAN MOVEMENT PLAN

5.11 PEDESTRIAN MOVEMENT

PEDESTRIAN MOVEMENT

- The main access points for pedestrians will be in the north east of the site, along Station Road, to provide surfaced routes to the nearby bus stops and services within Flitch Green.
- Additional pedestrian access will be provided from the Flitch Way, along the existing Public Right of Way which crosses the site, and in the north western corner of the site.
- From these access points convenient routes should be provided both through the development area and the open spaces towards access points on the opposite side of the site.
- The paths within the development area alongside the roads should be surfaced, as should the connections to the Flitch Way.
- Footpath connections within the open space should be more informal and could be mown grass paths or compacted gravel.

Extensive pedestrian access points have been provided throughout the residential areas and wider open space. The primary access point is provided off Station Road, with pavements provided as part of the access road for easy pedestrian access to nearby bus stops and services within Flitch Green.

Pedestrian access is also provided from the Flitch Way, with two connection points provided along the existing Public Right of Way and further access points to the corners of the site. A further pedestrian access point is proposed to the north west, connecting to the existing footpath network and providing connections to Little Dunmow.



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Extensive pedestrian routes are proposed throughout the development area and open spaces, enabling residents and the local community to enjoy the network of green spaces provided.

STREETS FOR ALL

The design code sets out points for creating Streets For All within the scheme, by following the 4 subheadings in the Building for Healthy Life guidance:

What the Design Code states:

- The green corridors within the development area must be designed as attractive routes which encourage walking, cycling, leisure and play.
- Raised table junctions must be included at key intersections to slow vehicle speeds.
- The development must be designed for 20mph speeds. As it will have less than 2000 vehicle movements per day then cycling on the carriageway is appropriate.
- Advice must be taken from appropriate groups regarding inclusive design, to ensure people with visual, mobility and other limitation will be able to use the street confidently and safely.

The Layout demonstrates a network of streets and spaces that create a rich street scene that is complimented by street trees, active frontages, continuous and stepped building lines, front gardens and a variety of materials that compliment the surrounding area.

The proposed schemes street hierarchy largely aligns with the plan contained within the Design Code.

The vitality and safety of the streets is key to the design proposal. The site benefits from the safe, pedestrian friendly route network that is proposed as part of the wider scheme. The roadways have been designed for 20mph speed limits to create a safe environment for pedestrians and cyclists. Key intersections also include feature raised table junctions to reduce vehicle speeds.

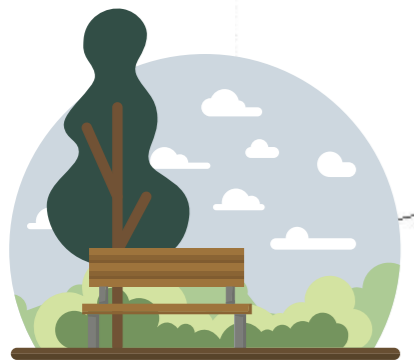
The detailed layout highlights how green corridors have been designed within the development as attractive routes which encourage walking, cycling, leisure and play.

A well-connected movement network, accessible by all users, is proposed which helps to ensure that all areas of the development will be accessible, easy to navigate, safe and secure to ensure people with visual, mobility and other limitation will be able to use the streets.

The introduction of traffic calming measures such as raised table junctions are to be included at key intersections to slow vehicle speeds and increase safety.



- KEY**
-  Public Open Space
 -  Existing Moor and Woodland
 -  Proposed Community Orchard
 -  Attenuation Pond
 -  Landscape Buffer
 -  Proposed Street Trees



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5.12 GREEN AND BLUE INFRASTRUCTURE

The delivery of well-designed accessible and inclusive public spaces and green infrastructure will offer residents spaces to socialise and engage with each other, encouraging interaction and opportunities to benefit from healthy lifestyle choices.

The design code sets out in great detail the potential for Green and Blue infrastructure within the layout as part of the generous provision afforded within this scheme.

The design code notes how the proposals deliver a generous landscape framework that helps assimilate the development into the surrounding countryside, whilst providing an accessible and high-quality open space for all. These are shown in the landscape plan, however key features of the green and blue infrastructure are included below.

LANDSCAPE STRATEGY

The open space across the site has been designed to benefit the whole community with areas of ecological value and the proposed development has worked to retain existing landscape and vegetation features within the proposed public open space network. A landscape led approach has been adopted through the provision of a significant area of Countryside Park in the south western section of the site, while green corridors throughout the layout, along with strategically located tree and hedge planting, supplement the existing Moors Woodland and other existing natural features.

The Moors woodland has been well integrated into the public open space and only where necessary have shrubs and

woodland been removed to reopen the PROW and allow natural surveillance.

Soft landscaping within the countryside park will include habitats such as grasslands (featuring wild-flower) and large vegetative buffers to the boundaries providing a permeable site for wildlife. SUDS ponds and swales provide 'blue' fingers within the layout linking to extensive 'green' corridors throughout the site, providing a wide range of natural ecological habitats, which will also be supplemented by additional features such as bird and bat boxes. These habitat enhancements have been informed by the ecological assessment work provided as part of the outline application.

The design code contains a moderately detailed outline masterplan for the landscape strategy, providing a good framework for future details.

The landscape plan opposite shows a number of different Landscape character areas that will be created within the site, including:

Play Area - A natural play area with soft boundaries will be provided within the countryside park, located close to the development edge for easy accessibility and natural surveillance. Where possible, further informal play opportunities will be encouraged along key routes in the form of naturalistic play such as logs or boulders.

Allotments and Community Orchard - semi enclosed area for allotments and a community orchard. Located close to the

development edge and along a key route between Fitch Green and Little Dunmow.

Buffers to Ainsworth Drive - Where the proposed houses adjoin the existing houses along Ainsworth Drive an 8m wide landscape buffer must be provided. This must be fenced at each end with a high fence so it is not publicly accessible. Gates must be provided so the space can be maintained as part of the management regime for the public open space.

COUNTRYSIDE PARK

The layout complies with the design code through the delivery of around 8.4 ha of public open space; primarily in the form of a countryside park, but also through green links within the development area.

Proposed landscape treatments will maintain distinction between surrounding settlements.

Extensive green space will provide potential for leisure, children's play, and recreation.

Walking and cycling connections will be made to the Flich Way, which runs along the south of the site.

Informed by the LVIA, the country park seeks to retain the physical and visual separation between Flich Green and Little Dunmow and to minimise the effect on the visual amenity of views from the surrounding landscape.

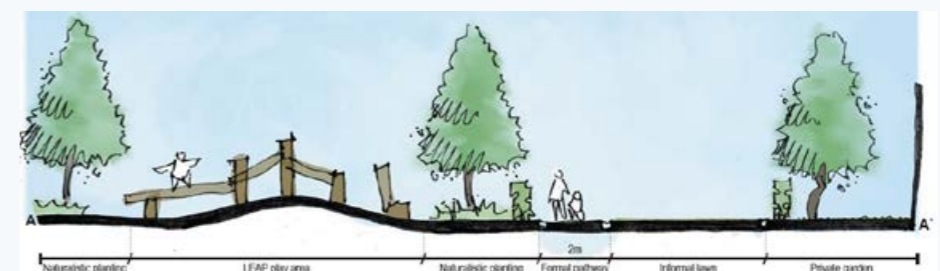
It provides access to natural space for residents, while containing nature walks, play-on-the-way, cycle trails, and preserve public rights of way.

The design of countryside park has been focused on the benefits to both people and wildlife. It includes a large blanket of species-rich grassland containing pockets of scrub planting and specimen trees and is surrounded by proposed woodland to the north and existing woodland to the south.

A network of existing and proposed footpaths cross the site that will accommodate recreational walking routes as well as reinforce connections between Flich Green and Little Dunmow. The mixture of landscape typologies will provide a network of diverse habitats that are connected to those across the wider landscape.



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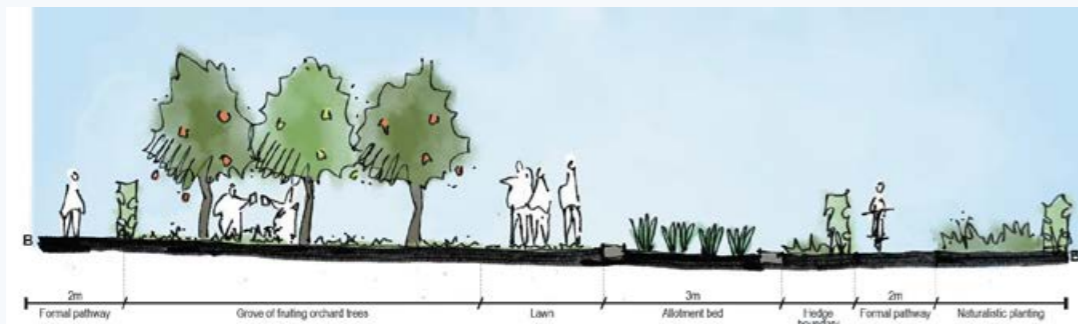
SKETCH OF COUNTRYSIDE PARK

COMMUNITY ORCHARD

Located close to the development edge and along a key route between Flich Green and Little Dunmow, this will be an accessible and social space for all.

The communal and shared orchard would provide a space for planned or informal social interaction and would include;

- Growing produce offers an opportunity for education.
- Fruit-bearing trees, as well as spaces for allotments, could provide food for residents.
- Opportunity to grow local or heritage fruit varieties.



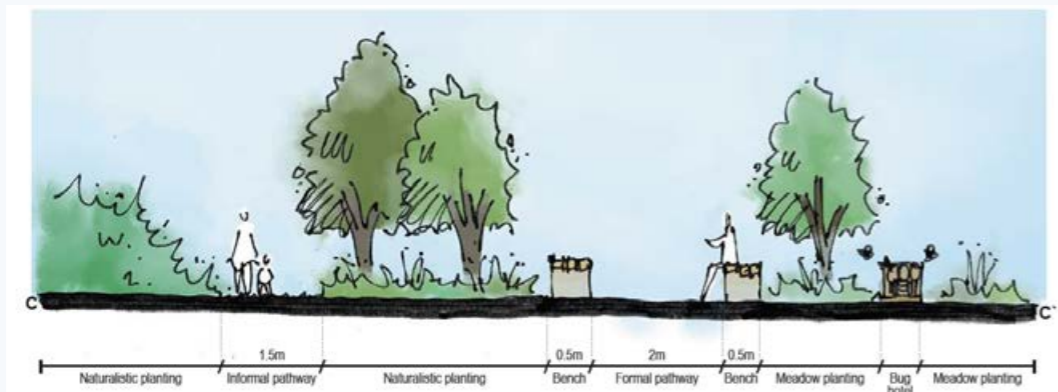
SKETCH OF COMMUNITY ORCHARD



WILDLIFE AREA

Set behind the moors woodland in a more secluded location, the wildlife area will preserve and enhance existing ecological assets, including the existing grasslands. This includes:


- Planting of species-rich wildflower meadow, providing habitats and food sources for birds and mammals, as well as pollinator species such as butterflies.
- Preserving, developing, and supporting diverse ecosystems.



SKETCH OF WILDLIFE AREA



LEGEND

-  POTENTIAL ECOLOGICAL ENHANCEMENT

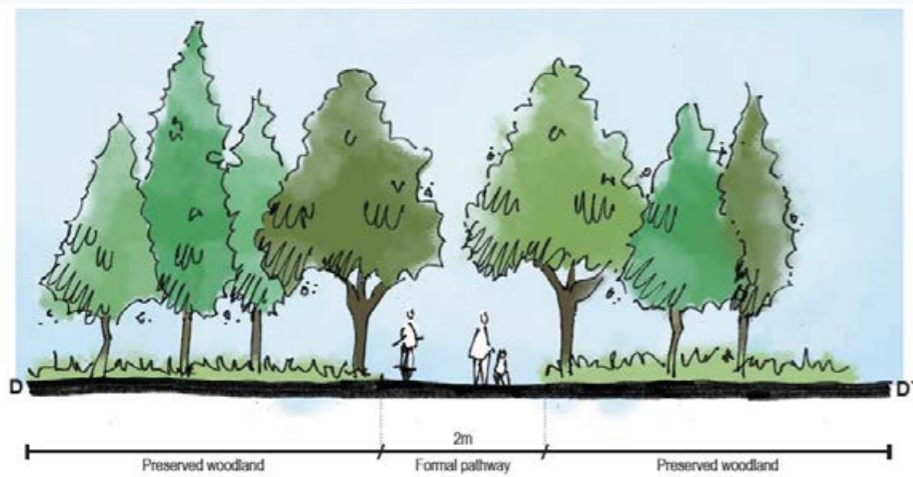


SAFE, SOCIAL & INCLUSIVE

MOORS WOODLAND

The existing wooded area is to be preserved. A dense vegetated buffer between the Flich Way Country Park and the development is proposed, as well as between the development and the enhanced wildlife area.

Clearing of overgrown or damaged pathways to offer pedestrian, cycle, and equestrian network connections will be undertaken to provide access to varied natural spaces for residents.



SKETCH OF MOORS WOODLAND



INFORMAL PLAY ON THE WAY

Provision of naturalistic, informal play provision within the country park and along the nature trails therein is proposed including;

- Designed orchard area with provision for community fruit growing, as well as allotment space
- Inclusion of a 'trim trail' within the country park, providing an active resource for children's play as well as exercise and

DEDICATED PLAY SPACE

A natural play area with soft boundaries is proposed within the countryside park, located close to the development edge for easy accessibility and natural surveillance. Where possible, further informal play opportunities will be encouraged along key routes in the form of naturalistic play such as logs or boulders.

- Provision of both a Local Area for Play (LAP) and a Local Equipment Area for Play (LEAP) in addition to play facilities within the green corridors running through the development
- Provision of naturalistic, informal play provision within the country park and along the nature trails therein.



ENHANCED & OPTIMISED

5.13 NATURE



GREEN AND BLUE CORRIDORS

The Moors Fields site is located entirely within Flood Zone 1, with a low probability of fluvial flooding. However, since the proposed development works will increase the impermeable coverage on a greenfield site it is necessary to ensure a sustainable approach is employed to regulate surface water runoff to manageable rates. Sustainable water management measures include:

- The provision of greenspace to reduce the volume of water which needs to be managed on site and as such maximising the green space provision.
- The installation of permeable pavement to collect surface water directly at the source.
- Provision of swales and ponds can provide surface water management whilst contributing positively to the amenity and ecology.

The southern pond will be natural in form, with no demarcation from the surrounding open space, shallow sloping grass sides and wetland planting as appropriate.

Design Code compliance:

A multi-functional green and blue infrastructure network is identified as central to the design process. The scheme provides a generous multi-functional landscape network that helps integrate the development into the surrounding countryside, whilst providing an accessible and high quality open space for all. A summary of each element provided is provided below

GREEN SPACE

The natural setting within the Fritch Way Country Park provides the opportunity to create large green corridors. Furthermore, the green corridors will feature extensive native and non-native planting to help achieve biodiversity net gain. The diversity that the SuDS pond and additional swales will add to the existing ecosystem will also help to achieve this.

The wildlife area will provide a mixture of habitats such as wild flower meadows and woodland planting to be incorporated throughout the open space to boost ecological benefits.

The orchard creates further amenity value through a semi enclosed area for allotments and community engagement. This is located close to the development edge and along a key route between Fritch Green and Little Dunmow, and will be an accessible and social space for all.

The Moors are an existing area of grassland that will be retained and enhanced through ongoing management. More details are provided in the Landscape section of this document.

BLUE CORRIDORS

Drainage attenuation will be located at the site's lowest point in the form of a linear drainage basin. The development will be able to regulate surface runoff to a manageable rate.

Extensive green spaces with naturalistic planting are provided to reduce the increase in surface runoff caused by hardscape by intercepting rainfall and allowing water to infiltrate into the soil. Provision of swales contributes to both water management and local ecology.

The proposed naturalistic landform and planting of swales and the southern drainage basin provide opportunities to interact with nature and be close to water, while also managing water in a sustainable and ecologically beneficial manner.

Permeable payment will also be utilised throughout the residential area to collect surface water.

5.14 SUSTAINABILITY

This section sets out key sustainable design measures incorporated into the detailed design to deliver low carbon homes, whilst also protecting and enhancing the environment, incorporating measures to adapt to and mitigate the effects of climate change.

The outline application as supported by a Sustainability Statement demonstrating how the proposed development responds positively to national and local sustainable policy requirements.

The NPPF states at paragraph 8 that the planning system has three interdependent and overarching objectives:

- An economic objective – to build a strong, responsive and competitive economy;
- A social objective – to support strong, vibrant and healthy communities; and
- An environmental objective – protecting and enhancing the natural, built and historic environment.

To achieve a sustainable development, that reduces reliance on natural resources and offers a long-term solution for the area the development proposals have been designed with these three key objectives in mind.

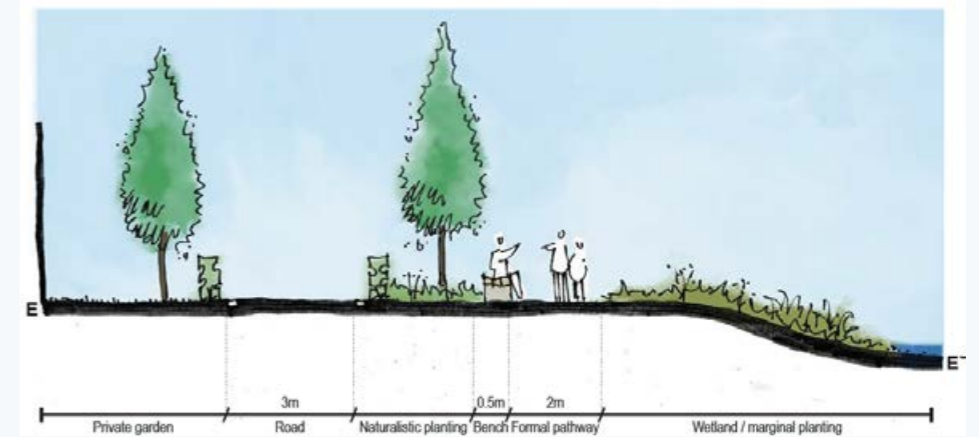
At a very high level, the objective of sustainable development can be summarised as meeting the needs of the present without compromising the ability of future generations to meet their own needs. The presumption in favour of sustainable development is at the heart of the planning system, as set out in paragraph 11 of the NPPF, and within the Local Development Plan.



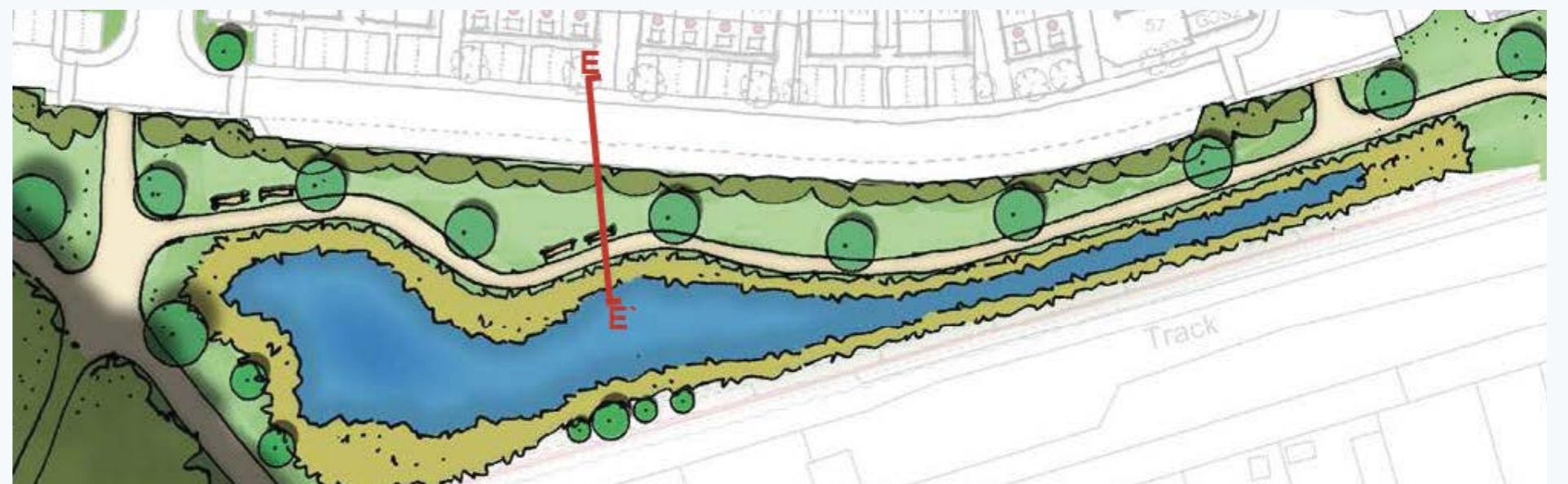
EFFICIENT & RESILIENT

SUSTAINABLE DRAINAGE (SUDS)

The integration of a comprehensive Sustainable Drainage System (SuDs) has been considered from the outset and shaped the masterplan development. The aim of SuDs is to maximise the existing potential of the site to attenuate and clean water, while providing valuable amenity by creating and integrating well designed landscaped features and promoting a greater diversity of flora and fauna. SuDs manage surface water run-off rates by mimicking natural drainage characteristics to achieve a sustainable drainage solution that balances water quality, water quantity, amenity and biodiversity.



ATTENUATION BASIN SECTION



SKETCH OF ATTENUATION BASIN



SOCIAL

Support strong, vibrant and healthy communities.



ENVIRONMENTAL

Protecting and enhancing the natural, built and historic environment.



ECONOMIC

Build a strong, responsive and competitive economy.

SOCIAL AND ECONOMIC BENEFITS

The development aims to provide a range of social and economic benefits to both new and existing residents, through:

- Provision of 160 new homes, including 40% affordable homes providing opportunities for local people.
- Creation of a low carbon local office hub providing a workspace for local residents or small start-up companies designed to incorporate low carbon renewable energy systems.
- Homes which will be designed to create healthy and sustainable living environments which are flexible for the future.
- The inclusion of extensive Green Infrastructure, including a new countryside park, providing local recreational opportunities for new and existing residents.
- Design in accordance with the Building for a Healthy Life Guidance to enhance the wellbeing of residents.

MITIGATING CLIMATE CHANGE













The development will incorporate a range of measures to reduce carbon emissions, mitigating the effects of climate change, and adaptation measures to ensure the long term resilience of the development to the effects of climate change. Measures include:

- Buildings designed to reduce carbon emissions, delivering at least a 75% reduction in carbon emissions beyond Part L 2013 through a range of fabric, energy efficiency and low carbon renewable energy measures.
- Orientation and design of homes to allow the installation of Solar PV.
- Specification of water efficient fittings to reduce water consumption to 110 litres per person per day in line with the government's higher water efficiency standard.
- Development of new homes in Flood Zone 1 and provision of a surface water drainage system designed to mitigate a 1 in 100 storm event, including a 40% allowance for climate change.
- Homes designed to take into account increasing annual temperatures set out in the UKCP18 climate projections to minimise the risk of overheating.

6

CONCLUSION

6.1	BUILDING FOR LIFE	89
6.2	CONCLUSION	89

INTEGRATING INTO THE NEIGHBOURHOOD	SCORE
Natural Connections – Improved and more clearly defined pedestrian and cycle links are now proposed to increase the overall permeability of the area by linking in to the existing footpaths and forming part of a wider network which provides strong north/south and east/west links.	
Walking, cycling and public transport – A large proportion of future residents be within easy walking distance of existing bus stops and routes on Station Road	
Facilities and services – This development is aimed at supporting the existing services and Facilities in Little Dunmow and Fitch Green and providing good pedestrian and cycle linkages to ensure the existing facilities are accessible.	
Homes for everyone – The site is identified in the Council’s emerging Core Strategy as a preferred location to meet housing needs. The scheme will also include a range of tenures with 40% of affordable housing provision.	
DISTINCTIVE PLACES	
Making the most of what’s there – The proposed development is designed to connect and fit in with houses to the south and west of the development. There are no existing buildings on site and the development will, where possible, retain existing boundary vegetation and internal hedgerows that will be supplemented by new planting as identified on the Landscape Masterplan.	
A memorable character – Details of the landscape-led character, with buildings of a traditional style using materials and boundary treatments that reflect the local vernacular, have been established within this document.	
Well defined streets and spaces – A clear street hierarchy has been established. The central spine road is the primary route through the development and will help create a sense of arrival when entering the site whilst clearly demonstrating that this is the principle link through the site from which smaller, more informal streets emanate.	
Easy to find your way around – Landmark buildings, of up to 2.5 storeys in height and/or including specialist architectural detailing, will be provided at the focal points of key vistas throughout the development, notably along the principle route.	
STREETS FOR ALL	
Healthy streets – A series of green links will be created to form a connected and permeable pedestrian network that will connect to the existing wider strategic movement network. The proposed road network has been designed in order to keep vehicular speeds to a minimum. The combination of horizontal and vertical deflections at regular intervals will help demonstrate that vehicular speeds should be kept to a minimum and in turn create a safer pedestrian and cycle network.	
Cycle and car parking – The majority of parking will be located on plot and will be set back from the primary road frontages with limited on street parking.	
Green and Blue Infrastructure – A landscape led approach has been adopted through the provision of a significant areas of Green and Blue Infrastructure through the creation of a countryside park, wildlife area, Woodland and formal and informal play spaces.	
Back of pavement front of home – All public spaces and pedestrian routes have been designed so that buildings front onto and overlook them in the interests of providing passive surveillance.	

6 CONCLUSION

6.1 BUILDING FOR A HEALTHY LIFE

Building for a Healthy Life questions are designed to help planning authorities, Urban Development Corporations, and home builders create a structured and focused design dialogue. BHL (2020) can also help identify what issues really matter to local communities and how development proposals can respond positively to these. BHL uses a simple traffic light system whereby a 'red' indicator suggests that one or more aspects of the design need to be considered; 'ambers' indicate a need for further discussion or refinement. 'Green' indicators suggest one or more aspects of the scheme have been well considered and resolved.

The table opposite represents the applicants Building for a Healthy Life Assessment of proposals. Following the detailed design of the reserved matters application, the proposals secured 12 out of the possible 12 'greens'.

6.2 CONCLUSION

The Design and Access Statement (DAS) is submitted in support of the Reserved Matters submission for residential development at Moors Field for 150 residential dwellings with associated office hub and open space.

This Design and Access Statement has outlined how the proposed development is a suitable response to the approved design code and other relevant national and local planning policy, demonstrating how the vision for Moors Field can be delivered to meet the Building for Healthy Life criteria and respond to climate change. This statement has demonstrated that the proposals have been underpinned by good urban design principles and integrate into the site's surroundings, responding to the local context to deliver high quality and ultimately beautiful development in this part of Flitch Green.

The assessment has been undertaken with reference to the approved detailed planning layout and demonstrates that the following has been accommodated on the site:

- Deliver 160 new homes, in a range of types and sizes to meet local policy requirements and identified need. This includes 40% affordable housing provision.
- Over 60% of the site would form generous public open space, laid out as a countryside park and include community allotments and orchard space.
- Significant new tree planting both around the public open space and throughout the development which strengthen the recreational role of the Moors as a woodland resource.
- Circular walking and cycling trails connecting to the existing Public Right of Way and Flitch Way.
- The provision of new children's play areas.
- Creation of a new office hub to provide local workspace.
- Improved pedestrian and cycle connections to Flitch Green.

The scheme responds to the landscape and heritage influences of the site and surroundings, with a large public open space along the northern and western boundaries, which will create a defensible edge to the settlement and preserve the gap to Little Dunmow in perpetuity. The scheme includes an equipped play area within a connected green infrastructure network.

The proposals represent a sustainable development with access to the facilities available within Flitch Green and the wider area. The homes will be constructed to minimise their carbon consumption through the use of efficient features and sustainable materials (amongst other measures) which will help the Council towards its carbon reduction targets.

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