

# **High Speed Rail (Crewe – Manchester)**

## **Supplementary Environmental Statement 2 and Additional Provision 2 Environmental Statement**

### **Volume 5: Map Book**

Planning Data / Committed Developments (CT-13)

# **High Speed Rail (Crewe – Manchester)**

## **Supplementary Environmental Statement 2 and Additional Provision 2 Environmental Statement**

### **Volume 5: Map Book**

Planning Data / Committed Developments (CT-13)



Department  
for Transport

High Speed Two (HS2) Limited has been tasked by the Department for Transport (DfT) with managing the delivery of a new national high speed rail network. It is a non-departmental public body wholly owned by the DfT.

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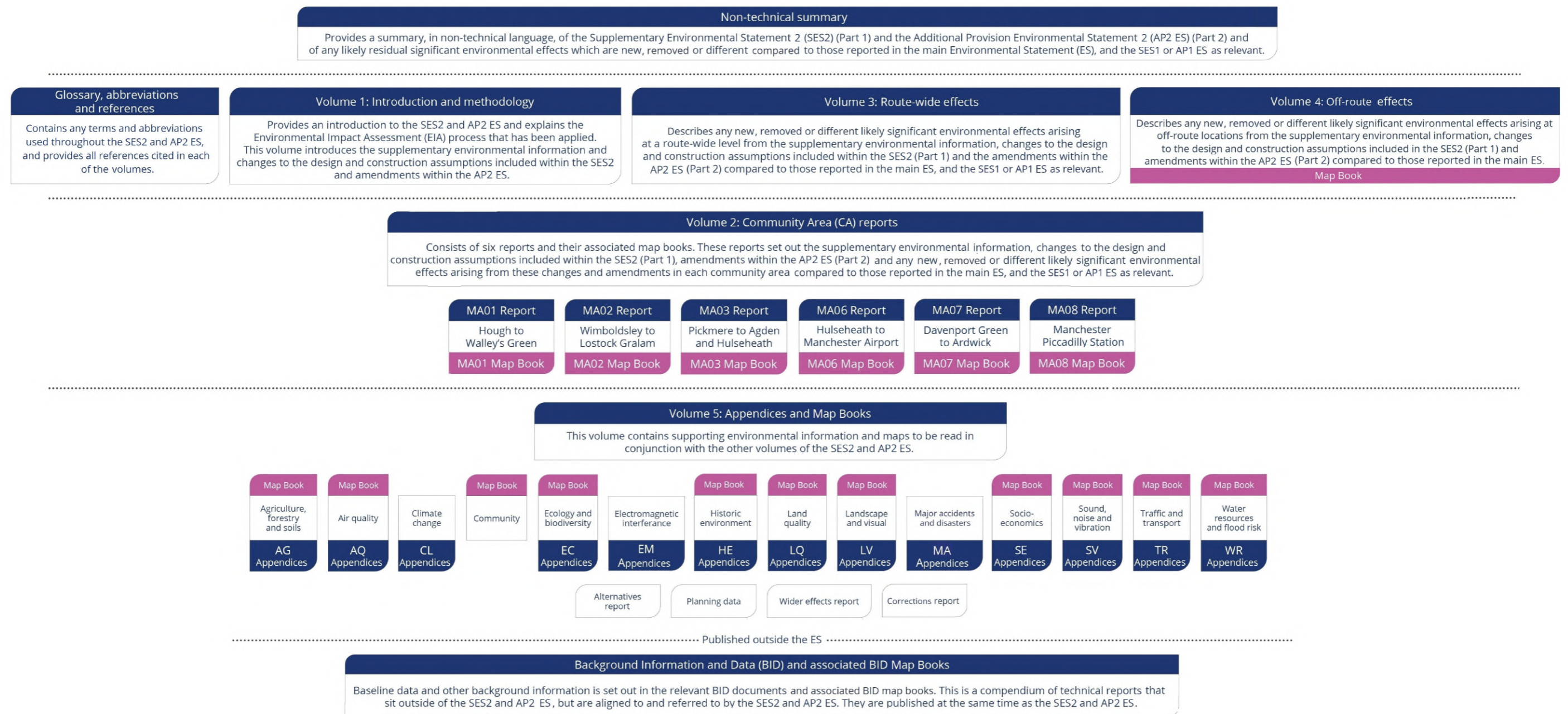
Data dictionary and definitions

Map series name	CT-13 - Committed Developments
<b>Map series description</b>	CT-13 series shows the geographical distribution of as yet unimplemented but committed development which is either considered to be largely built out prior to commencement of the Proposed Scheme (future baseline) or those projects that are assumed to largely take place during implementation of the Proposed Scheme (cumulative development). These maps show those committed developments that have been taken into account in the assessment of environmental effects.
<b>Community Area name</b>	
MA01 Hough to Walley's Green	✓
MA02 Wimboldsley to Lostock Gralam	✓
MA03 Pickmere to Agden and Hulseheath	✓
MA06 Hulseheath to Manchester Airport	✓
MA07 Davenport Green to Ardwick	✓
MA08 Manchester Piccadilly Station	✓
ORW1 Preston Station (PSTN)	
ORW2 Carlisle Station (CSTN)	
ORW3 Annandale Depot (ADEP)	✓

# Mapping explanatory notes

## Structure of the Supplementary Environmental Statement 2 and the Additional Provision 2 Environmental Statement

This map book is part of the suite of documents that make up the Supplementary Environmental Statement 2 (SES2) and the Additional Provision 2 Environmental Statement (AP2 ES) for the High Speed Rail (Crewe – Manchester) scheme. The SES2 and the AP2 ES are separate documents; however, they are bound together and presented in a number of volumes shown in Figure 1.



## Copyright statements

Copyright statements are presented in the data dictionary and definitions section at the front of the map book, due to limited space to include this on the individual maps themselves.

## Ordnance Survey data

All maps produced as part of the SES2 and AP2 ES contain Ordnance Survey (OS) data. HS2 Ltd use the most up to date mapping available, where possible, supplied by the OS and as such, we cannot be held responsible for any inaccuracies within this data. As part of our licence conditions, all digital maps carry a watermark.

## Chainage

Most of the maps presented as part of the SES2 and AP2 ES have a chainage value shown next to the alignment. Chainage is presented on the maps in black font, in the form of XXX+YYY, e.g. 192+000 or 239+500.

Chainage (known as reference chainage) is referenced from Euston Station, which is 0+000, and the value presented is in metres, e.g. 192+000 refers to the point, 192,000m, or 192km, from Euston Station. Chainage values increase in intervals dependent on the map scale. For maps at 1:50,000 scale chainage is shown at 5km intervals. For maps at 1:25,000 scale chainage is shown at 2km intervals. For maps at 1:20,000, 1:10,000, 1:5,000 and 1:2,500 scales, chainage is shown at 1km intervals.

Chainage has been included on the maps as a useful tool for comparing different map sets showing the different environmental themes or engineering plans, due to map sets having different scales, and therefore, showing differing amounts of alignment on the map.

## Map sheet layout

Each map in Volume 2 is presented twice, with the main ES map (In Parliament - Session 2021 - 2022) on the left-hand page and the SES2 and the AP2 ES map on the right. For the CT-05 and CT-06 map series, the SES2 and the AP2 ES is shown with different coloured hatching, annotated with labels, which highlights the areas of change. The coloured text box provides a brief description of the design change and amendment, and gives the SES2 and the AP2 ES reference number. In some instances where the effect covers a large area, a box without hatching but with a label, is shown. Only maps which have been amended as a result of the SES2 and the AP2 ES are included within the map books. Other design changes which are within the existing limits of the Bill and do not result in new or different likely significant effects are also shown on the maps, but are not indicated by hatching.

Each Volume 5 map is annotated to describe the change to a receptor or significant effect, and to give the SES2 and the AP2 ES reference number. For more detailed information about the SES2 and the AP2 ES annotation, refer to the map series legend. Only maps which have been amended as a result of the SES2 and the AP2 ES are included within the map book. Changes to receptors or significant effects relating to SES1 or AP1 ES are also provided for reference. Where this is the case they are shown in a faded box.

Where a significant effect has been changed due to a correction to mapping produced in the main ES or previous SES/AP, the significant effect reference is suffixed with a double asterisk (\*\*). The map now shows the correct effect.

## Map orientation

The majority of the maps presented in these map books are presented with the railway alignment running horizontally across the page. The direction of travel to London would be following the alignment to the left hand side of the page, and Manchester to the right.

The exception to this, are map series LV-00, LV-02, LV-07 and LV-08, which present the alignment running from bottom to top of the page. This is to allow more of the modelled outputs to be shown at the appropriate map scale. In this instance, the direction of travel to London would be to the bottom of the page, and Manchester to the top.

Further details on the approach to mapping is set out in Section 6 of Volume 1 of the SES2 and AP2 ES.

## Map books

In total, there are 19 map books included in the SES2 and AP2 ES Volume 2, Volume 4 and Volume 5. A list of the map book titles and map series is provided below for reference.

Map book title (map series)	Map book title (map series)
Volume 2: Map Book – MA01: Hough to Walley’s Green (CT-05, CT-06, CT-10, LV-03, SV-05)	Volume 5: Map Book – Planning Data / Committed Developments (CT-13)
Volume 2: Map Book – MA02: Wimboldsley to Lostock Gralam (CT-05, CT-06, CT-10, LV-03, SV-05)	Volume 5: Map Book – Ecology and biodiversity (EC-01)
Volume 2: Map Book – MA03: Pickmere to Agden and Hulseheath (CT-05, CT-06, CT-10, LV-03, LV-04, SV-05)	Volume 5: Map Book – Historic environment (HE-01, HE-02)
Volume 2: Map Book – MA06: Hulseheath to Manchester Airport (CT-05, CT-06, CT-10, LV-01, LV-03, LV-04, SV-05)	Volume 5: Map Book – Land quality (LQ-01)
Volume 2: Map Book – MA07: Davenport Green to Ardwick (CT-05, CT-06, CT-10, LV-01, LV-03, LV-04)	Volume 5: Map Book – Landscape and visual (LV-00, LV-02, LV-07, LV-08, LV-17)
Volume 2: Map Book – MA08: Manchester Piccadilly Station (CT-05, CT-06, CT-10, LV-03, LV-04, SV-05)	Volume 5: Map Book – Socio-economics (SE-01)
Volume 4: Off-route effects map book	Volume 5: Map Book – Sound, noise and vibration (SV-02, SV-03, SV-08, SV-09)
Volume 5: Map Book – Agriculture, forestry and soils (AG-01, AG-04)	Volume 5: Map Book – Traffic and transport (TR-01, TR-03, TR-04, TR-08)
Volume 5: Map Book – Air quality (AQ-01)	Volume 5: Map Book – Water resources and flood risk (WR-01, WR-03, WR-05, WR-06)
Volume 5: Map Book – Community (CM-01)	

## **High Speed Rail (Crewe – Manchester)**

### **Supplementary Environmental Statement 2 and Additional Provision 2 Environmental Statement**

Data dictionary and definitions



# Data dictionary and definitions

Legend features	Definition	Source	Copyright
Area of assessment	On or close to the Proposed Scheme i.e. a distance of between 100m and 2km depending on the stage of implementation and scale of the proposed development.	High Speed Two (HS2) Ltd	
Community area boundary	The Environmental Statement has been split into sections called Community Areas.	High Speed Two (HS2) Ltd	© Crown copyright. Reproduced by permission of Ordnance Survey Licence Number 100049190. Year of Publication 2019.
County boundary	County boundaries from Ordnance Survey boundary mapping.	Ordnance Survey	© Crown copyright. Reproduced by permission of Ordnance Survey Licence Number 100049190. Year of Publication 2021.
County/Unitary Authority boundary	County and local authority boundaries from Ordnance Survey boundary mapping.	Ordnance Survey	© Crown copyright. Reproduced by permission of Ordnance Survey Licence Number 100049190. Year of Publication 2021.
Cumulative developments	Committed developments that are considered likely to be constructed between 2025 and 2038 (i.e. at the same time as the Proposed Scheme) are considered to determine whether they have the potential to give rise to cumulative effects for each environmental topic.	High Speed Two (HS2) Ltd	
Depot, station, headhouse or portal building	Extends to cover operational footprint of each depot and station and the footprint of each tunnel vent shaft and headhouse at surface level. Excludes any ancillary buildings associated with these structures.	High Speed Two (HS2) Ltd	
District/Borough boundary	Ordnance Survey local authority boundary mapping.	Ordnance Survey	© Crown copyright. Reproduced by permission of Ordnance Survey Licence Number 100049190. Year of Publication 2021.
Future baseline	Committed developments are defined as developments with planning permission and sites allocated for development in adopted development plans. Committed developments that are likely to have been completed by 2025 have the potential to alter the future baseline for the assessment of the Proposed Scheme.	High Speed Two (HS2) Ltd	

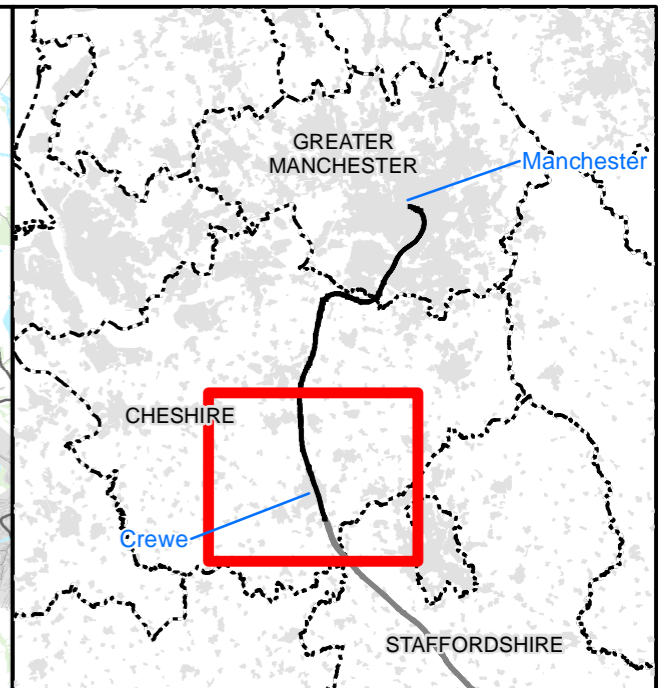
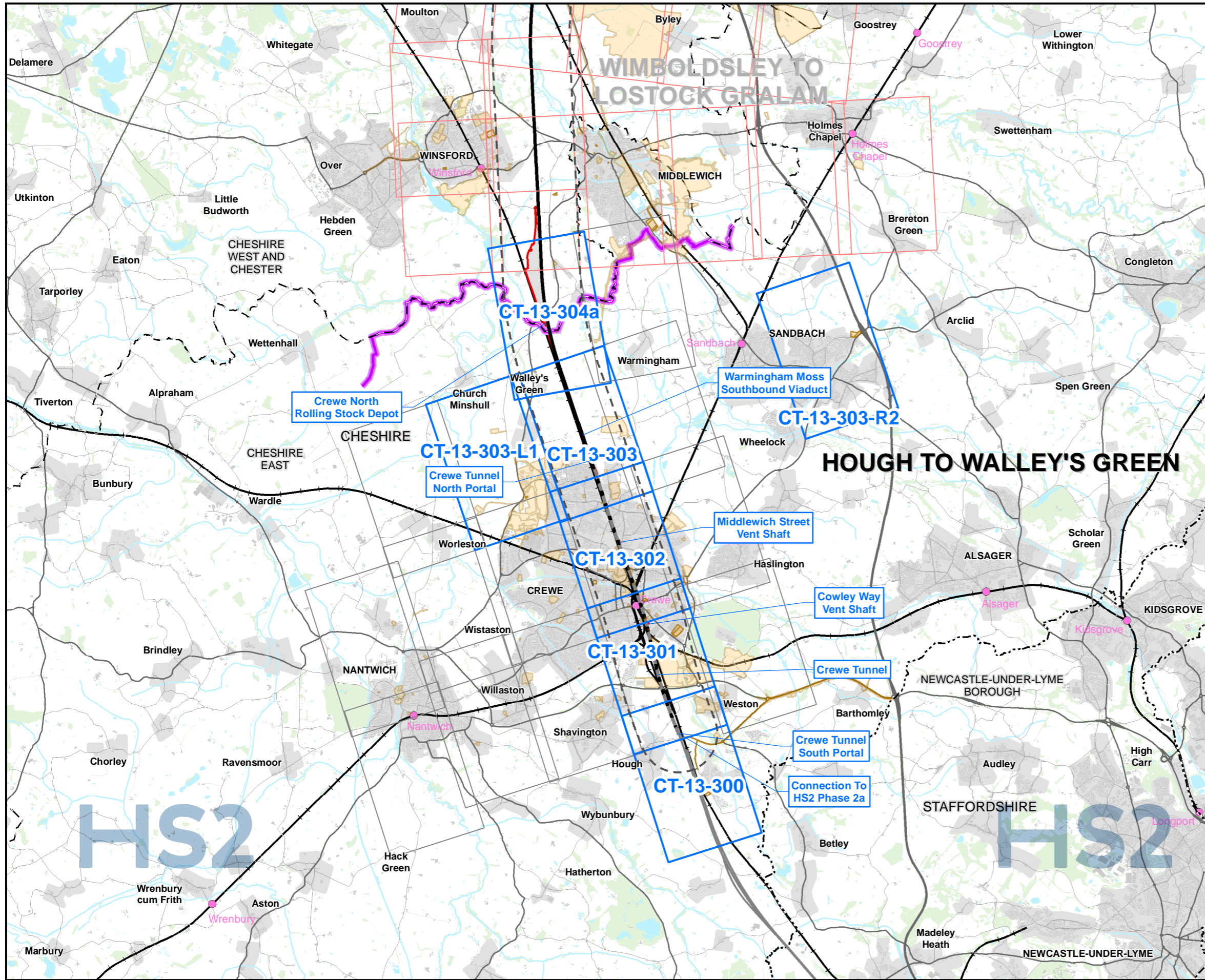
<b>Legend features</b>	<b>Definition</b>	<b>Source</b>	<b>Copyright</b>
Land potentially required during construction	Boundary defining the maximum possible extent of construction works required to build HS2 as far as the current level of design allows. This only covers surface works and includes all tunnel portals, vent shafts and headhouses, but does not apply to wholly tunnelled sections or to air rights. It also encompasses associated highway, access, drainage and utility works.	High Speed Two (HS2) Ltd	
Route in tunnel	Represents the proposed route of HS2, split into route on surface and tunnelled sections.	High Speed Two (HS2) Ltd	
Route on surface	Represents the proposed route of HS2, split into route on surface and tunnelled sections.	High Speed Two (HS2) Ltd	
Water body	Any mass of water having definite hydrological, physical, chemical and biological characteristics.	Ordnance Survey	© Crown copyright. Reproduced by permission of Ordnance Survey Licence Number 100049190. Year of Publication 2022.
Watercourse	Any channel through which water flows, can be natural or manmade. They are split into two categories (main rivers or ordinary watercourses) in England and Wales for regulation purposes.	Ordnance Survey	© Crown copyright. Reproduced by permission of Ordnance Survey Licence Number 100049190. Year of Publication 2022.
Woodland	Woodland areas derived from Ordnance Survey MasterMap data.	Ordnance Survey	© Crown copyright. Reproduced by permission of Ordnance Survey Licence Number 100049190. Year of Publication 2022.

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## **High Speed Rail (Crewe – Manchester)**

### **Supplementary Environmental Statement 2 and Additional Provision 2 Environmental Statement**

MA01: Hough to Walley's Green  
CT-13 – Committed Developments



**Map Series Information:**

CT-13 series shows the geographical distribution of as yet unimplemented but committed development which is either considered to be largely built out prior to commencement of the Proposed Scheme (future baseline) or those projects that are assumed to largely take place during implementation of the Proposed Scheme (cumulative development). These maps show those committed developments that have been taken into account in the assessment of environmental effects.

Main Map Legend	
Route in tunnel	Map sheets included in this community area
Route on surface	Map sheets included in this community area with no amendment
Depot, station, headhouse or portal building	Map sheets not included in this community area
Community area boundary	Area of assessment
Existing railway station	<b>Committed consent and development allocations:</b>
County boundary	Future Baseline
District/Borough boundary	Cumulative Development
	<b>New committed consent and development allocations:</b>
	Future Baseline
	Cumulative Development

Map Number	CT-13-INDEX-MA01
Map Name	Index Map of: Committed Developments SES2 and AP2 ES Community Area MA01: Hough to Walley's Green

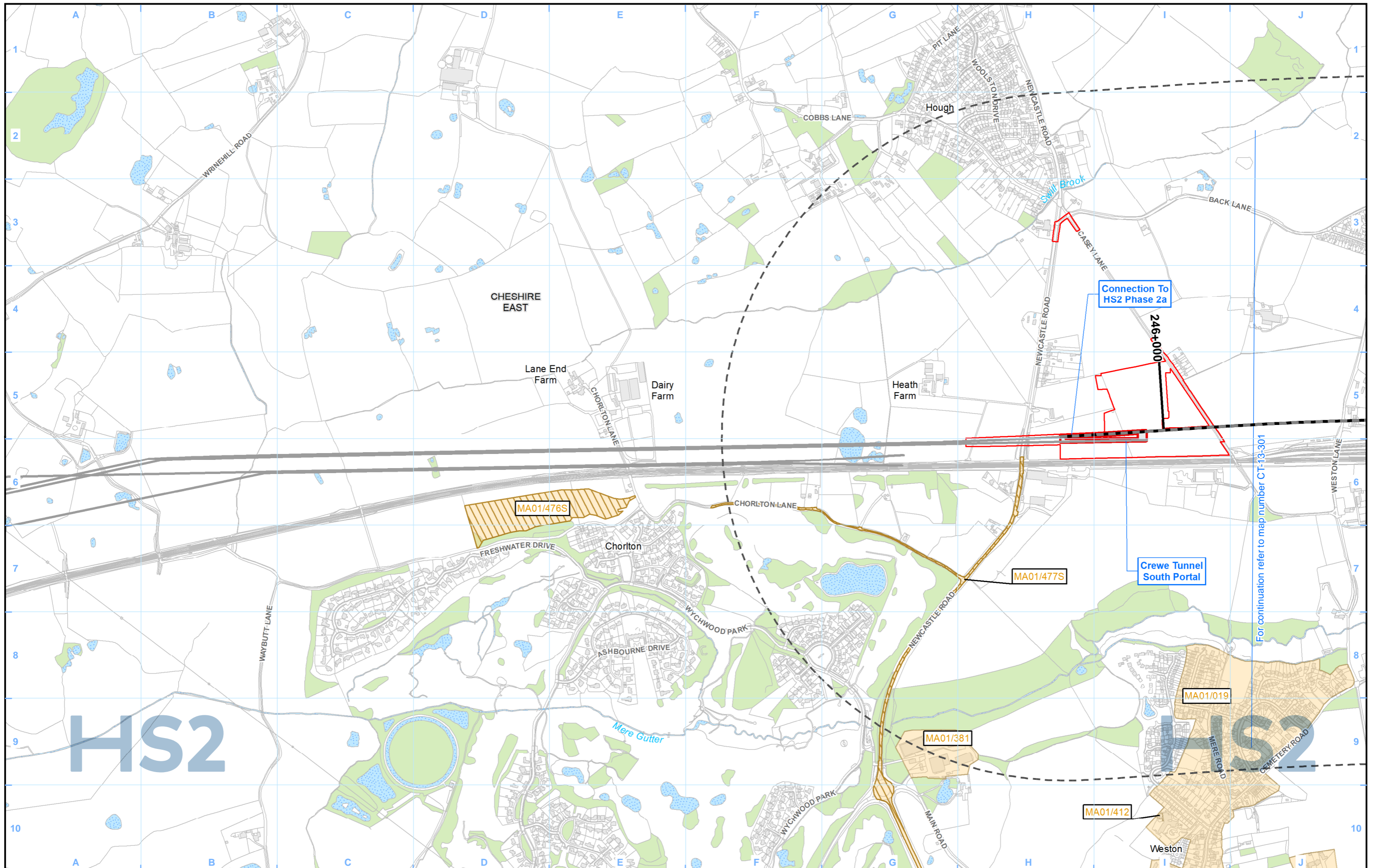
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- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary
  - Watercourse
  - Water body
  - Woodland
  - Area of assessment

- Committed consent and development allocations:
- Future Baseline
  - Cumulative Development
- New committed consent and development allocations:
- Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number  
**CT-13-300**

Map Name  
**Committed Developments  
SES2 and AP2 ES**

Community Area MA01:  
Hough to Walley's Green

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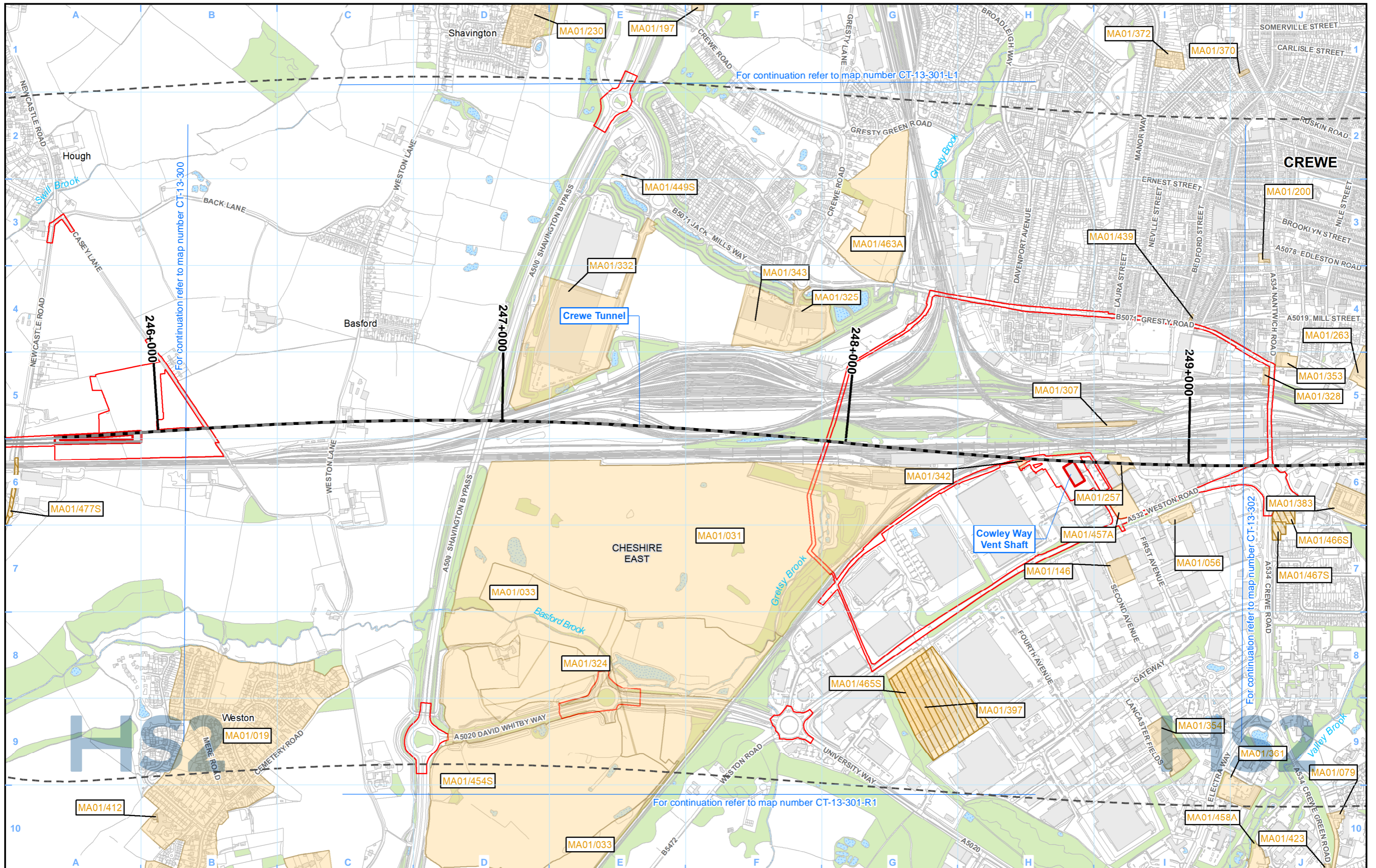
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Metres

Date: 05/04/23

For continuation refer to map number CT-13-301



**Legend**

	Route in tunnel		Watercourse		Area of assessment
	Route on surface		Water body		Committed consent and development allocations:
	Depot, station, headhouse or portal building		Woodland		Future Baseline
	Land potentially required during construction				Cumulative Development
	Community area boundary				New committed consent and development allocations:
	County boundary				Future Baseline
	District/Borough boundary				Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number: CT-13-301

Map Name: Committed Developments SES2 and AP2 ES

Community Area MA01: Hough to Walley's Green

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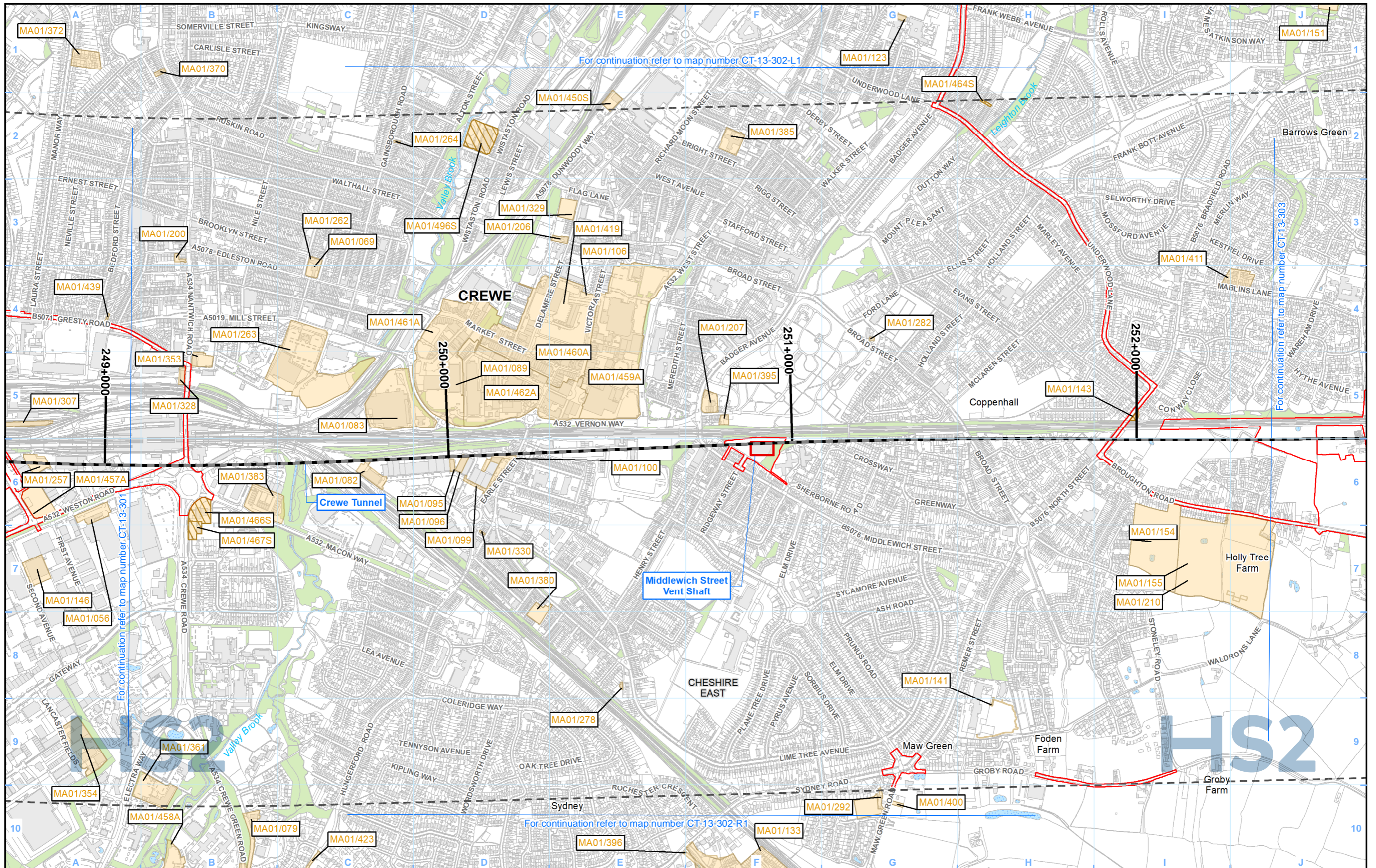
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- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary
  - Watercourse
  - Water body
  - Woodland
  - Area of assessment
  - Committed consent and development allocations:**
  - Future Baseline
  - Cumulative Development
  - New committed consent and development allocations:**
  - Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number **CT-13-302**

Map Name **Committed Developments SES2 and AP2 ES**

Community Area MA01:  
Hough to Walley's Green

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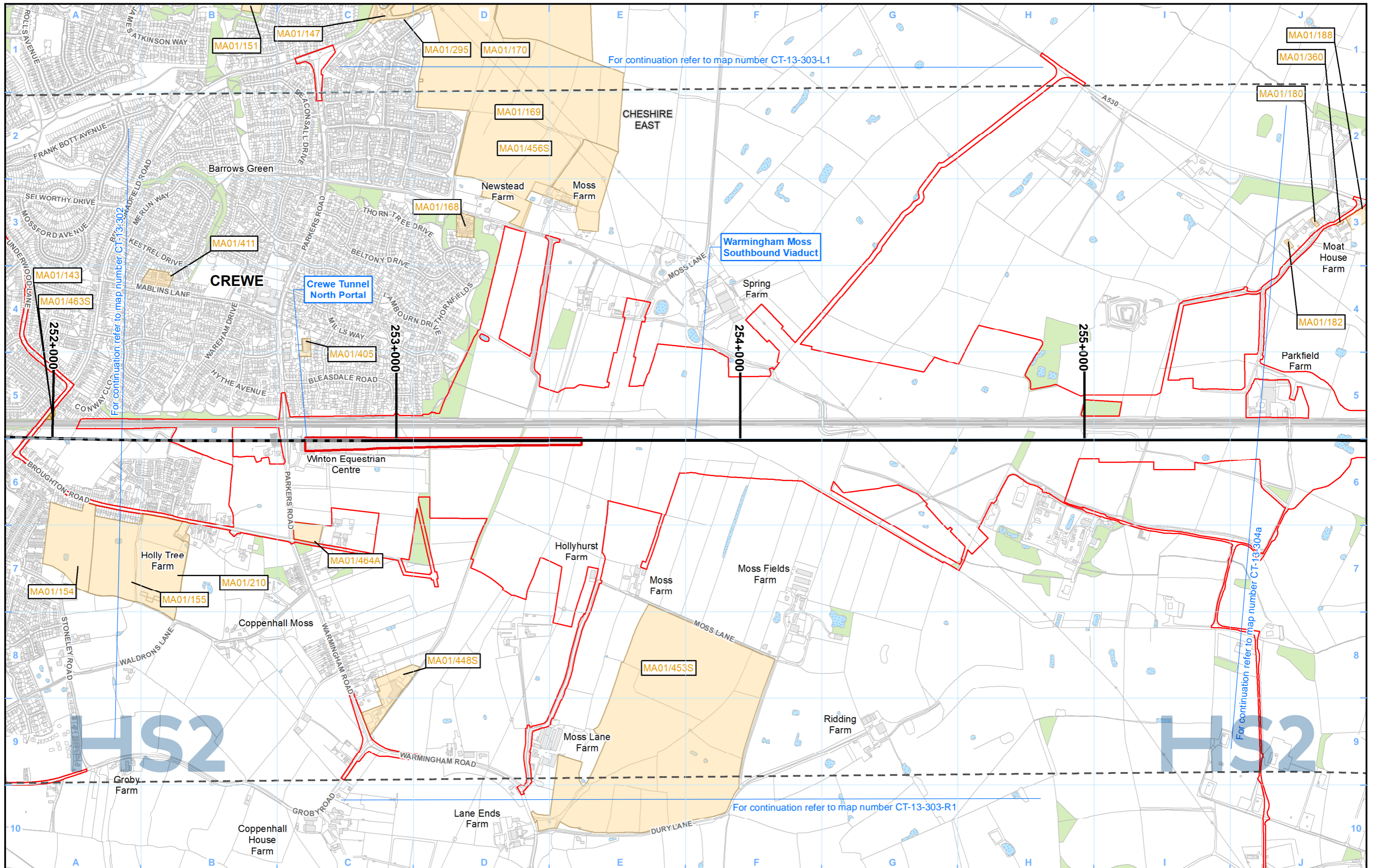
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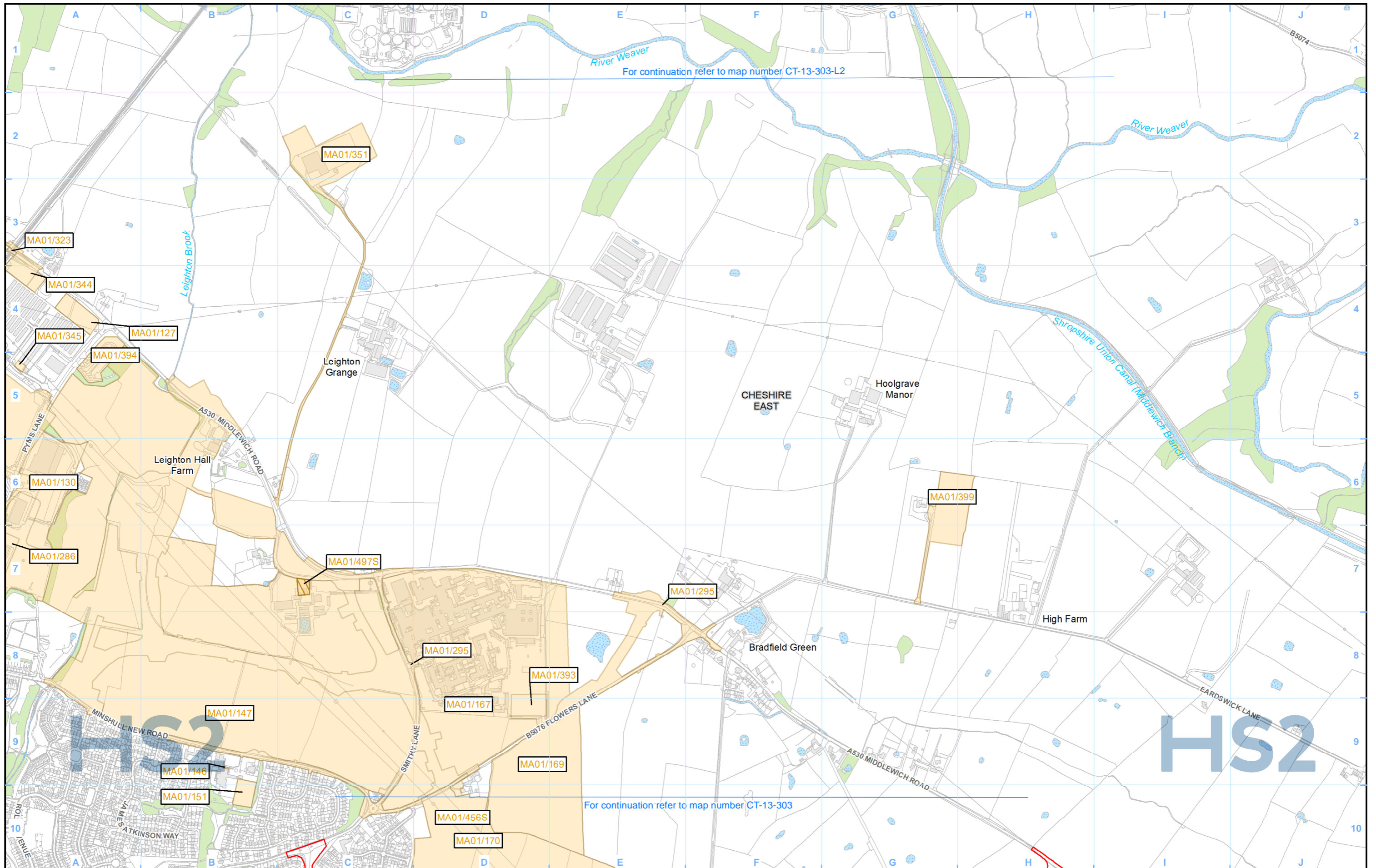


For continuation refer to map number CT-13-303-L1

For continuation refer to map number CT-13-304a

For continuation refer to map number CT-13-303-R1

<p><b>Legend</b></p> <ul style="list-style-type: none"> <li>Route in tunnel</li> <li>Route on surface</li> <li>Depot, station, headhouse or portal building</li> <li>Land potentially required during construction</li> <li>Community area boundary</li> <li>County boundary</li> <li>District/Borough boundary</li> </ul>	<ul style="list-style-type: none"> <li>Watercourse</li> <li>Water body</li> <li>Woodland</li> </ul>	<ul style="list-style-type: none"> <li>Area of assessment</li> <li>Committed consent and development allocations: <ul style="list-style-type: none"> <li>Future Baseline</li> <li>Cumulative Development</li> </ul> </li> <li>New committed consent and development allocations: <ul style="list-style-type: none"> <li>Future Baseline</li> <li>Cumulative Development</li> </ul> </li> </ul>	<p>Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.</p> <p>Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.</p>	<p>Map Number: CT-13-303</p> <p>Map Name: Committed Developments SES2 and AP2 ES</p> <p>Community Area MA01: Hough to Walley's Green</p>	<p>HS2 Ltd accept no responsibility for any circumstances, which arise from the reproduction of this map after alteration, amendment or abbreviation or if it is issued in part or issued incomplete in any way.</p> <p>Registered in England. Registration number 06791686. Registered office: Two Snowhill, Snow Hill Queensway, Birmingham B4 6GA.</p> <p>© Crown copyright and database rights 2022 OS 100049190</p> <p>Doc Number: 2PT27-MWJ-EV-MAP-M000-000677-P01</p> <p>Date: 05/04/23</p> <p>Scale at A3: 1:10,000</p>
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- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary
  - Watercourse
  - Water body
  - Woodland
  - Area of assessment

- Committed consent and development allocations:**
- Future Baseline
  - Cumulative Development
- New committed consent and development allocations:**
- Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number	CT-13-303-L1
Map Name	Committed Developments SES2 and AP2 ES
Community Area MA01: Hough to Walley's Green	

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**Legend**

- Route in tunnel
- Route on surface
- Depot, station, headhouse or portal building
- Land potentially required during construction
- Community area boundary
- County boundary
- District/Borough boundary

- Watercourse
- Water body
- Woodland

Area of assessment

Committed consent and development allocations:

- Future Baseline
- Cumulative Development

New consent and development allocations:

- Future Baseline
- Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number: CT-13-303-R2

Map Name: Committed Developments SES2 and AP2 ES

Community Area MA01: Hough to Walley's Green

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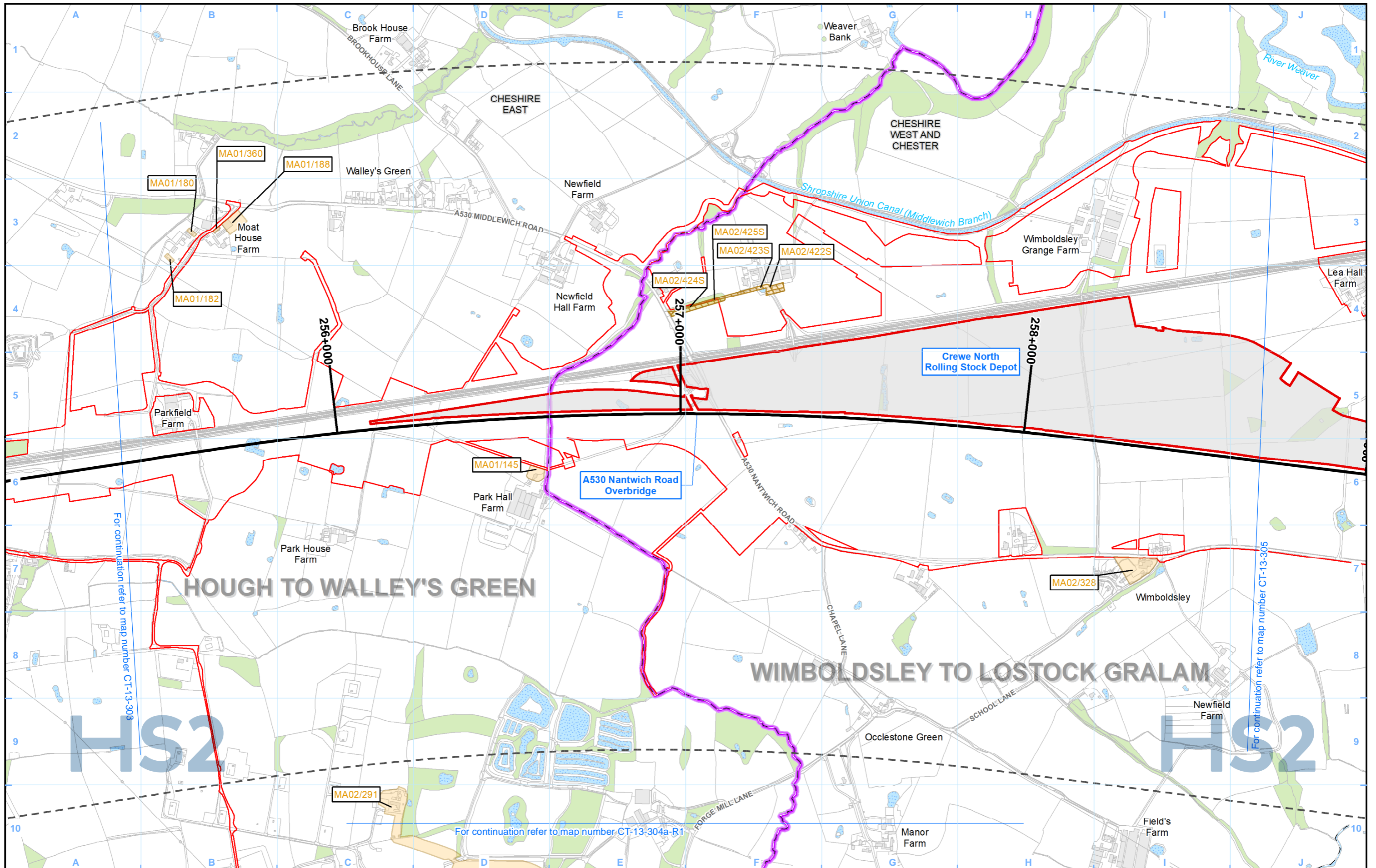
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**Legend**

- Route in tunnel
- Route on surface
- Depot, station, headhouse or portal building
- Land potentially required during construction
- Community area boundary
- County boundary
- District/Borough boundary

- Watercourse
- Water body
- Woodland

Area of assessment

Committed consent and development allocations:

- Future Baseline
- Cumulative Development

New committed consent and development allocations:

- Future Baseline
- Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number: CT-13-304a

Map Name: Committed Developments SES2 and AP2 ES

Community Area MA01: Hough to Walley's Green

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For continuation refer to map number CT-13-303

For continuation refer to map number CT-13-305

For continuation refer to map number CT-13-304a-R1

**HOUGH TO WALLEY'S GREEN**

**WIMBOLDSLEY TO LOSTOCK GRALAM**

**HS2**

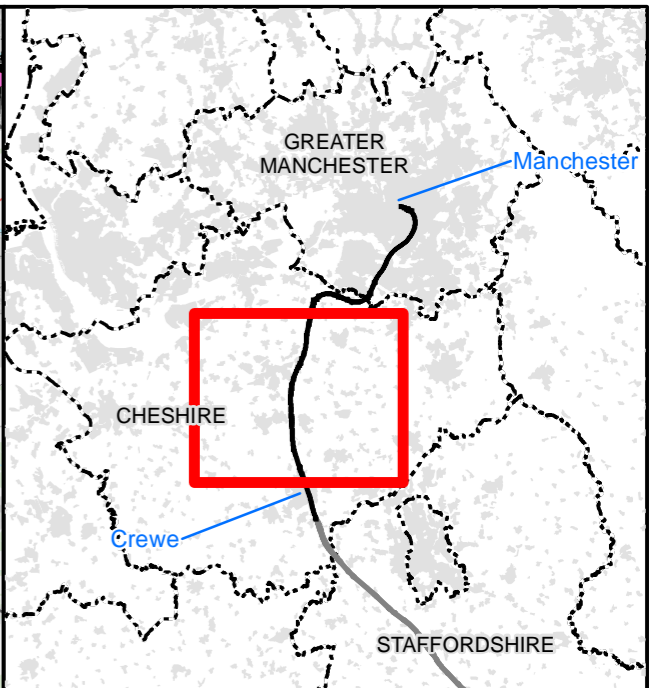
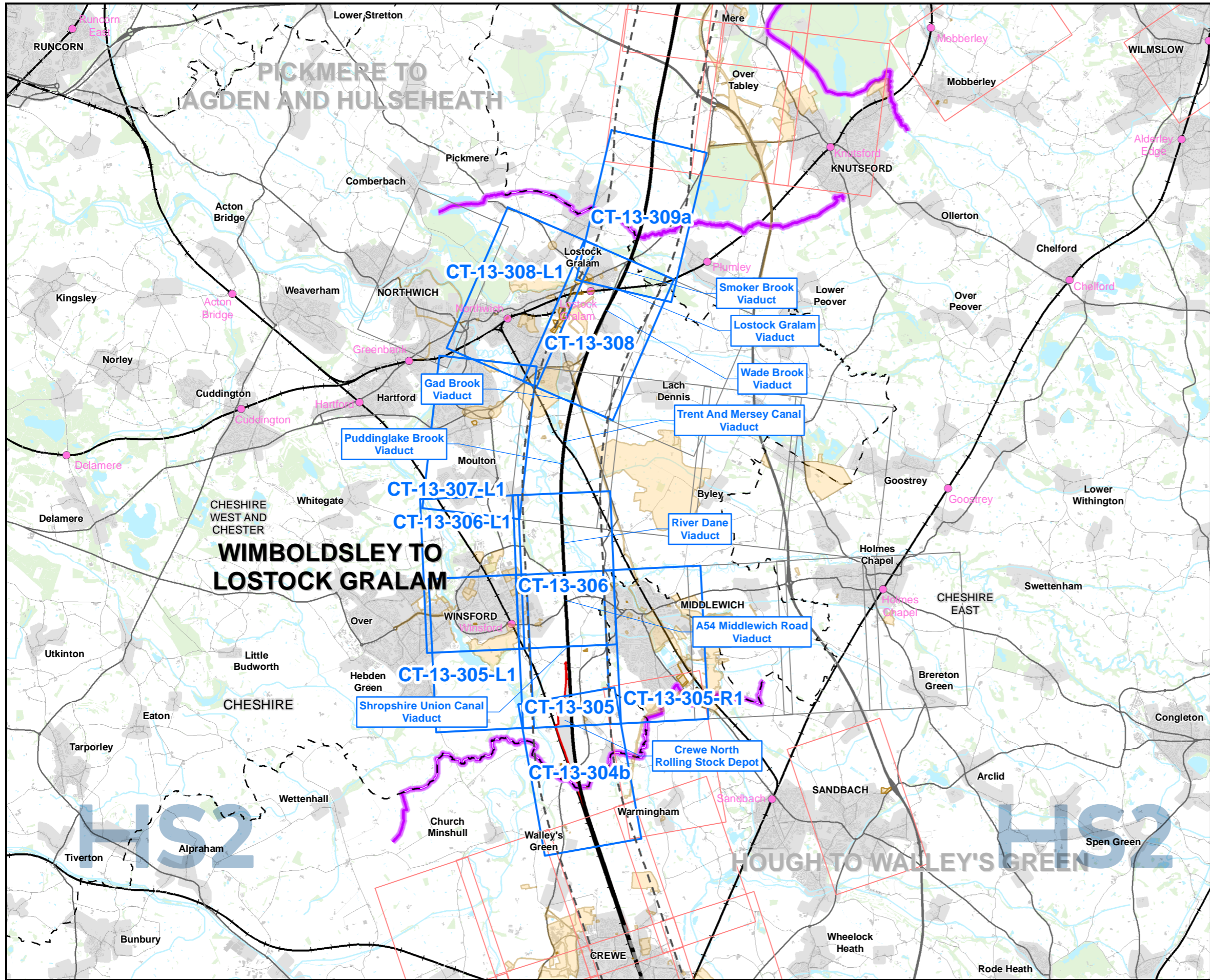
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## **High Speed Rail (Crewe – Manchester)**

### **Supplementary Environmental Statement 2 and Additional Provision 2 Environmental Statement**

MA02: Wimboldsley to Lostock Gralam  
CT-13 – Committed Developments



**Map Series Information:**

CT-13 series shows the geographical distribution of as yet unimplemented but committed development which is either considered to be largely built out prior to commencement of the Proposed Scheme (future baseline) or those projects that are assumed to largely take place during implementation of the Proposed Scheme (cumulative development). These maps show those committed developments that have been taken into account in the assessment of environmental effects.

**Main Map Legend**

- Route in tunnel
- Route on surface
- Depot, station, headhouse or portal building
- Community area boundary
- Existing railway station
- County boundary
- District/Borough boundary
- Map sheets included in this community area
- Map sheets included in this community area with no amendment
- Map sheets not included in this community area
- Area of assessment

- Committed consent and development allocations:**
- Future Baseline
  - Cumulative Development
- New committed consent and development allocations:**
- Future Baseline
  - Cumulative Development

Map Number	CT-13-INDEX-MA02
Map Name	Index Map of: Committed Developments SES2 and AP2 ES Community Area MA02: Wimboldsley to Lostock Gralam

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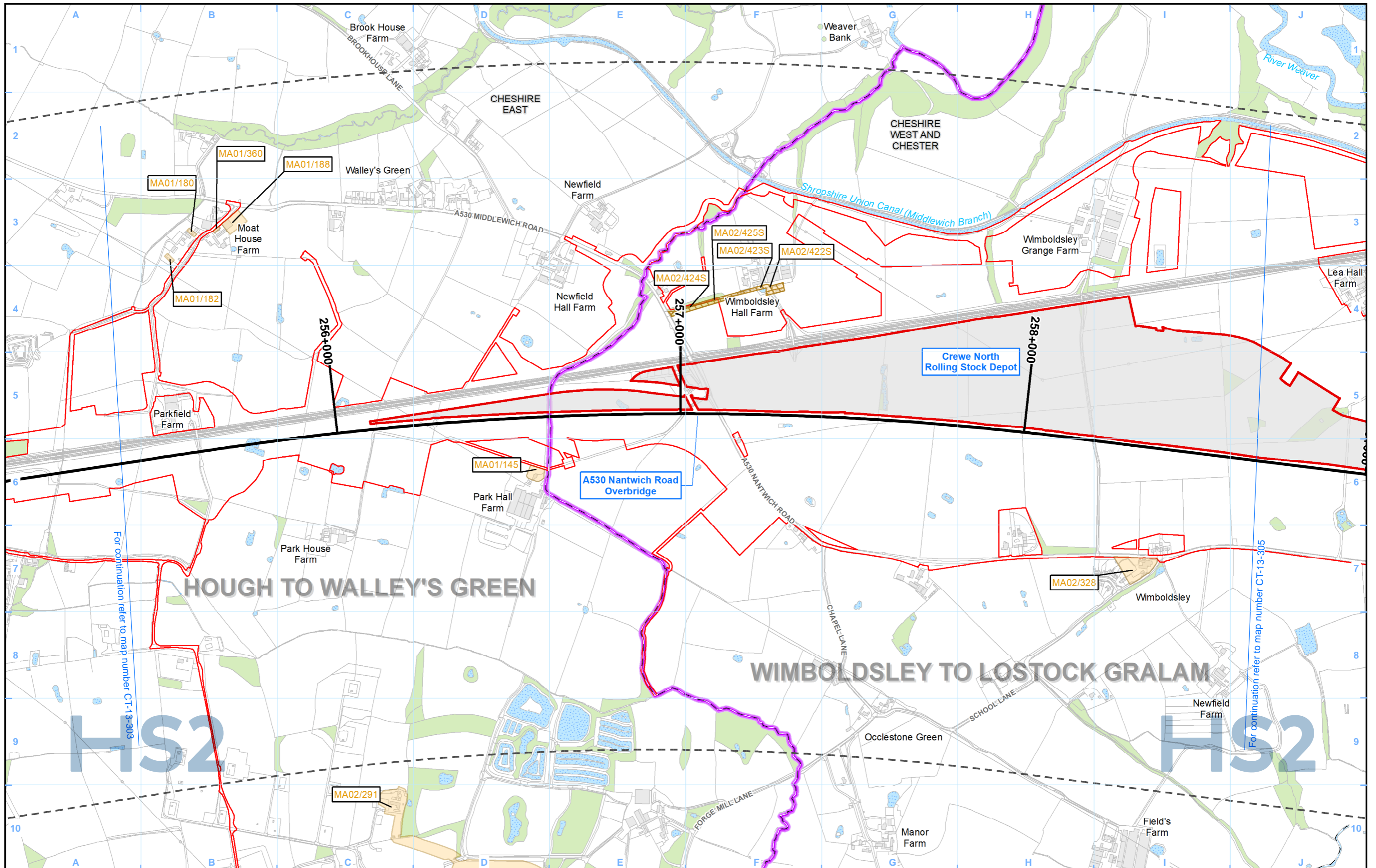
Scale at A3: 1:100,000

0 1,000 2,000 3,000 4,000  
Metres

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Note: Not all data layers in the legend are represented on every map.



**HOUGH TO WALLEY'S GREEN**

**WIMBOLDSLEY TO LOSTOCK GRALAM**

- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary

- Watercourse
- Water body
- Woodland

- Area of assessment
- Committed consent and development allocations:
- Future Baseline
  - Cumulative Development
- New committed consent and development allocations:
- Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number	CT-13-304b
Map Name	Committed Developments SES2 and AP2 ES
Community Area MA02: Wimboldsley to Lostock Gralam	

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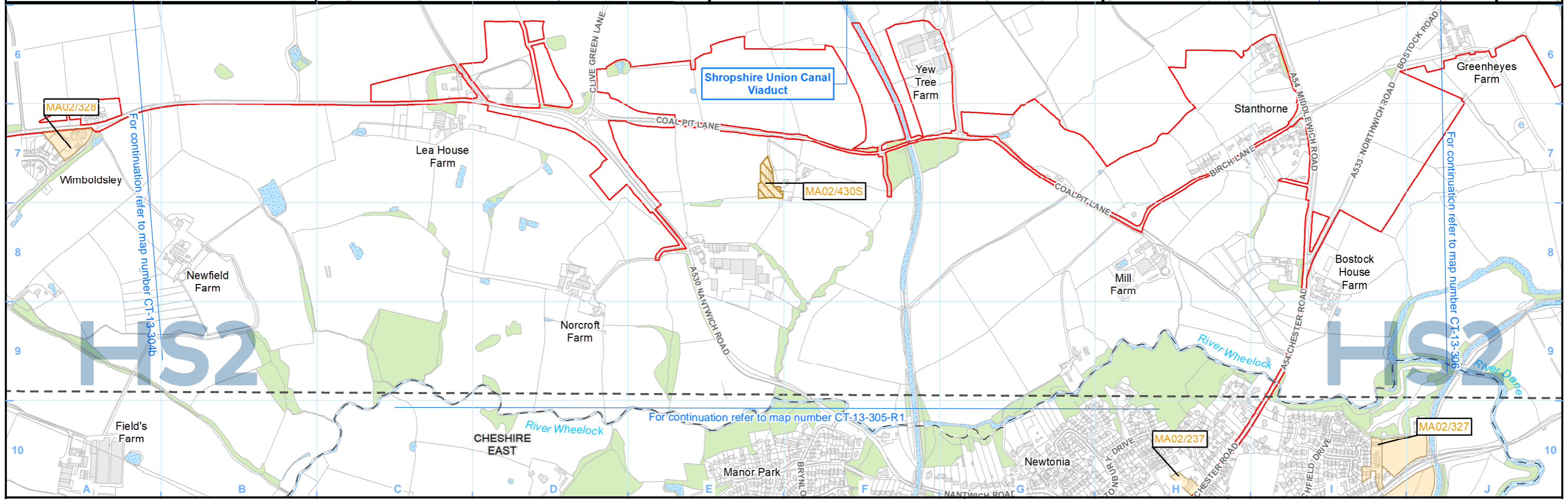
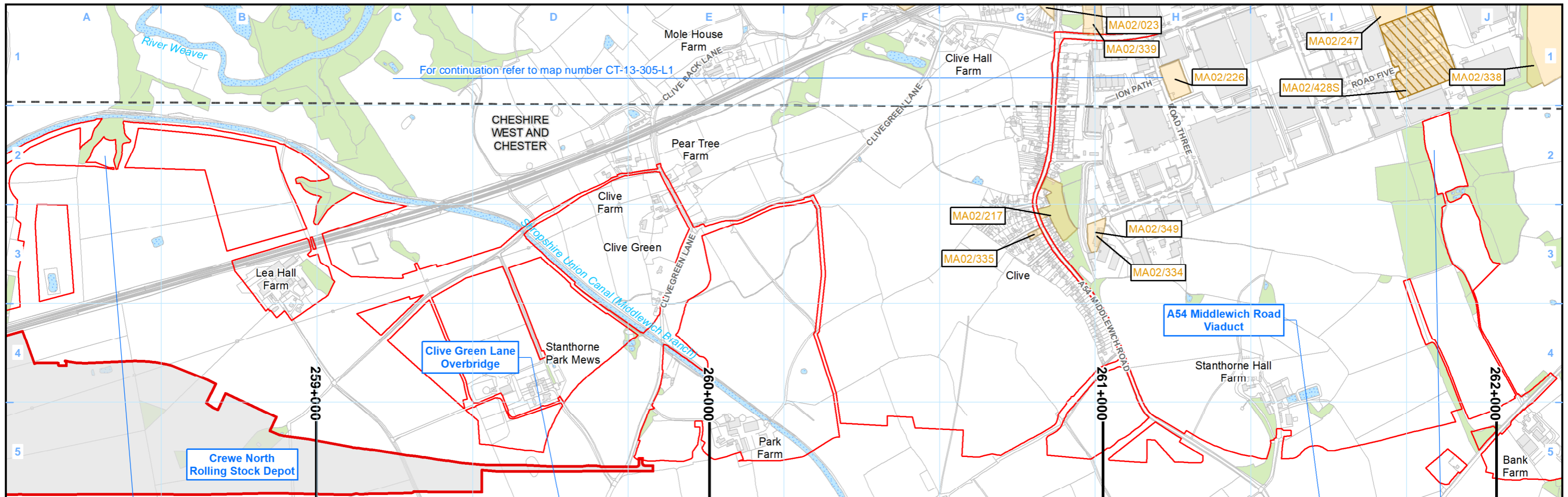
Scale at A3: 1:10,000

Date: 05/04/23

For continuation refer to map number CT-13-303

For continuation refer to map number CT-13-305





- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary
  - Watercourse
  - Water body
  - Woodland
  - Area of assessment
  - Committed consent and development allocations: Future Baseline
  - Cumulative Development
  - New committed consent and development allocations: Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number  
**CT-13-305**

Map Name  
**Committed Developments  
SES2 and AP2 ES**

Community Area MA02:  
Wimboldsley to Lostock Gralam

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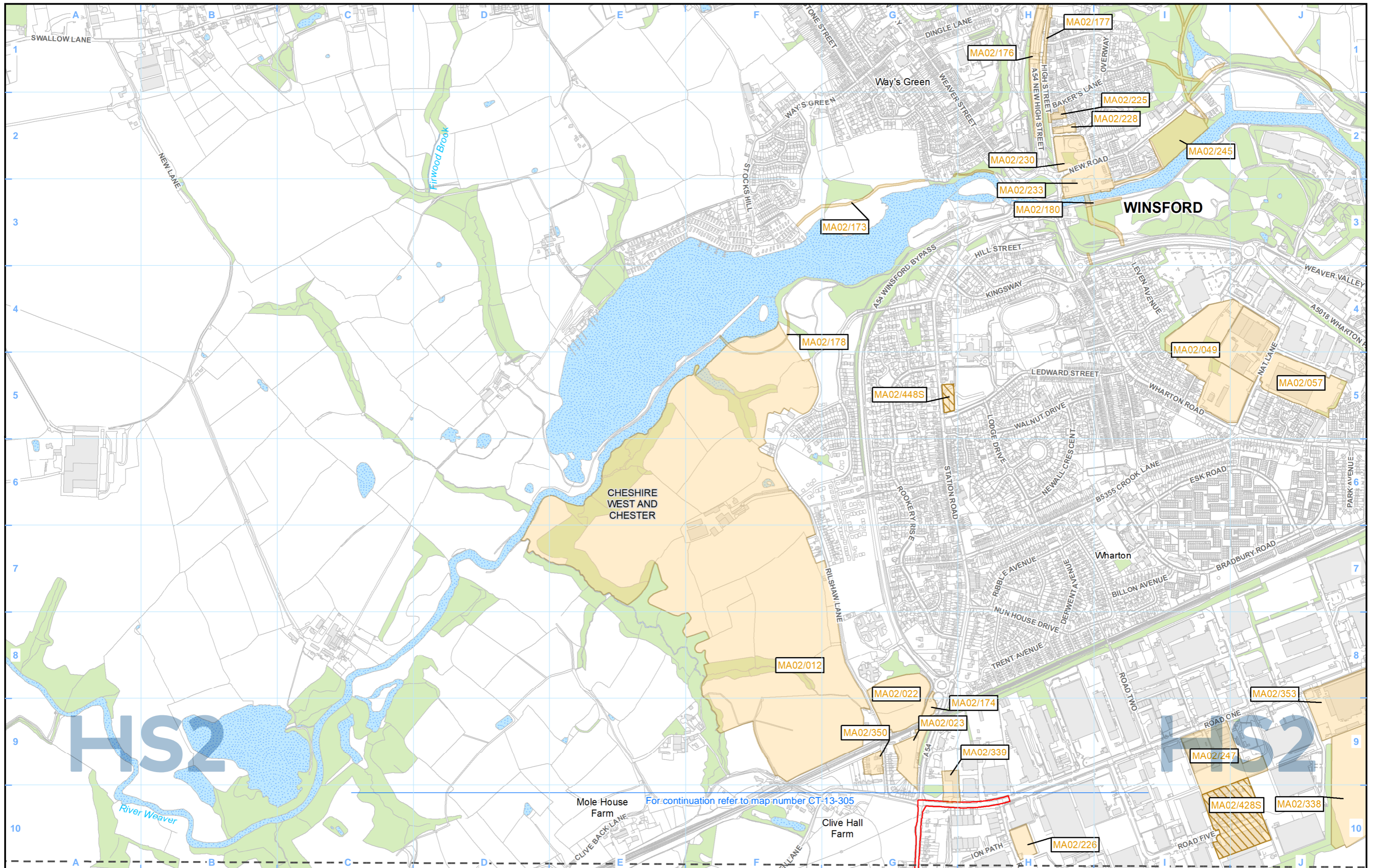
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Scale at A3: 1:10,000

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Metres

Date: 05/04/23





**Legend**

		<b>Committed consent and development allocations:</b>
		<b>New committed consent and development allocations:</b>

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number **CT-13-305-L1**

Map Name **Committed Developments SES2 and AP2 ES**

Community Area MA02:  
Wimboldsley to Lostock Gralam

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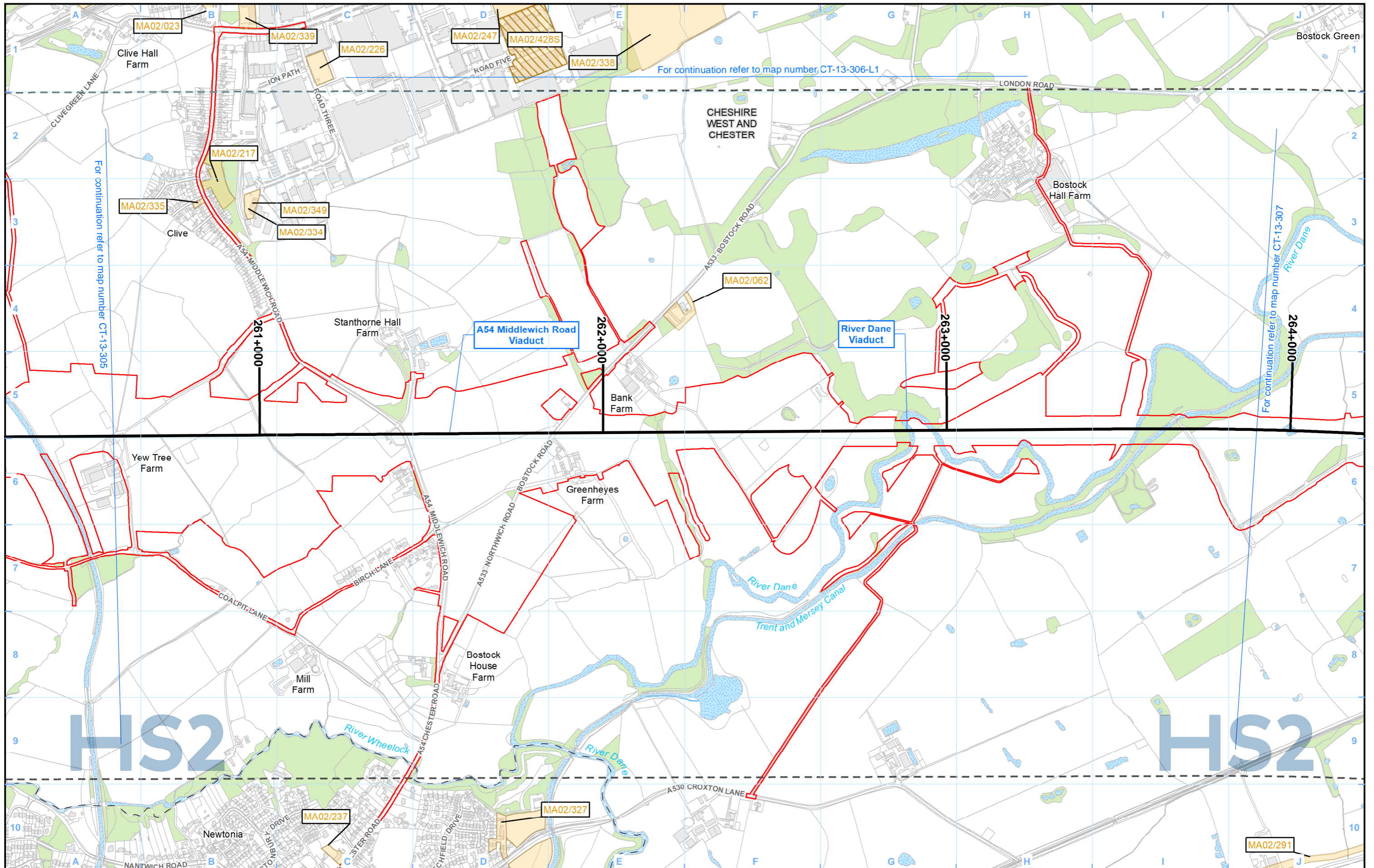
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- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary
  - Watercourse
  - Water body
  - Woodland
  - Area of assessment

- Committed consent and development allocations:**
- Future Baseline
  - Cumulative Development
- New committed consent and development allocations:**
- Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number: CT-13-306

Map Name: Committed Developments SES2 and AP2 ES

Community Area MA02: Wimboldsley to Lostock Gralam

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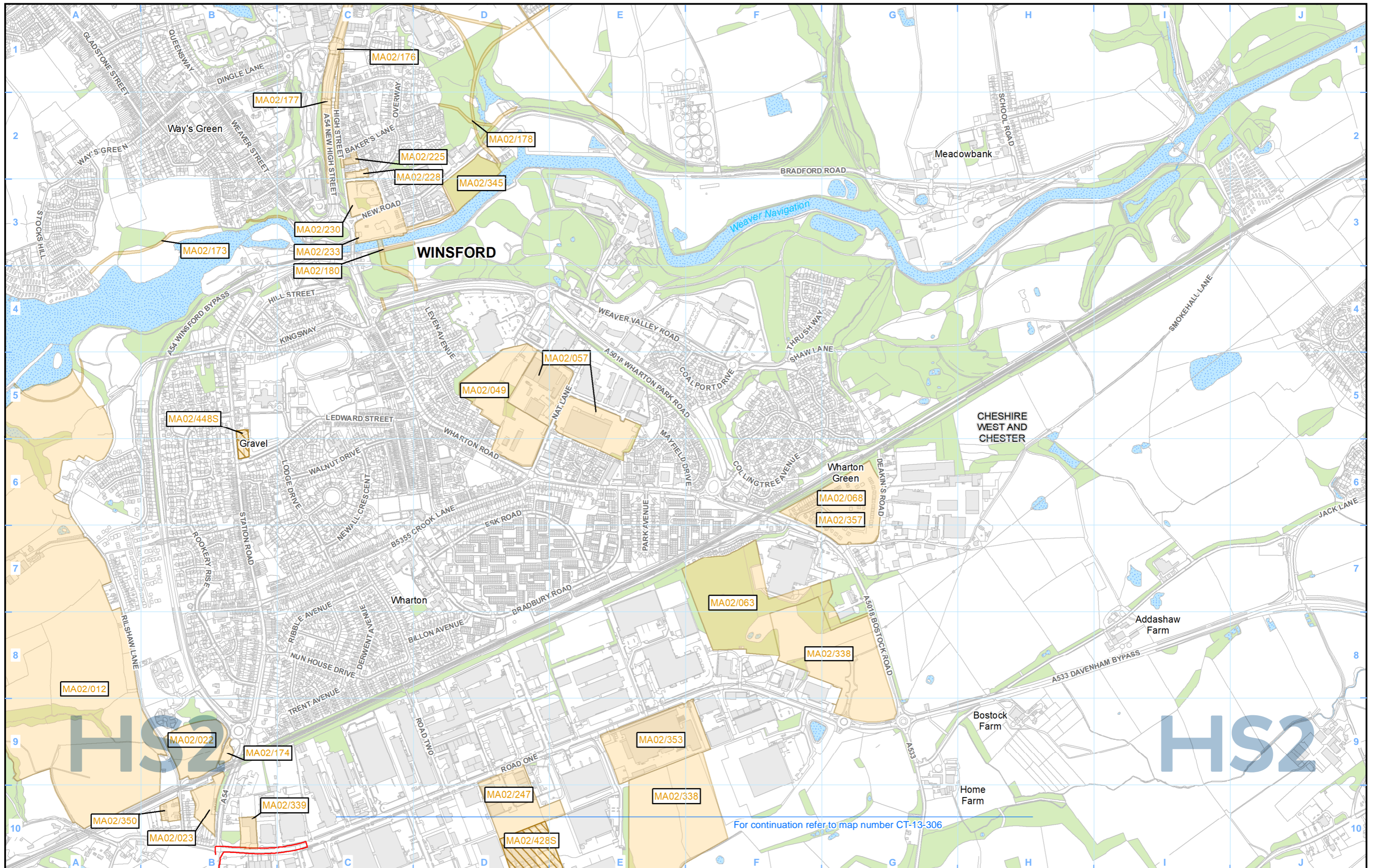
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Date: 05/04/23

Scale at A3: 1:10,000

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- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary

- Watercourse
- Water body
- Woodland
- Area of assessment

- Committed consent and development allocations:
- Future Baseline
  - Cumulative Development
- New committed consent and development allocations:
- Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number: CT-13-306-L1

Map Name: Committed Developments SES2 and AP2 ES

Community Area MA02: Wimboldsley to Lostock Gralam

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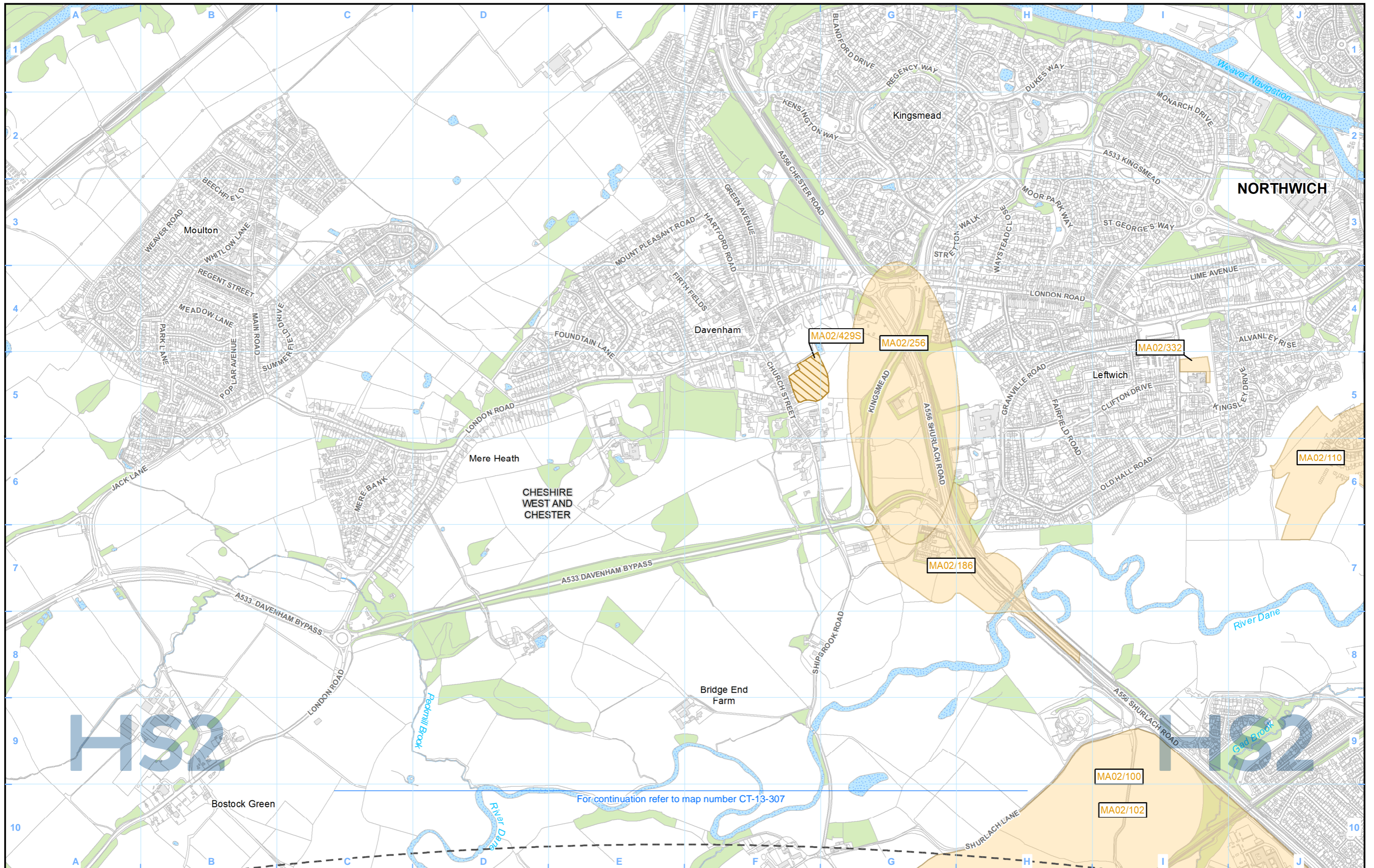
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For continuation refer to map number CT-13-306



- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary
  - Watercourse
  - Water body
  - Woodland
  - Area of assessment

- Committed consent and development allocations:
- Future Baseline
  - Cumulative Development
- New committed consent and development allocations:
- Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number  
**CT-13-307-L1**

Map Name  
**Committed Developments  
SES2 and AP2 ES**

Community Area MA02:  
Wimboldsley to Lostock Gralam

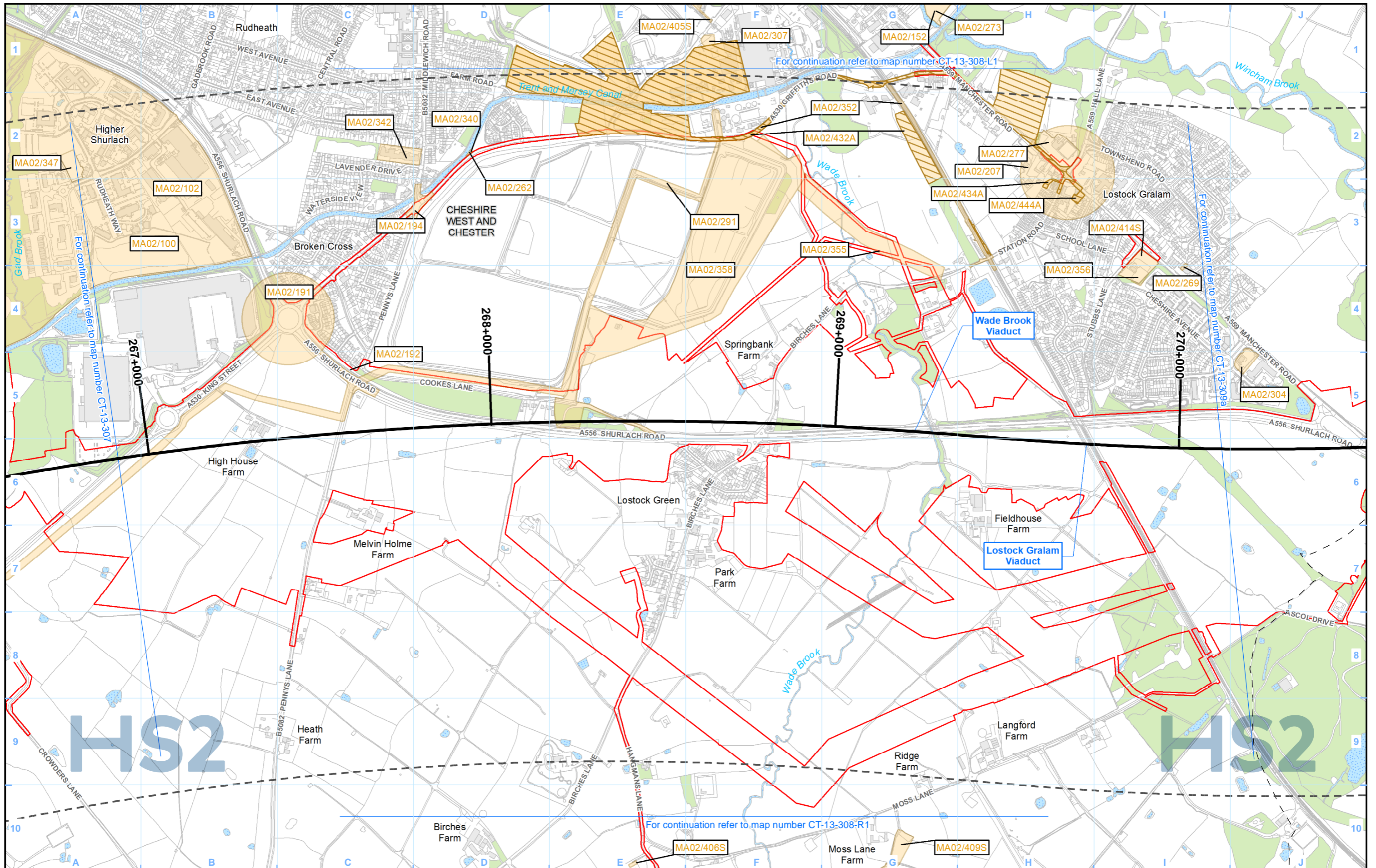
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- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary

- Watercourse
- Water body
- Woodland

- Area of assessment
- Committed consent and development allocations:
- Future Baseline
  - Cumulative Development
- New committed consent and development allocations:
- Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number	CT-13-308
Map Name	Committed Developments SES2 and AP2 ES
Community Area MA02: Wimboldsley to Lostock Gralam	

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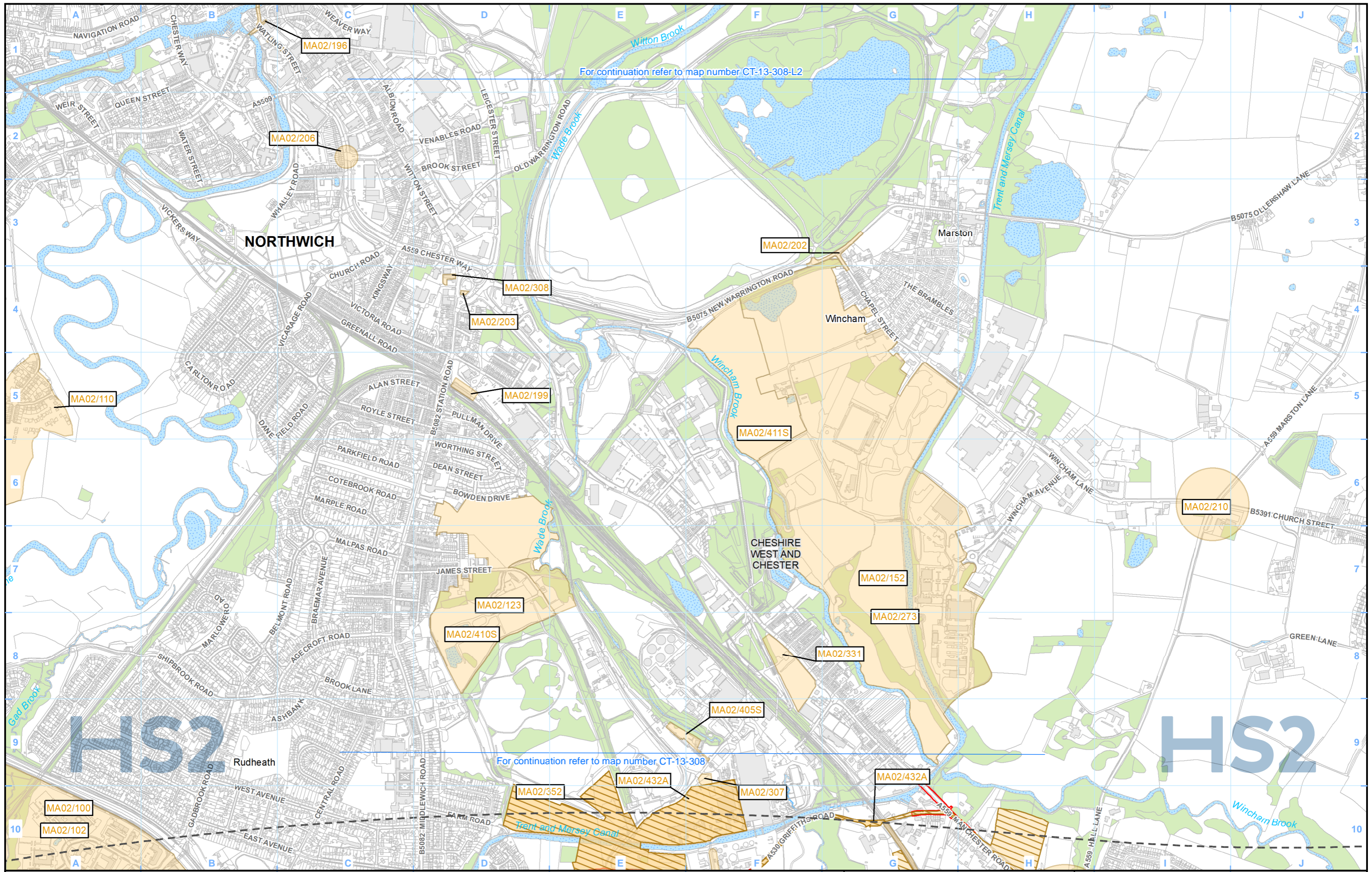
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Scale at A3: 1:10,000

0 100 200 300 400 Metres

Date: 05/04/23



For continuation refer to map number CT-13-308-L2

For continuation refer to map number CT-13-308

**Legend**

		<b>Committed consent and development allocations:</b>
		<b>New committed consent and development allocations:</b>

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number  
**CT-13-308-L1**

Map Name  
**Committed Developments  
SES2 and AP2 ES**

Community Area MA02:  
Wimboldsley to Lostock Gralam

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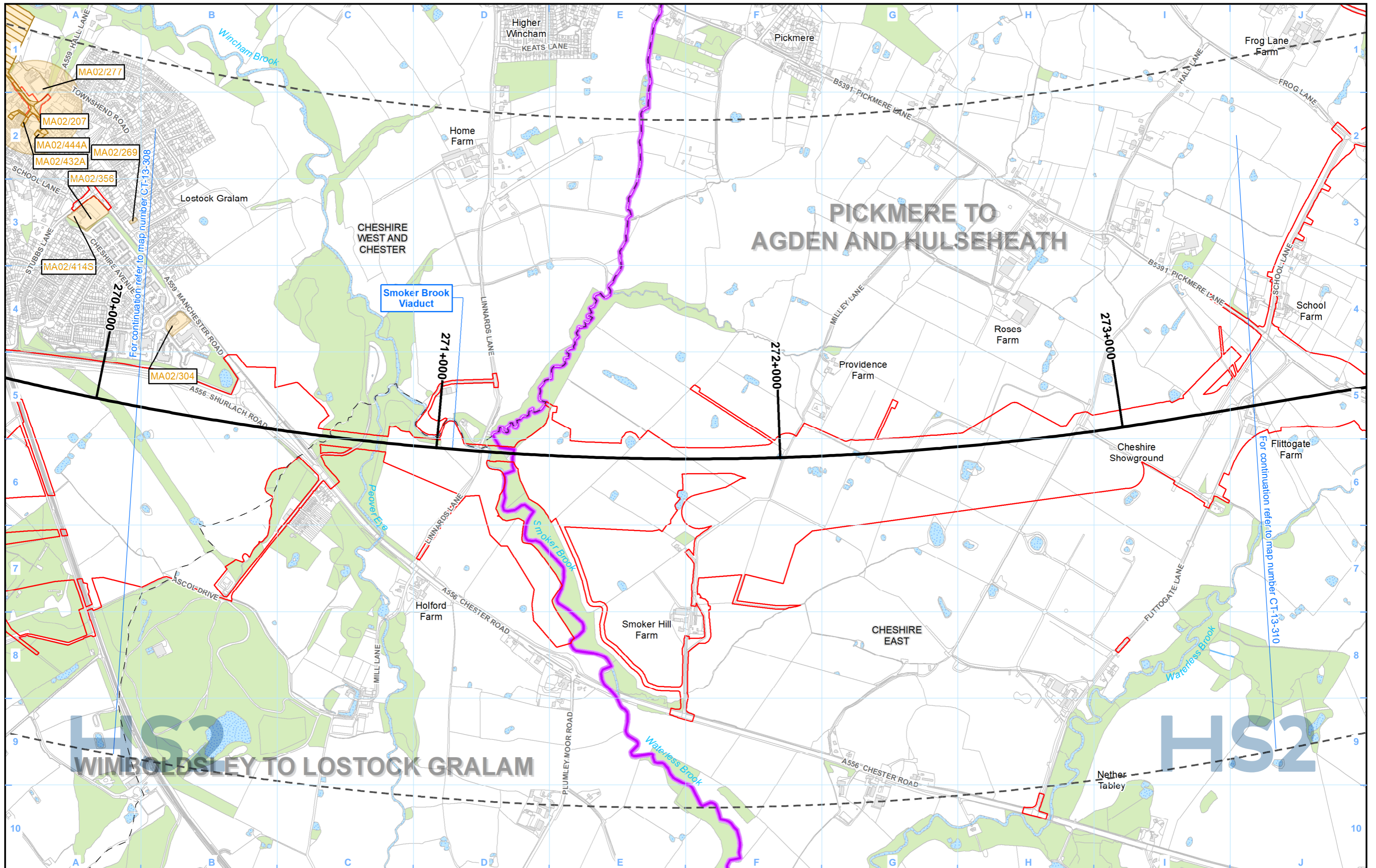
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Metres

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- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary


- Watercourse
- Water body
- Woodland

- Area of assessment
- Committed consent and development allocations:
- Future Baseline
  - Cumulative Development
- New committed consent and development allocations:
- Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number	CT-13-309a
Map Name	Committed Developments SES2 and AP2 ES
Community Area MA02: Wimboldsley to Lostock Gralam	




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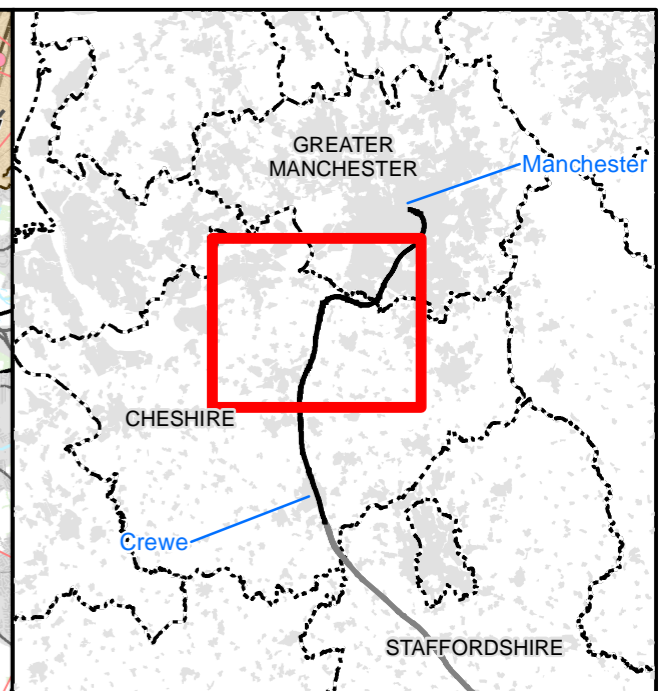
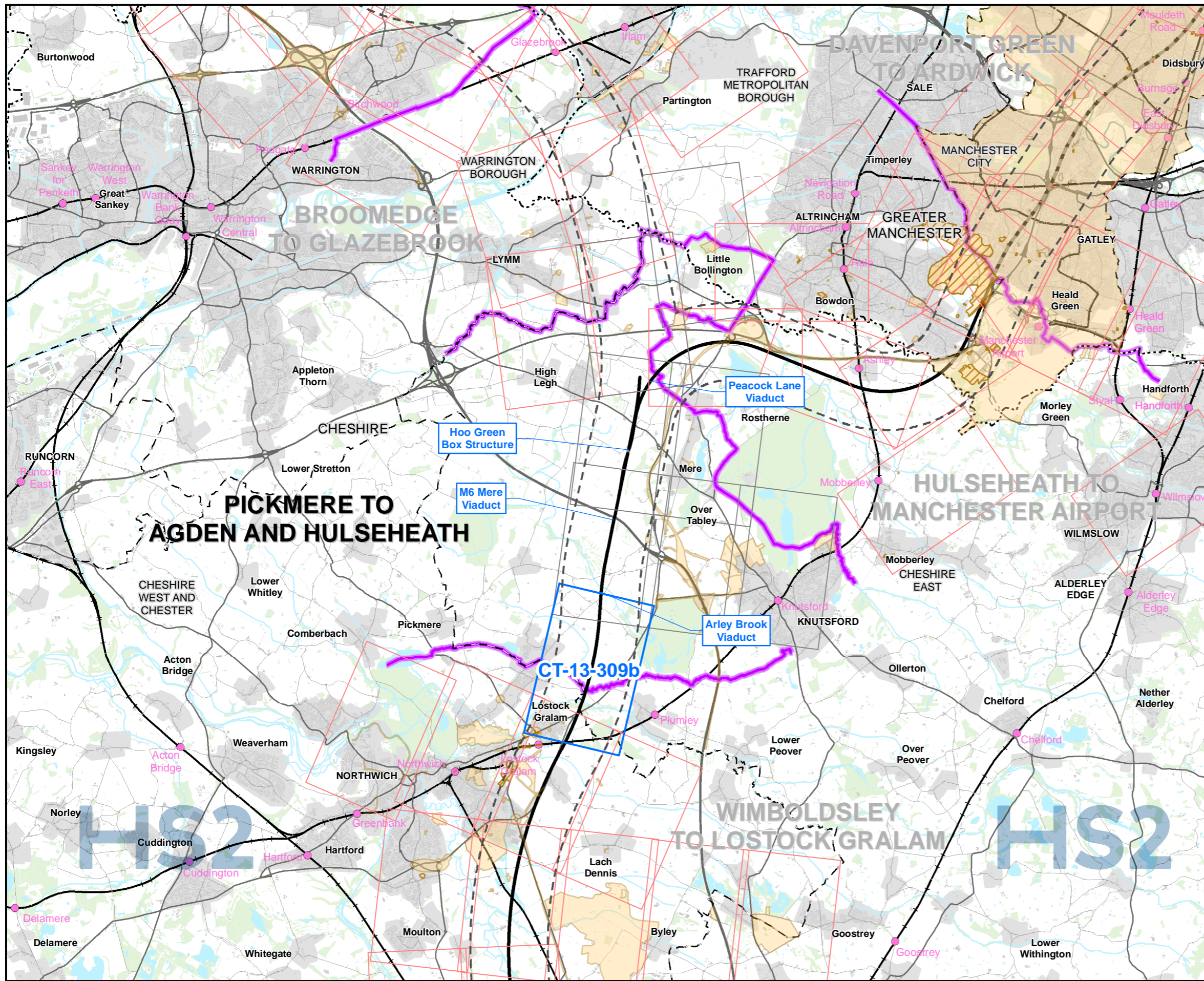


Date: 05/04/23

## **High Speed Rail (Crewe – Manchester)**

### **Supplementary Environmental Statement 2 and Additional Provision 2 Environmental Statement**

MA03: Pickmere to Agden and Hulseheath  
CT-13 – Committed Developments



**Map Series Information:**

CT-13 series shows the geographical distribution of as yet unimplemented but committed development which is either considered to be largely built out prior to commencement of the Proposed Scheme (future baseline) or those projects that are assumed to largely take place during implementation of the Proposed Scheme (cumulative development). These maps show those committed developments that have been taken into account in the assessment of environmental effects.

**Main Map Legend**

- Route in tunnel
- Route on surface
- Depot, station, headhouse or portal building
- Community area boundary
- Existing railway station
- County boundary
- District/Borough boundary
- Map sheets included in this community area
- Map sheets included in this community area with no amendment
- Map sheets not included in this community area
- Area of assessment

- Committed consent and development allocations:**
- Future Baseline
  - Cumulative Development
- New committed consent and development allocations:**
- Future Baseline
  - Cumulative Development

Map Number	CT-13-INDEX-MA03
Map Name	Index Map of: Committed Developments SES2 and AP2 ES Community Area MA03: Pickmere to Agden and Hulseheath

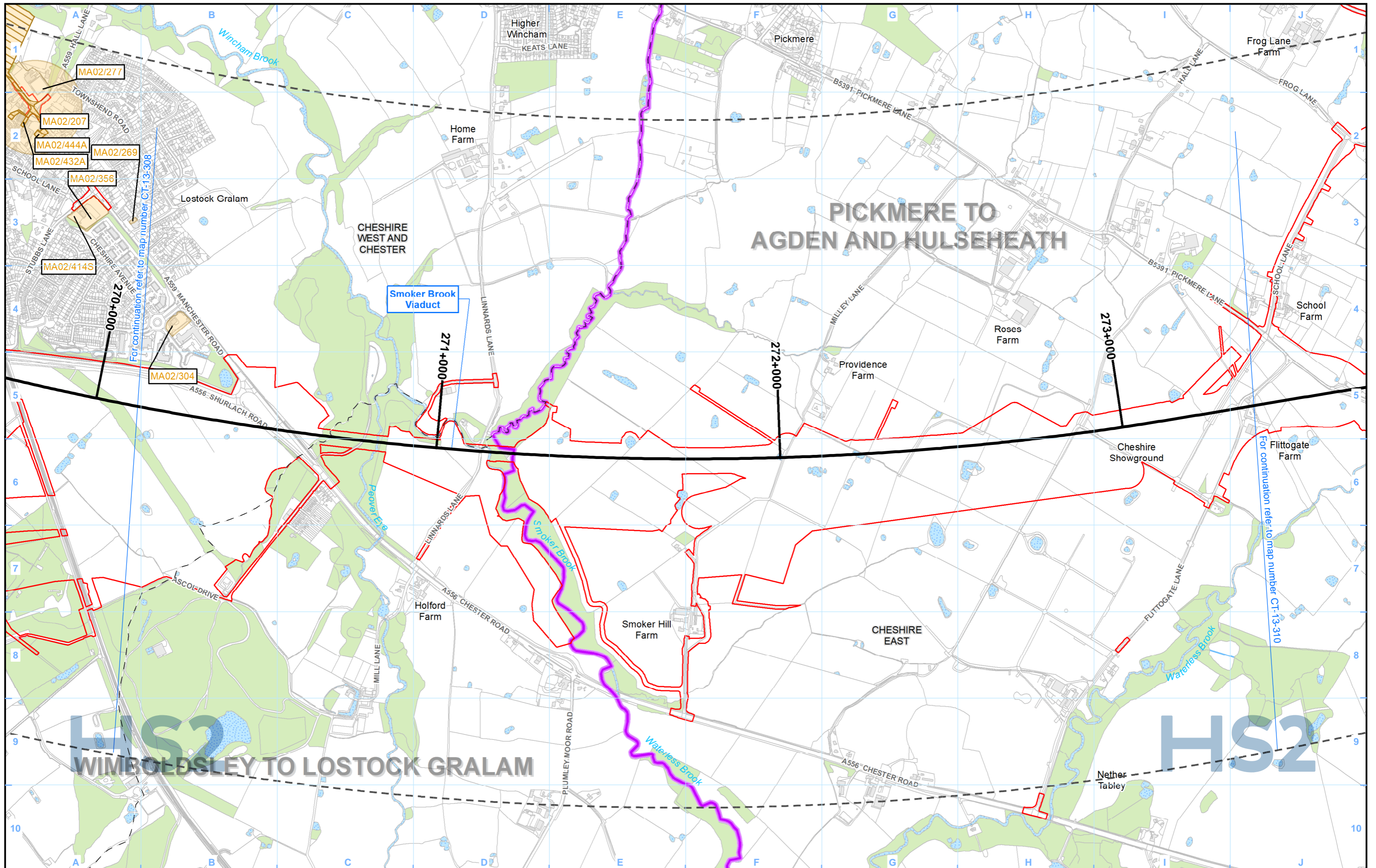
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- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary
  - Watercourse
  - Water body
  - Woodland

- Area of assessment**
- Committed consent and development allocations:**
- Future Baseline
  - Cumulative Development
- New committed consent and development allocations:**
- Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number	CT-13-309b
Map Name	Committed Developments SES2 and AP2 ES
Community Area MA03: Pickmere to Agden and Hulseheath	

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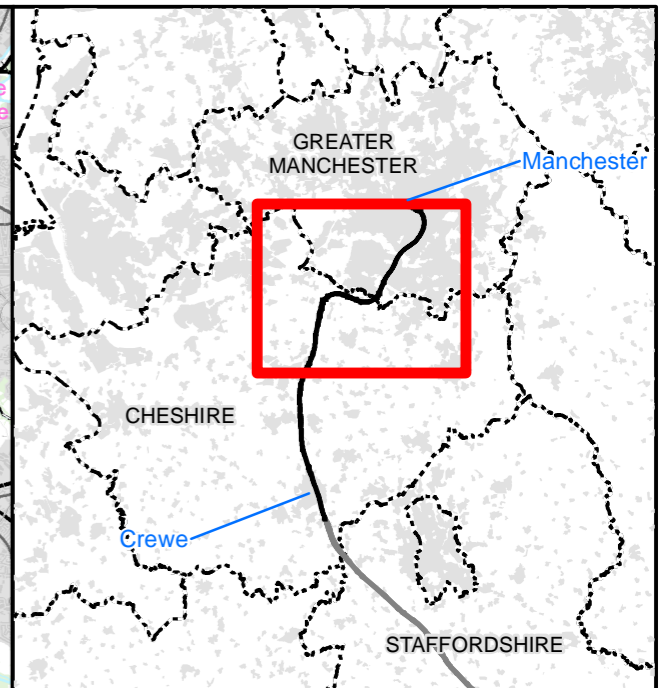
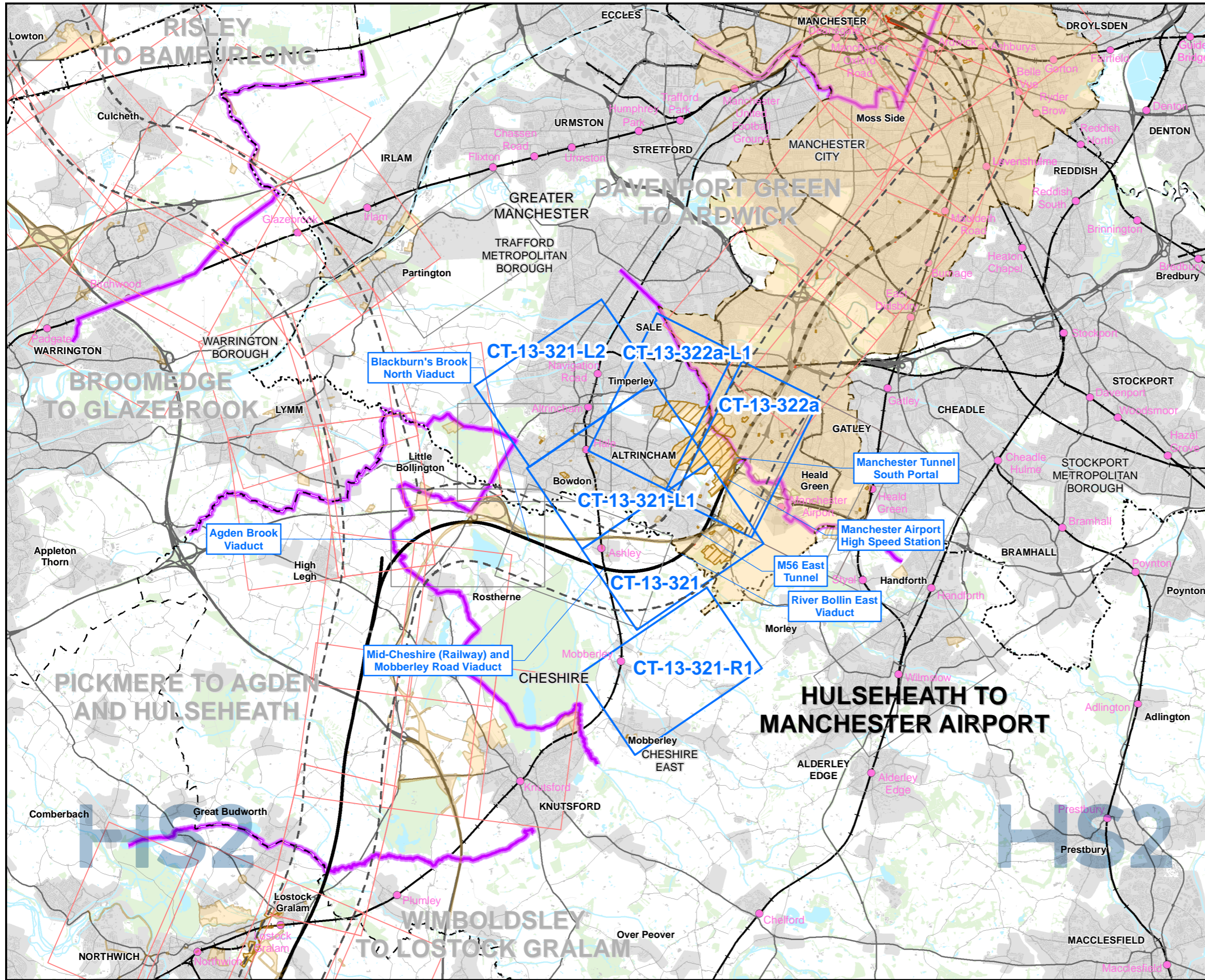
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## **High Speed Rail (Crewe – Manchester)**

### **Supplementary Environmental Statement 2 and Additional Provision 2 Environmental Statement**

MA06: Hulseheath to Manchester Airport  
CT-13 – Committed Developments



**Map Series Information:**

CT-13 series shows the geographical distribution of as yet unimplemented but committed development which is either considered to be largely built out prior to commencement of the Proposed Scheme (future baseline) or those projects that are assumed to largely take place during implementation of the Proposed Scheme (cumulative development). These maps show those committed developments that have been taken into account in the assessment of environmental effects.

**Main Map Legend**

Route in tunnel	Map sheets included in this community area	Area of assessment
Route on surface	Map sheets included in this community area with no amendment	<b>Committed consent and development allocations:</b>
Depot, station, headhouse or portal building	Map sheets not included in this community area	Future Baseline
Community area boundary		Cumulative Development
Existing railway station		<b>New committed consent and development allocations:</b>
County boundary		Future Baseline
District/Borough boundary		Cumulative Development

Map Number	CT-13-INDEX-MA06
Map Name	Index Map of: Committed Developments SES2 and AP2 ES Community Area MA06: Hulseheath to Manchester Airport

**HS2**

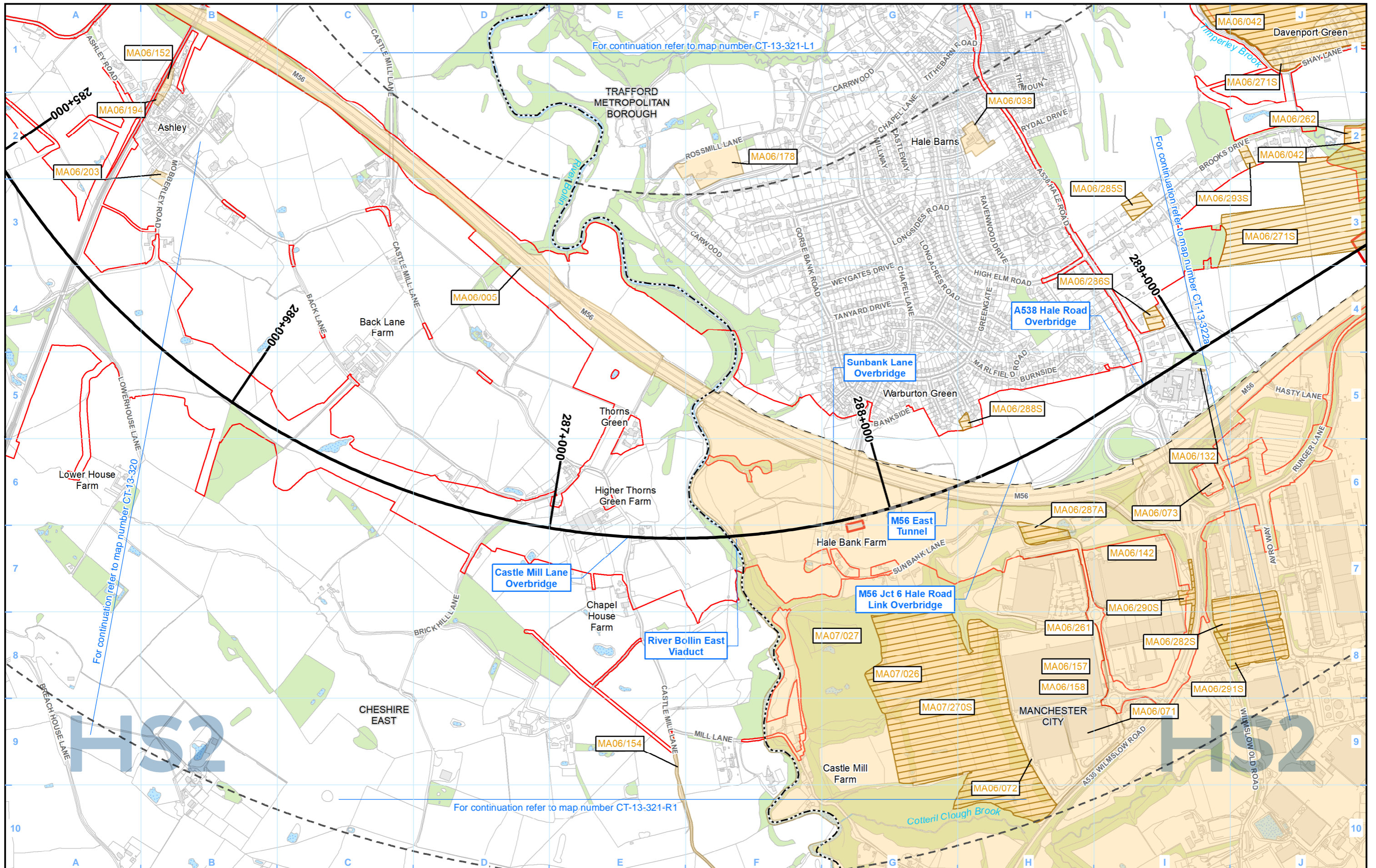
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Metres

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For continuation refer to map number CT-13-321-L1

For continuation refer to map number CT-13-322-R

For continuation refer to map number CT-13-320

For continuation refer to map number CT-13-321-R1

- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary

- Watercourse
- Water body
- Woodland

- Area of assessment
- Committed consent and development allocations:
- Future Baseline
  - Cumulative Development
- New committed consent and development allocations:
- Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number: CT-13-321

Map Name: Committed Developments SES2 and AP2 ES

Community Area MA06: Hulse Heath to Manchester Airport

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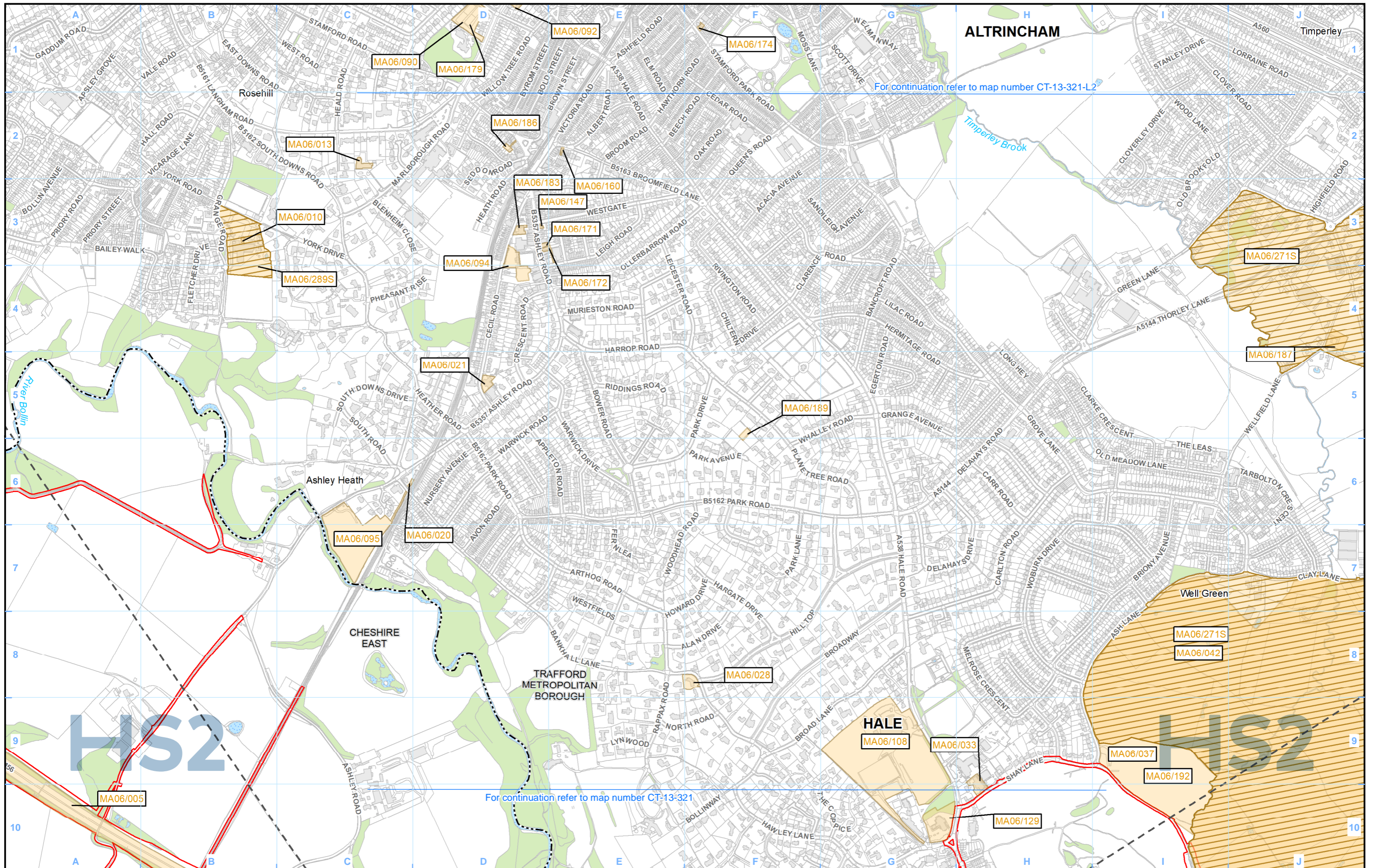
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Date: 05/04/23

Scale at A3: 1:10,000

0 100 200 300 400 Metres





For continuation refer to map number CT-13-321-L2

For continuation refer to map number CT-13-321

**Legend**


Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number **CT-13-321-L1**

Map Name **Committed Developments SES2 and AP2 ES**

Community Area MA06:  
Hulseheath to Manchester Airport

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# BROOMEDGE TO GLAZEBROOK

# HULSEHEATH TO MANCHESTER AIRPORT



For continuation refer to map number CT-13-321-L1

- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary

- Watercourse
- Water body
- Woodland
- Area of assessment
- Committed consent and development allocations:**
- Future Baseline
- Cumulative Development
- New committed consent and development allocations:**
- Future Baseline
- Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number	CT-13-321-L2
Map Name	Committed Developments SES2 and AP2 ES
Community Area MA06: Hulseheath to Manchester Airport	

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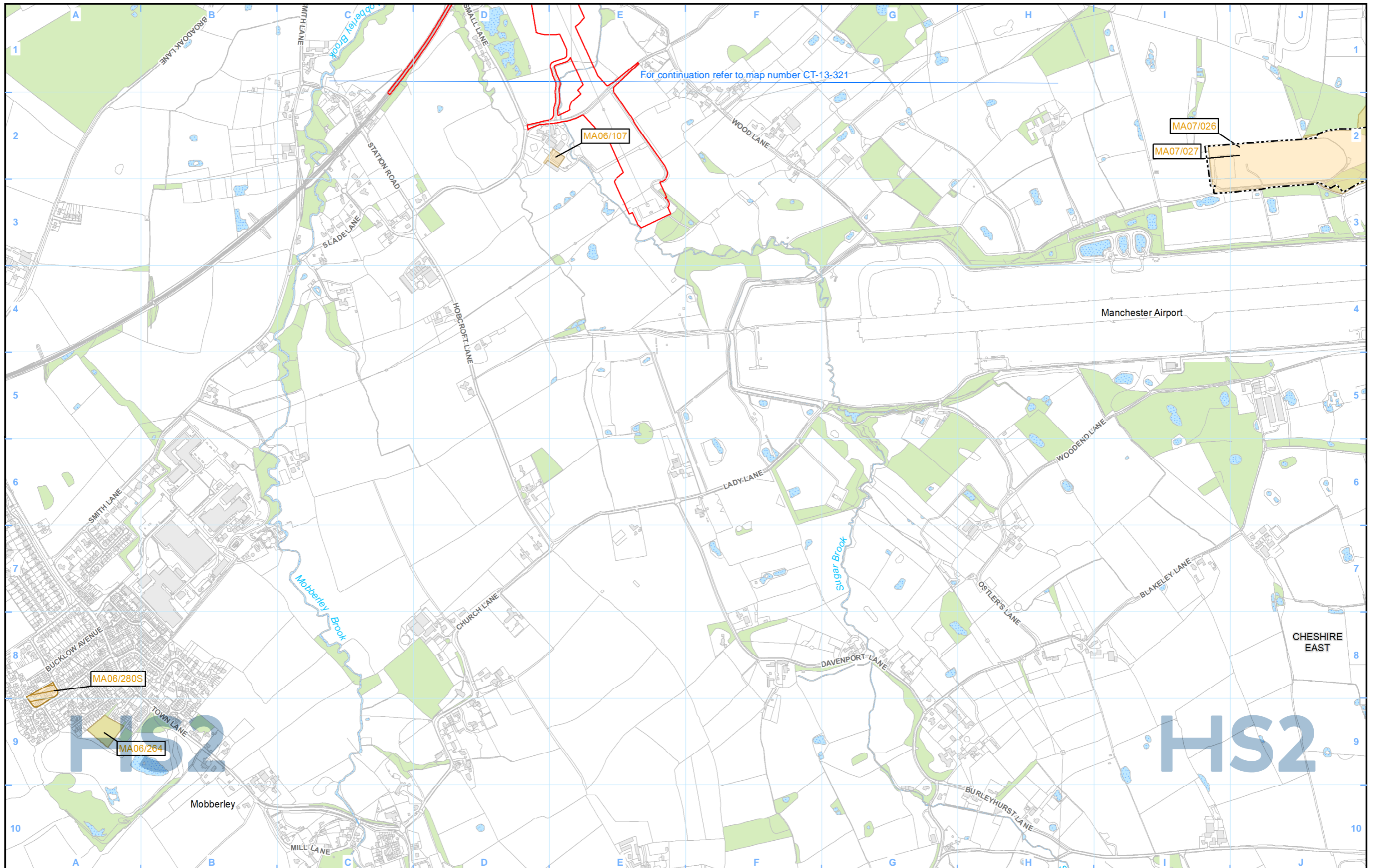
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Date: 05/04/23



For continuation refer to map number CT-13-321

- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary
  - Watercourse
  - Water body
  - Woodland
  - Area of assessment

- Committed consent and development allocations:**
- Future Baseline
  - Cumulative Development
- New committed consent and development allocations:**
- Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number	CT-13-321-R1
Map Name	Committed Developments SES2 and AP2 ES
Community Area MA06: Hulseheath to Manchester Airport	

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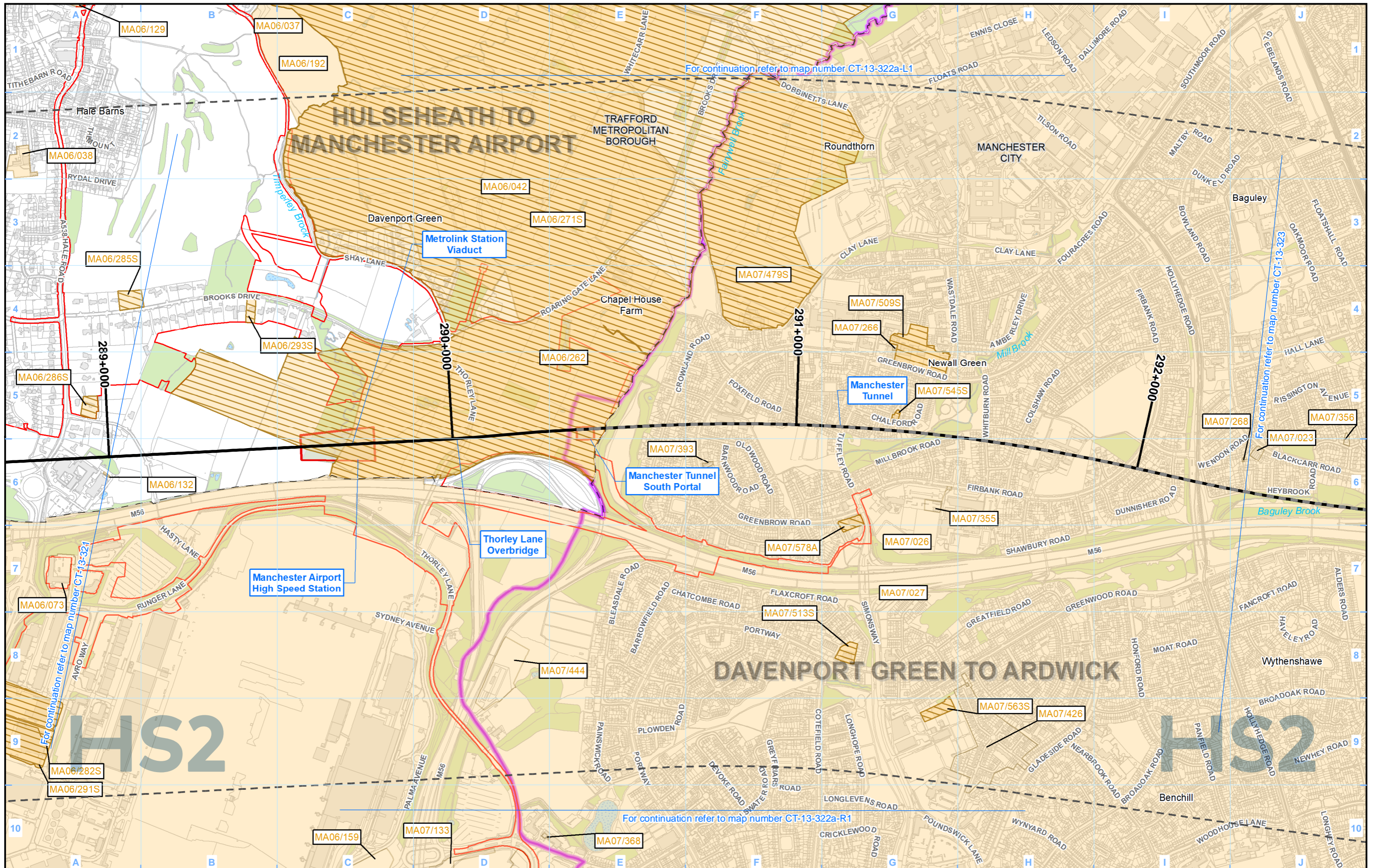
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Scale at A3: 1:10,000

Metres

Date: 05/04/23



- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary

- Watercourse
- Water body
- Woodland
- Area of assessment

- Committed consent and development allocations:
- Future Baseline
  - Cumulative Development
- New committed consent and development allocations:
- Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number: CT-13-322a

Map Name: Committed Developments SES2 and AP2 ES

Community Area MA06: Hulseheath to Manchester Airport

**HS2**

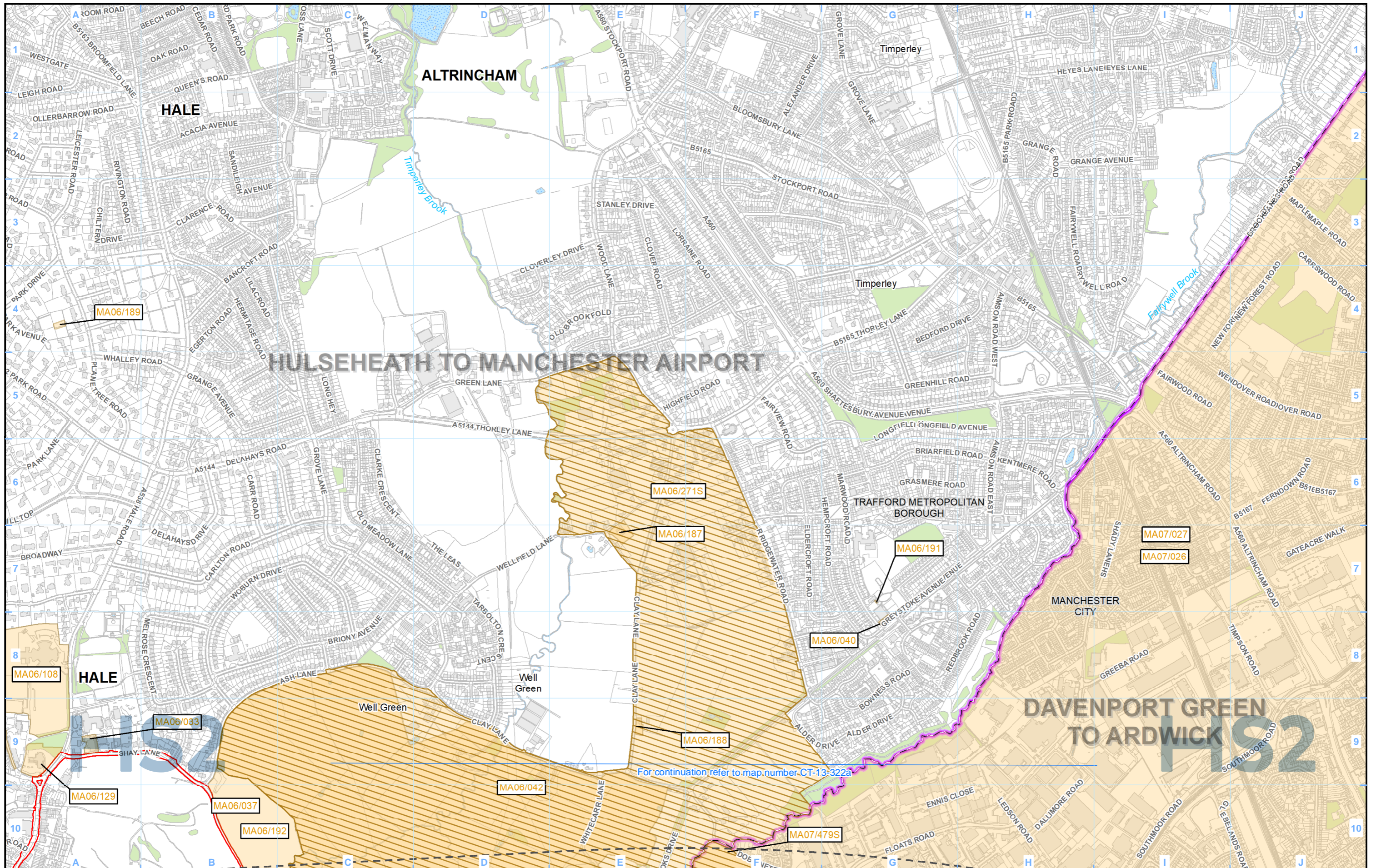
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**Legend**

		<b>Committed consent and development allocations:</b>
		<b>New committed consent and development allocations:</b>

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number **CT-13-322a-L1**

Map Name **Committed Developments SES2 and AP2 ES**

Community Area MA06:  
Hulseheath to Manchester Airport

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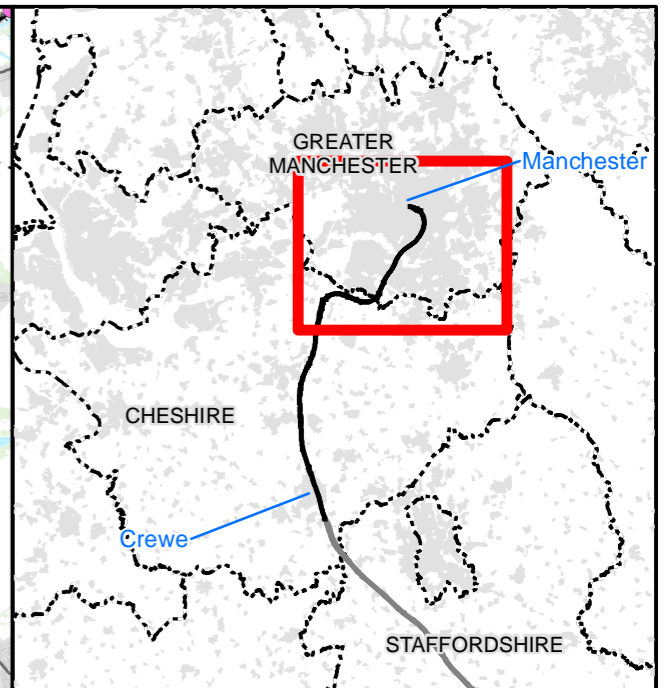
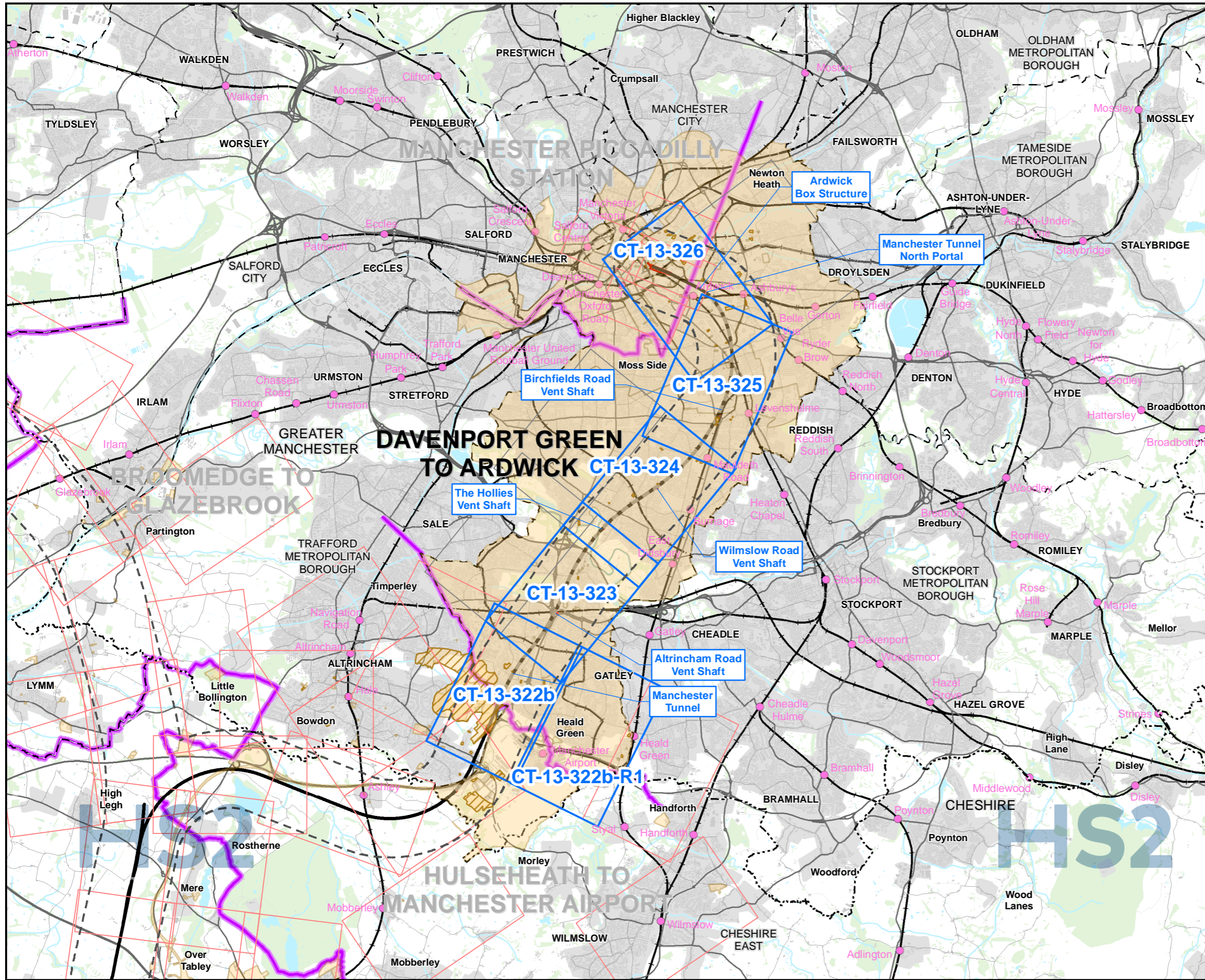
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## **High Speed Rail (Crewe – Manchester)**

### **Supplementary Environmental Statement 2 and Additional Provision 2 Environmental Statement**

MA07: Davenport Green to Ardwick  
CT-13 – Committed Developments



**Map Series Information:**

CT-13 series shows the geographical distribution of as yet unimplemented but committed development which is either considered to be largely built out prior to commencement of the Proposed Scheme (future baseline) or those projects that are assumed to largely take place during implementation of the Proposed Scheme (cumulative development). These maps show those committed developments that have been taken into account in the assessment of environmental effects.

**Main Map Legend**

- Route in tunnel
- Route on surface
- Depot, station, headhouse or portal building
- Community area boundary
- Existing railway station
- County boundary
- District/Borough boundary
- Map sheets included in this community area
- Map sheets included in this community area with no amendment
- Map sheets not included in this community area
- Area of assessment
- Committed consent and development allocations:**
  - Future Baseline
  - Cumulative Development
- New committed consent and development allocations:**
  - Future Baseline
  - Cumulative Development

Map Number	CT-13-INDEX-MA07
Map Name	Index Map of: Committed Developments SES2 and AP2 ES Community Area MA07: Davenport Green to Ardwick

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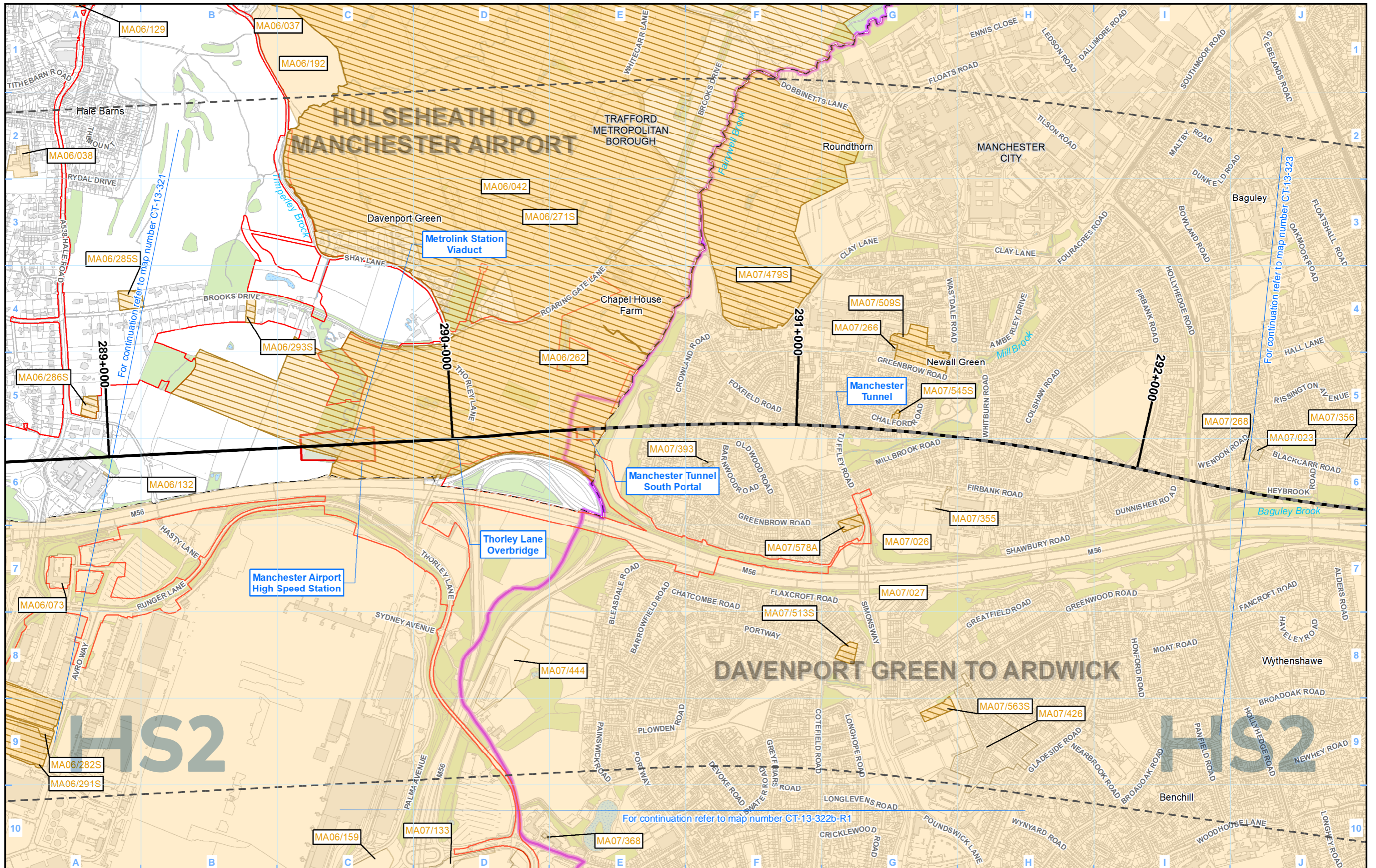
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- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary

- Watercourse
- Water body
- Woodland
- Area of assessment
- Committed consent and development allocations:
  - Future Baseline
  - Cumulative Development
- New committed consent and development allocations:
  - Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number: CT-13-322b

Map Name: Committed Developments SES2 and AP2 ES

Community Area MA07: Davenport Green to Ardwick

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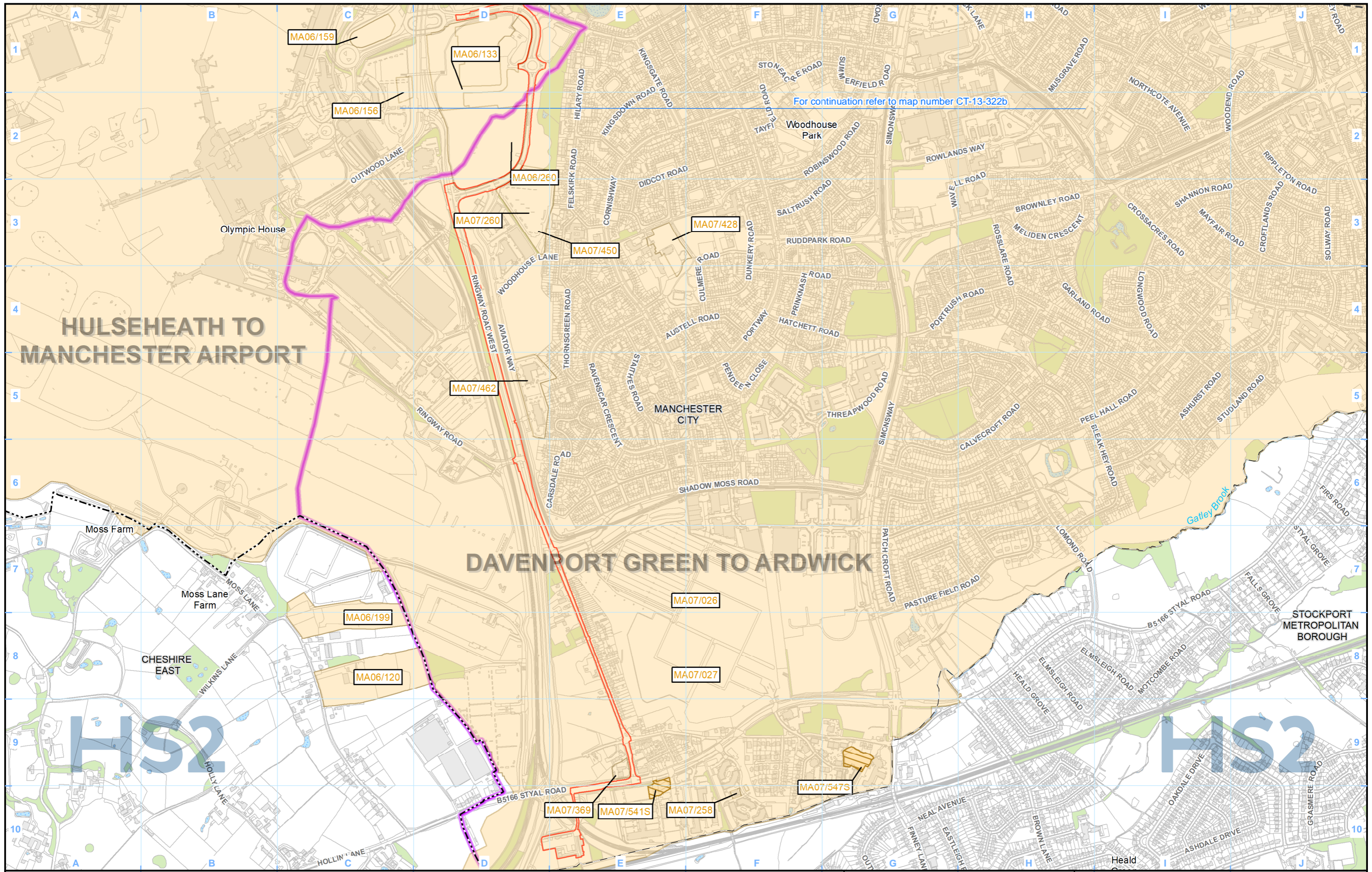
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For continuation refer to map number CT-13-322b

# HULSEHEATH TO MANCHESTER AIRPORT

# DAVENPORT GREEN TO ARDWICK

**Legend**

		<b>Committed consent and development allocations:</b>
		<b>New committed consent and development allocations:</b>

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number	CT-13-322b-R1
Map Name	Committed Developments SES2 and AP2 ES
	Community Area MA07: Davenport Green to Ardwick

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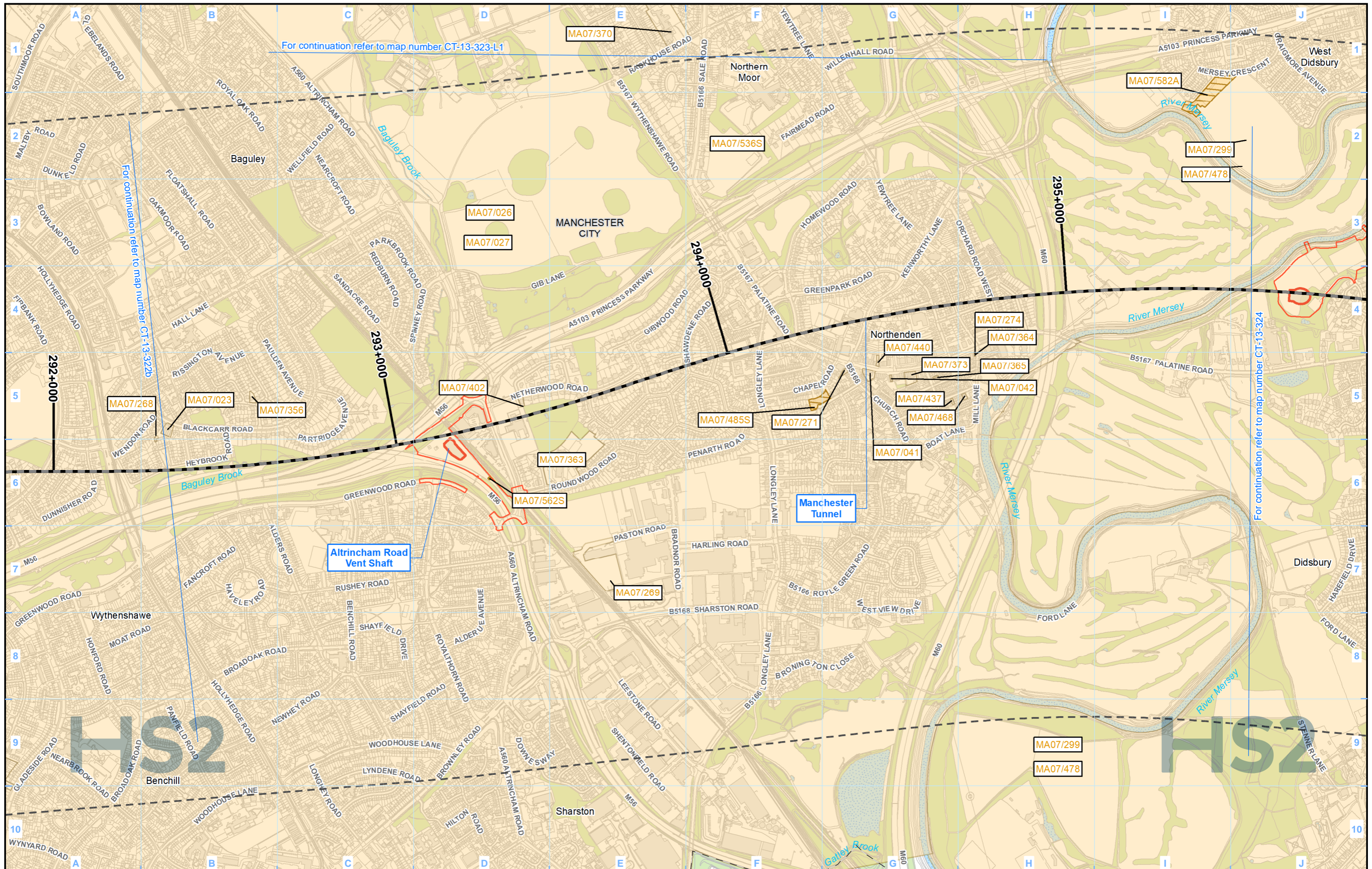
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For continuation refer to map number CT-13-323-L1

For continuation refer to map number CT-13-322b

For continuation refer to map number CT-13-324

- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary
  - Watercourse
  - Water body
  - Woodland
  - Area of assessment
  - Committed consent and development allocations:**
  - Future Baseline
  - Cumulative Development
  - New committed consent and development allocations:**
  - Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number	CT-13-323
Map Name	Committed Developments SES2 and AP2 ES
Community Area MA07: Davenport Green to Ardwick	

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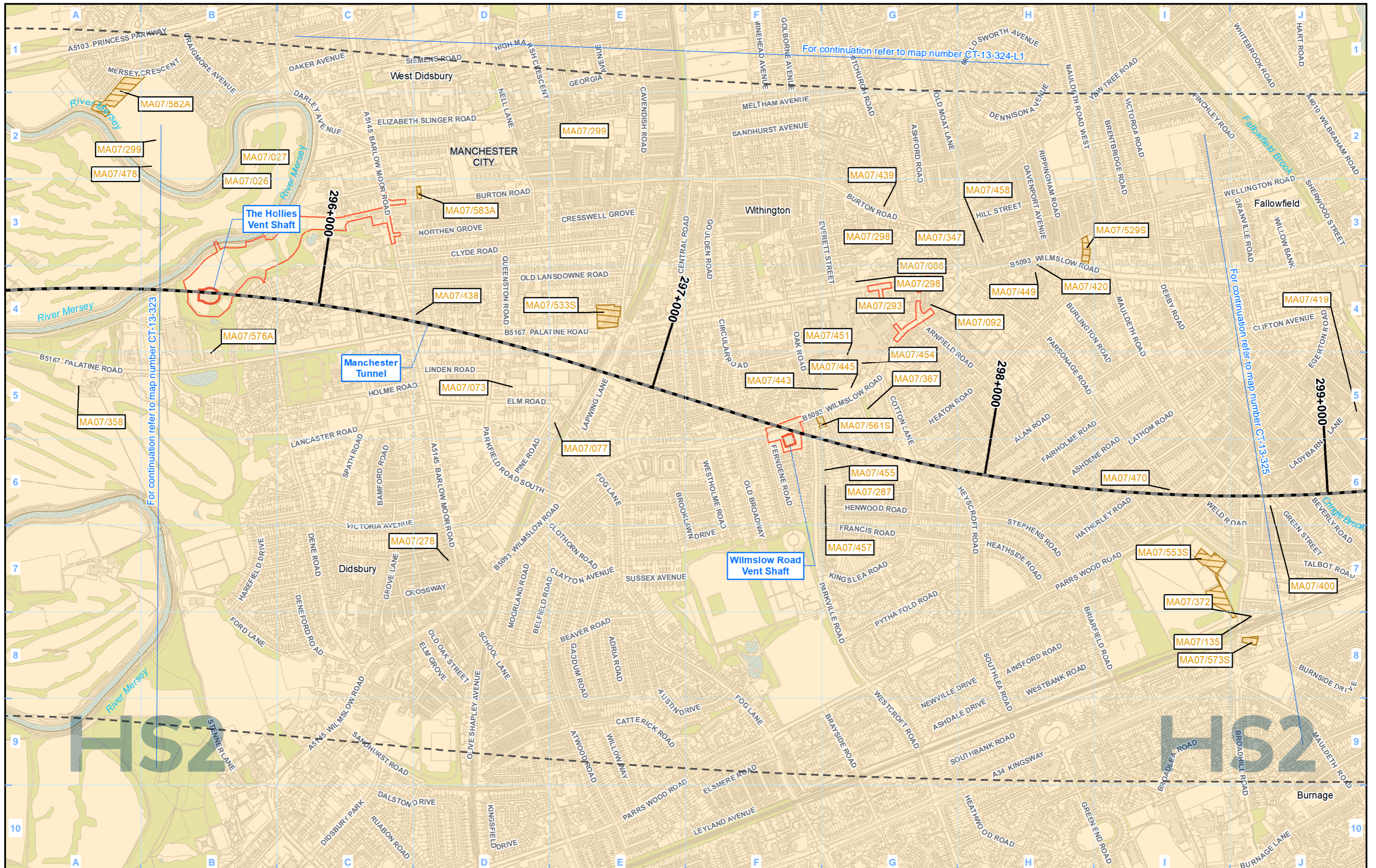
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Metres

Date: 05/04/23



**Legend**


Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number: CT-13-324

Map Name: Committed Developments SES2 and AP2 ES

Community Area MA07: Davenport Green to Ardwick

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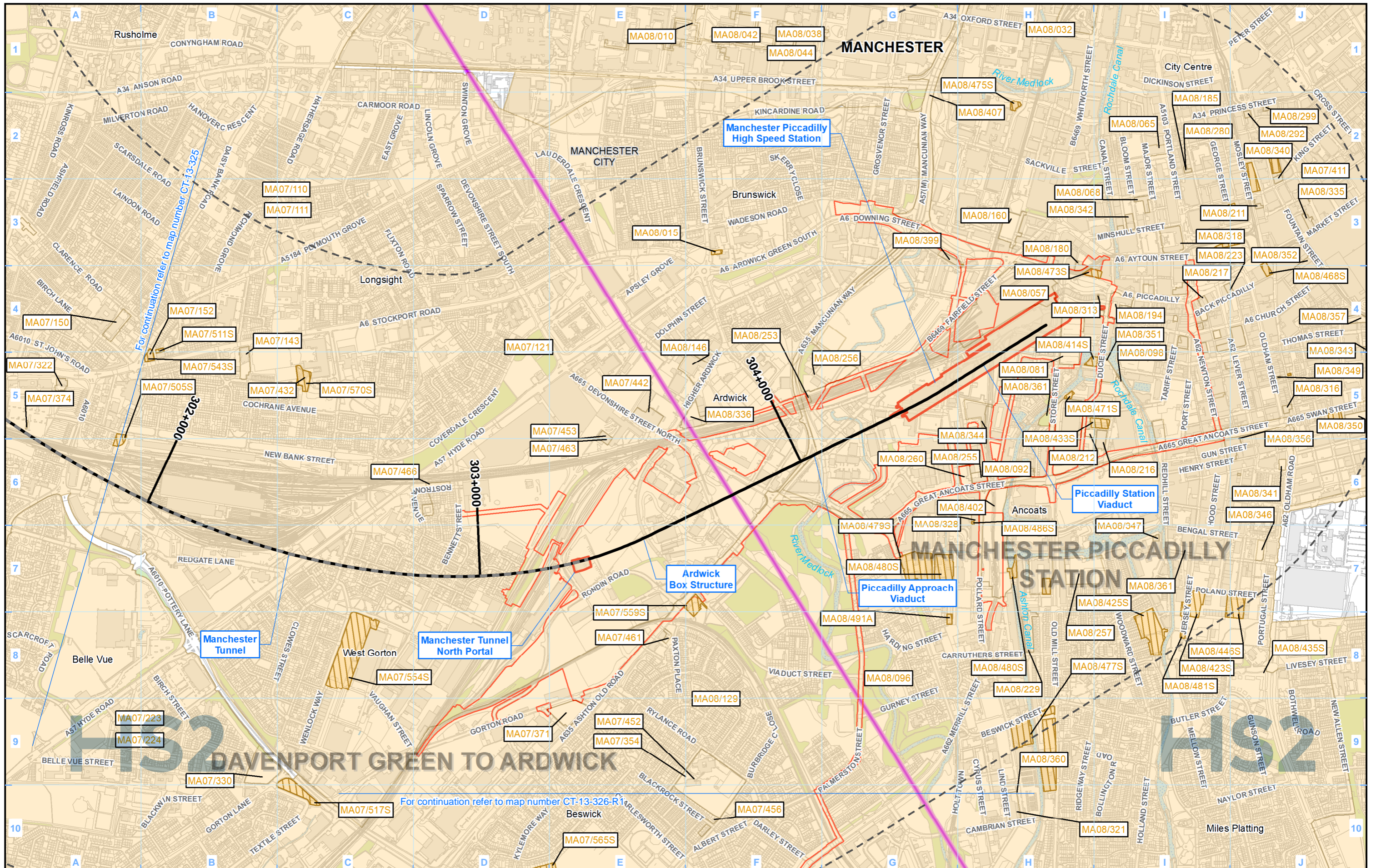
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0 100 200 300 400 Metres





- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary
  - Watercourse
  - Water body
  - Woodland
  - Area of assessment
  - Committed consent and development allocations:
    - Future Baseline
    - Cumulative Development
  - New committed consent and development allocations:
    - Future Baseline
    - Cumulative Development

For continuation refer to map number CT-13-325

For continuation refer to map number CT-13-326-R1

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number: CT-13-326

Map Name: Committed Developments SES2 and AP2 ES

Community Area MA07: Davenport Green to Ardwick

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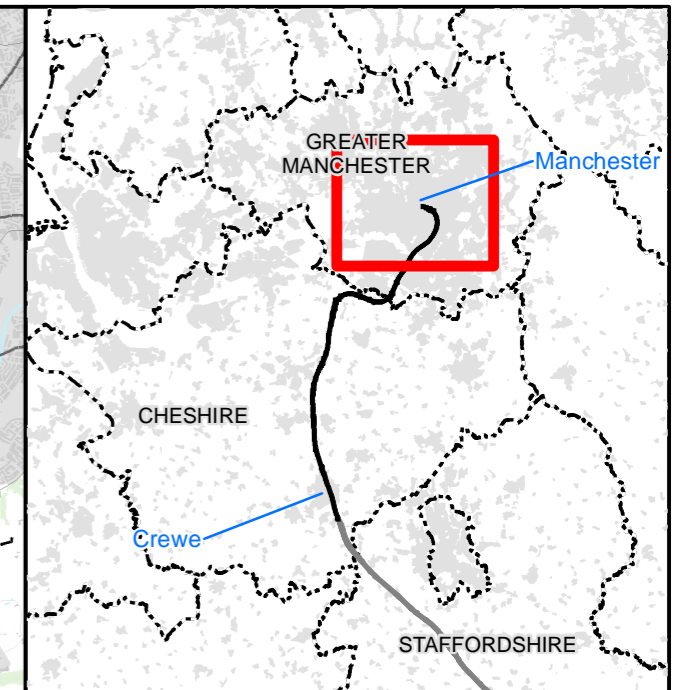
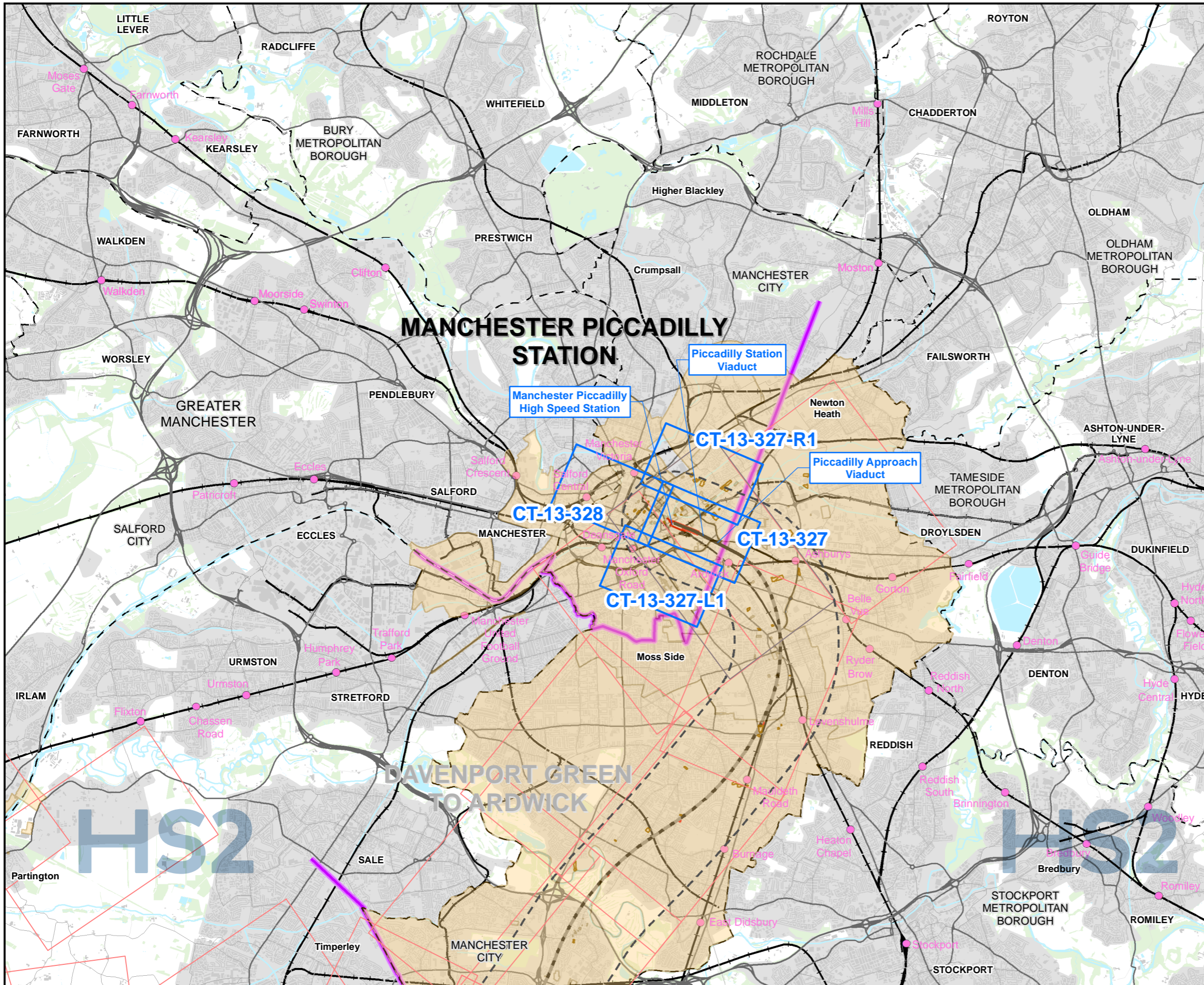
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Date: 06/04/23

## **High Speed Rail (Crewe – Manchester)**

### **Supplementary Environmental Statement 2 and Additional Provision 2 Environmental Statement**

MA08: Manchester Piccadilly Station  
CT-13 – Committed Developments



### Map Series Information:

CT-13 series shows the geographical distribution of as yet unimplemented but committed development which is either considered to be largely built out prior to commencement of the Proposed Scheme (future baseline) or those projects that are assumed to largely take place during implementation of the Proposed Scheme (cumulative development). These maps show those committed developments that have been taken into account in the assessment of environmental effects.

#### Main Map Legend

- |  |  |   |
|--|--|---|
|  |  |   |
|  |  | <b>Committed consent and development allocations:</b>     |
|  |  |   |
|  |  |   |
|  |  | <b>New committed consent and development allocations:</b> |
|  |  |   |
|  |  |   |

Map Number	CT-13-INDEX-MA08
Map Name	Index Map of: Committed Developments SES2 and AP2 ES Community Area MA08: Manchester Piccadilly Station

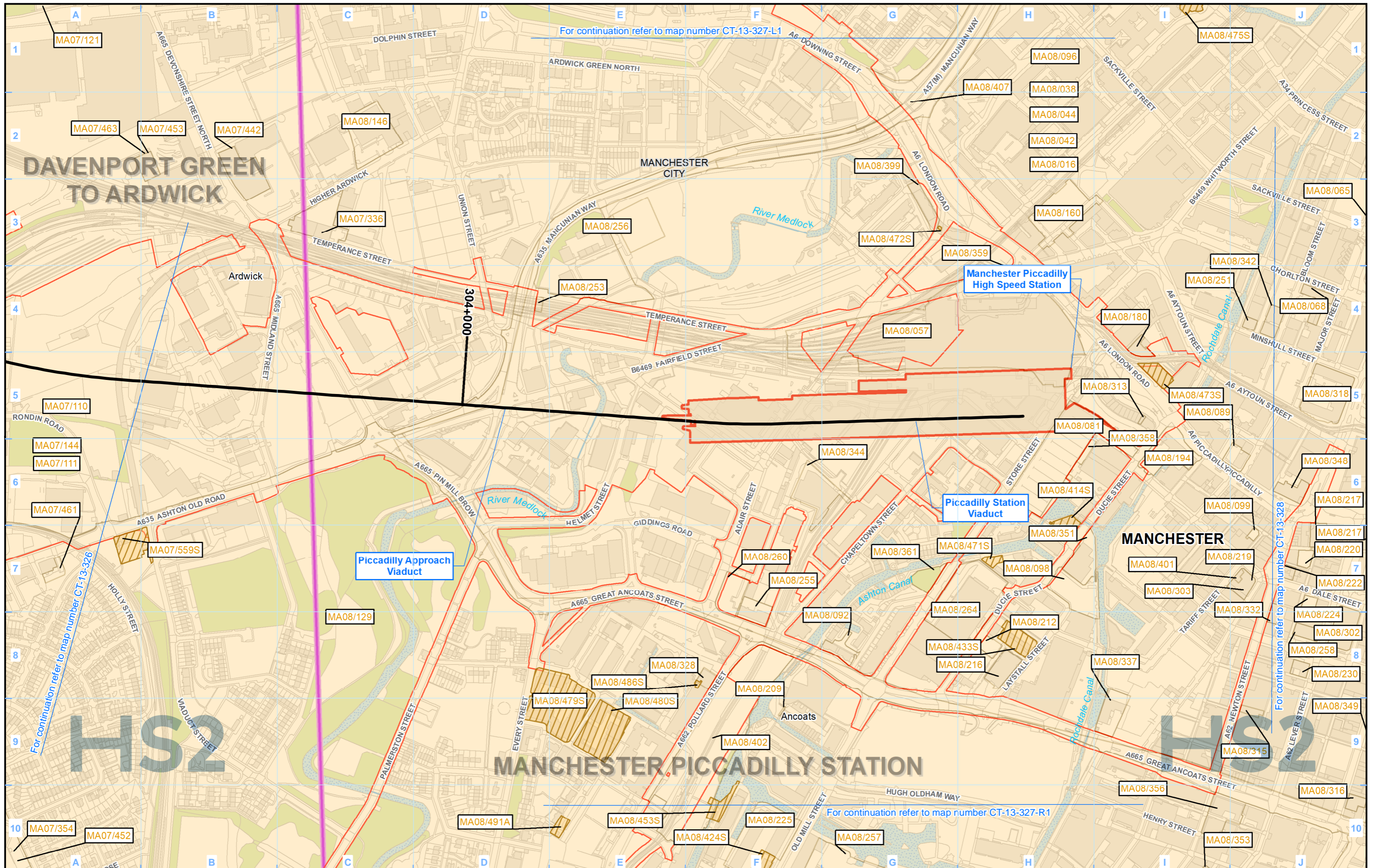
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**Legend**


Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number: CT-13-327

Map Name: Committed Developments SES2 and AP2 ES

Community Area MA08: Manchester Piccadilly Station

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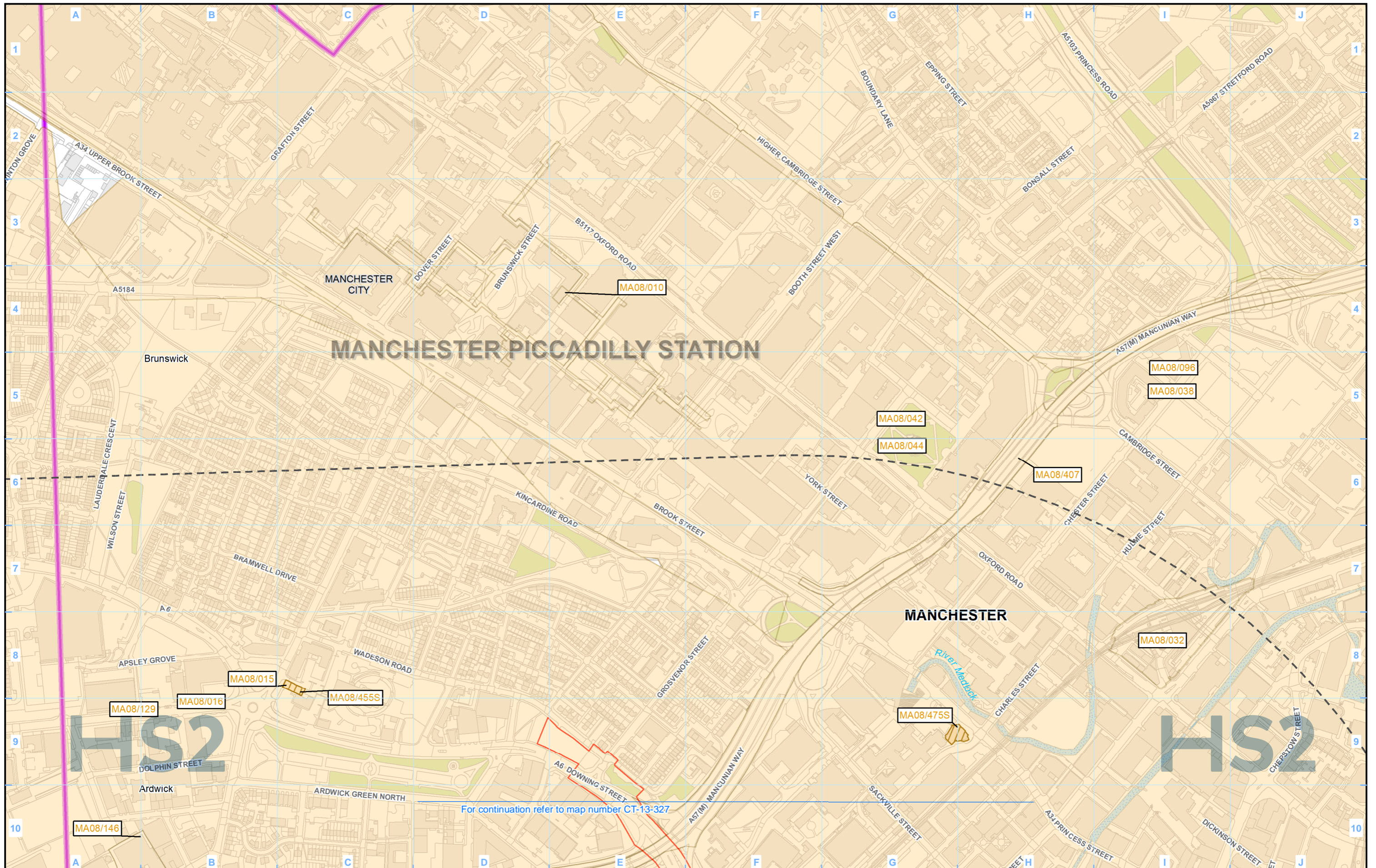
Date: 06/04/23

Scale at A3: 1:5,000

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Date: 06/04/23





- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary

- Watercourse
- Water body
- Woodland

- Area of assessment
- Committed consent and development allocations:
- Future Baseline
  - Cumulative Development
- New consent and development allocations:
- Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number: CT-13-327-L1

Map Name: Committed Developments SES2 and AP2 ES

Community Area MA08: Manchester Piccadilly Station

**HS2**

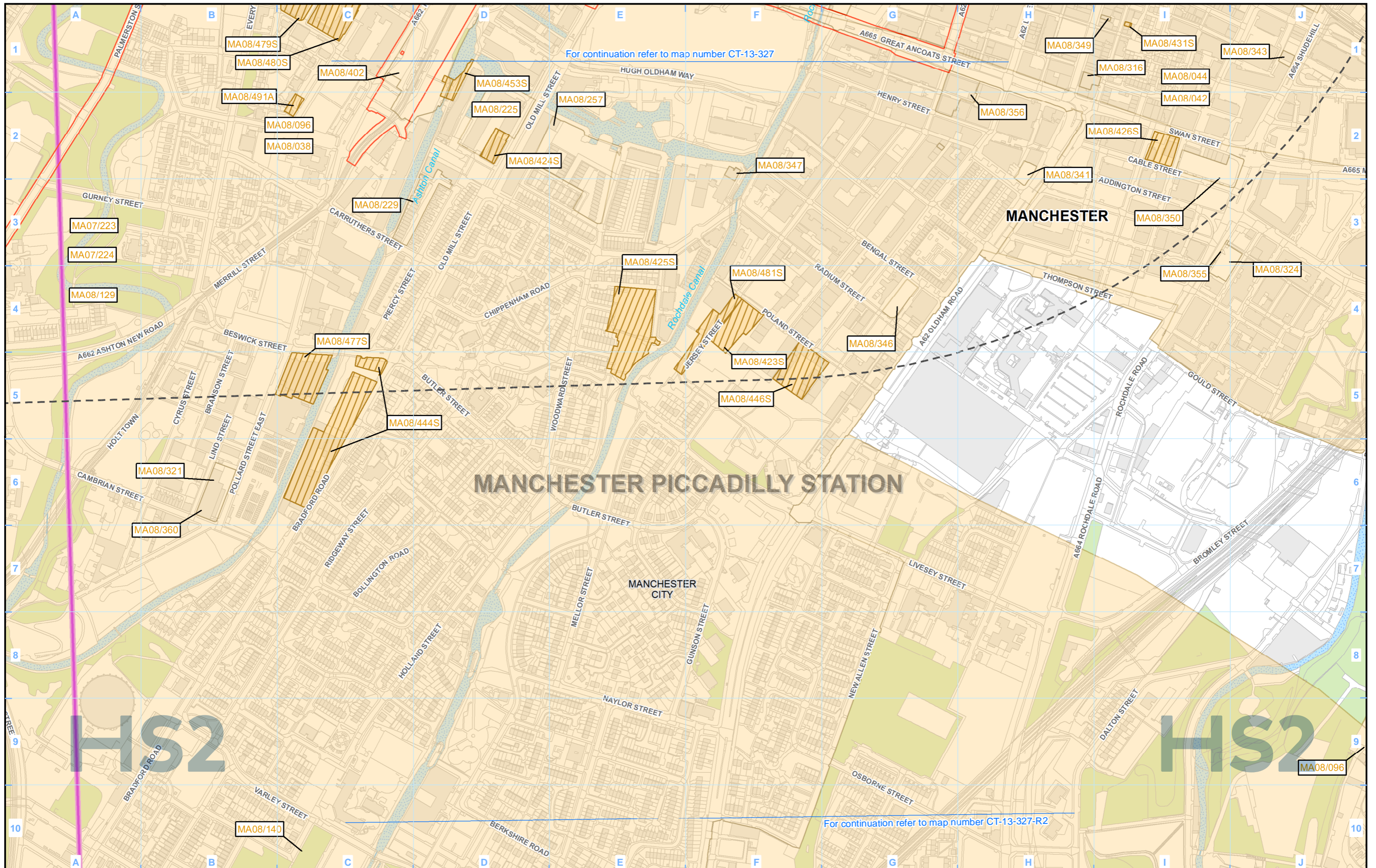
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For continuation refer to map number CT-13-327

For continuation refer to map number CT-13-327-R2

- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary

- Watercourse
- Water body
- Woodland

- Area of assessment**
- Area of assessment
- Committed consent and development allocations:**
- Future Baseline
  - Cumulative Development
- New committed consent and development allocations:**
- Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number	CT-13-327-R1
Map Name	Committed Developments SES2 and AP2 ES
Community Area MA08: Manchester Piccadilly Station	

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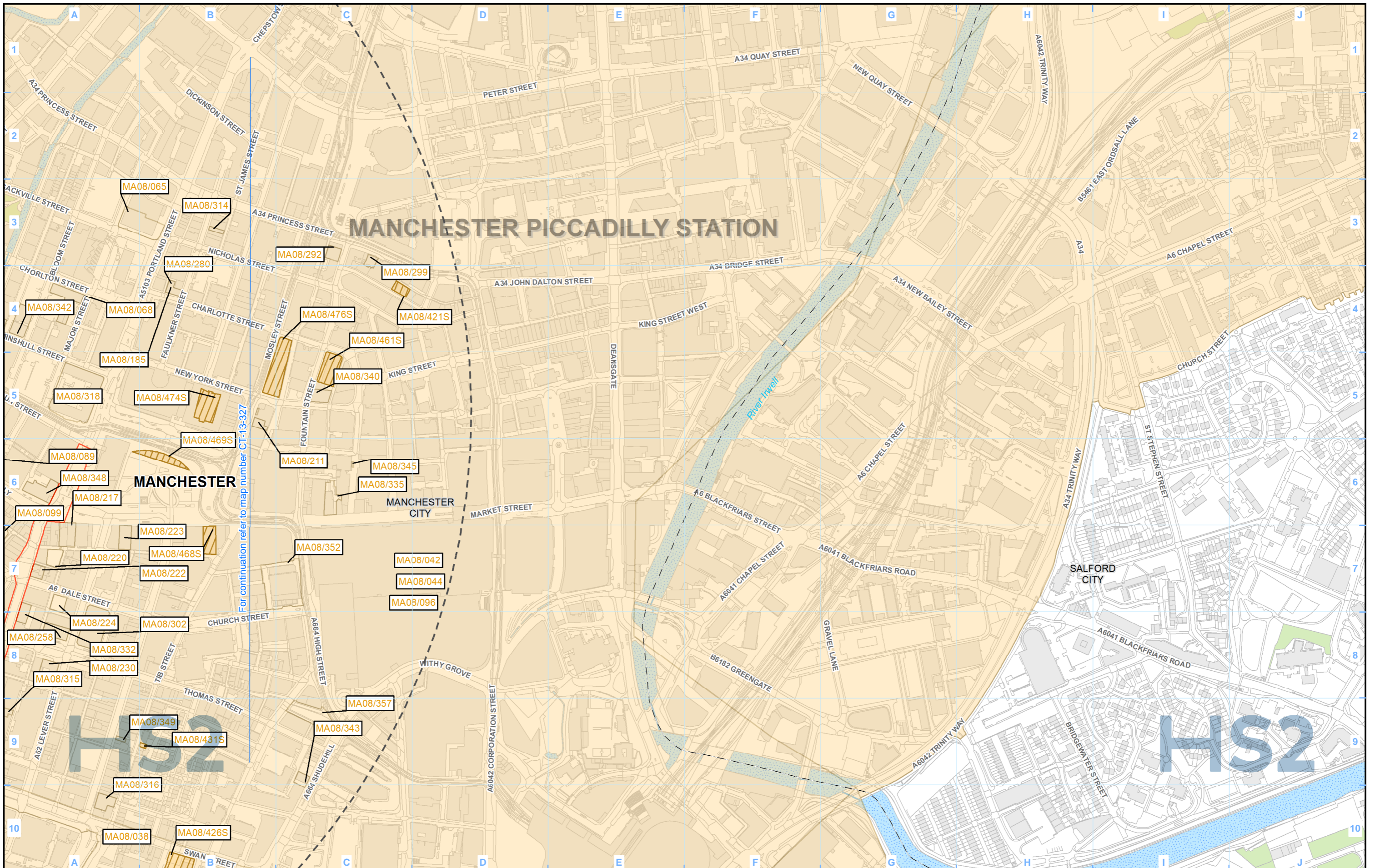
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- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Land potentially required during construction
  - Community area boundary
  - County boundary
  - District/Borough boundary
  - Watercourse
  - Water body
  - Woodland
  - Area of assessment
  - Committed consent and development allocations:**
  - Future Baseline
  - Cumulative Development
  - New committed consent and development allocations:**
  - Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number: CT-13-328

Map Name: Committed Developments SES2 and AP2 ES

Community Area MA08: Manchester Piccadilly Station

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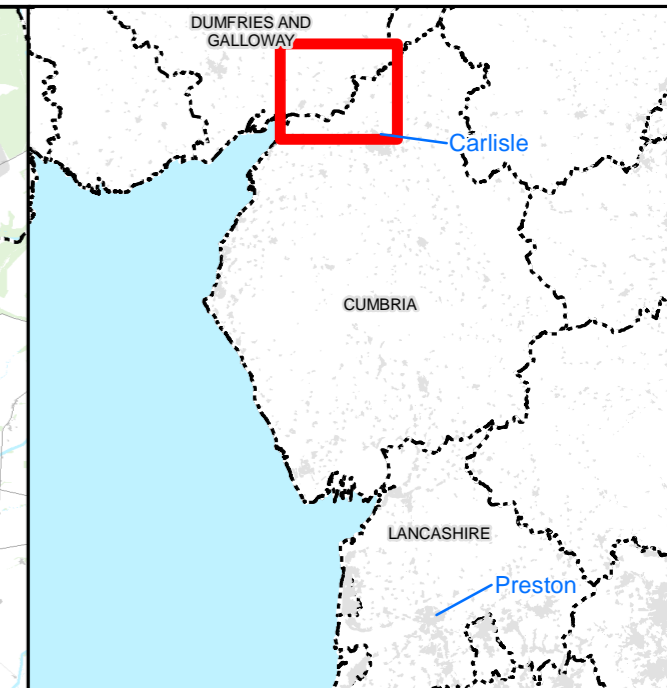
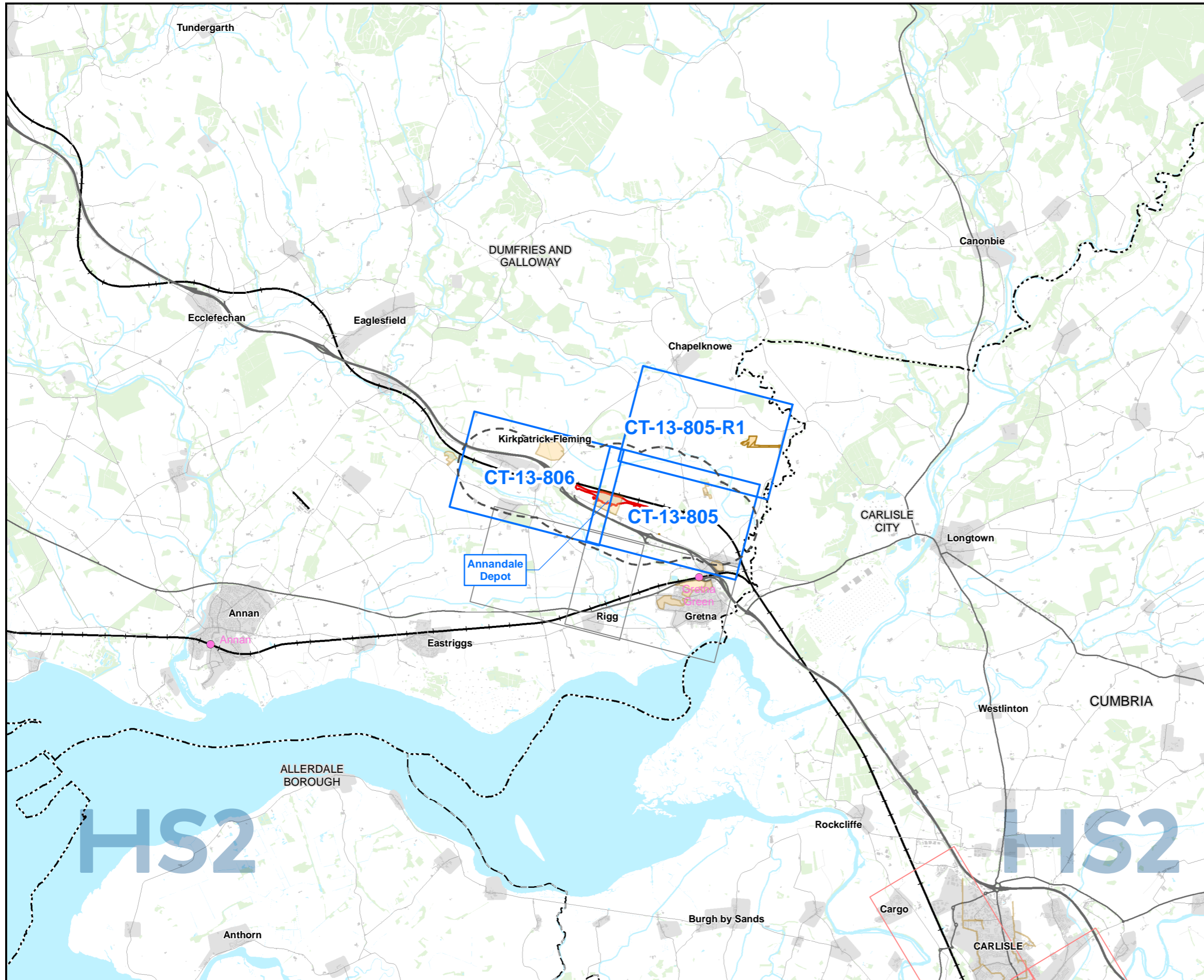
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**High Speed Rail  
(Crewe – Manchester)**

**Supplementary Environmental Statement 2 and  
Additional Provision 2 Environmental Statement**

Annandale Depot

CT-13 - Committed Developments



**Map Series Information:**

CT-13 series shows the geographical distribution of as yet unimplemented but committed development which is either considered to be largely built out prior to commencement of the Proposed Scheme (future baseline) or those projects that are assumed to largely take place during implementation of the Proposed Scheme (cumulative development). These maps show those committed developments that have been taken into account in the assessment of environmental effects.

**Main Map Legend**

Route in tunnel	Area of assessment
Route on surface	<b>Committed consent and development allocations:</b>
Depot, station, headhouse or portal building	Future Baseline
Community area boundary	Cumulative Development
Existing railway station	<b>New committed consent and development allocations:</b>
County/Unitary boundary	Future Baseline
District/Borough boundary	Cumulative Development

Map Number	CT-13-INDEX-ADEP
Map Name	Index Map of: Committed Developments SES2 and AP2 ES
	Off-route ADEP: Annandale Depot

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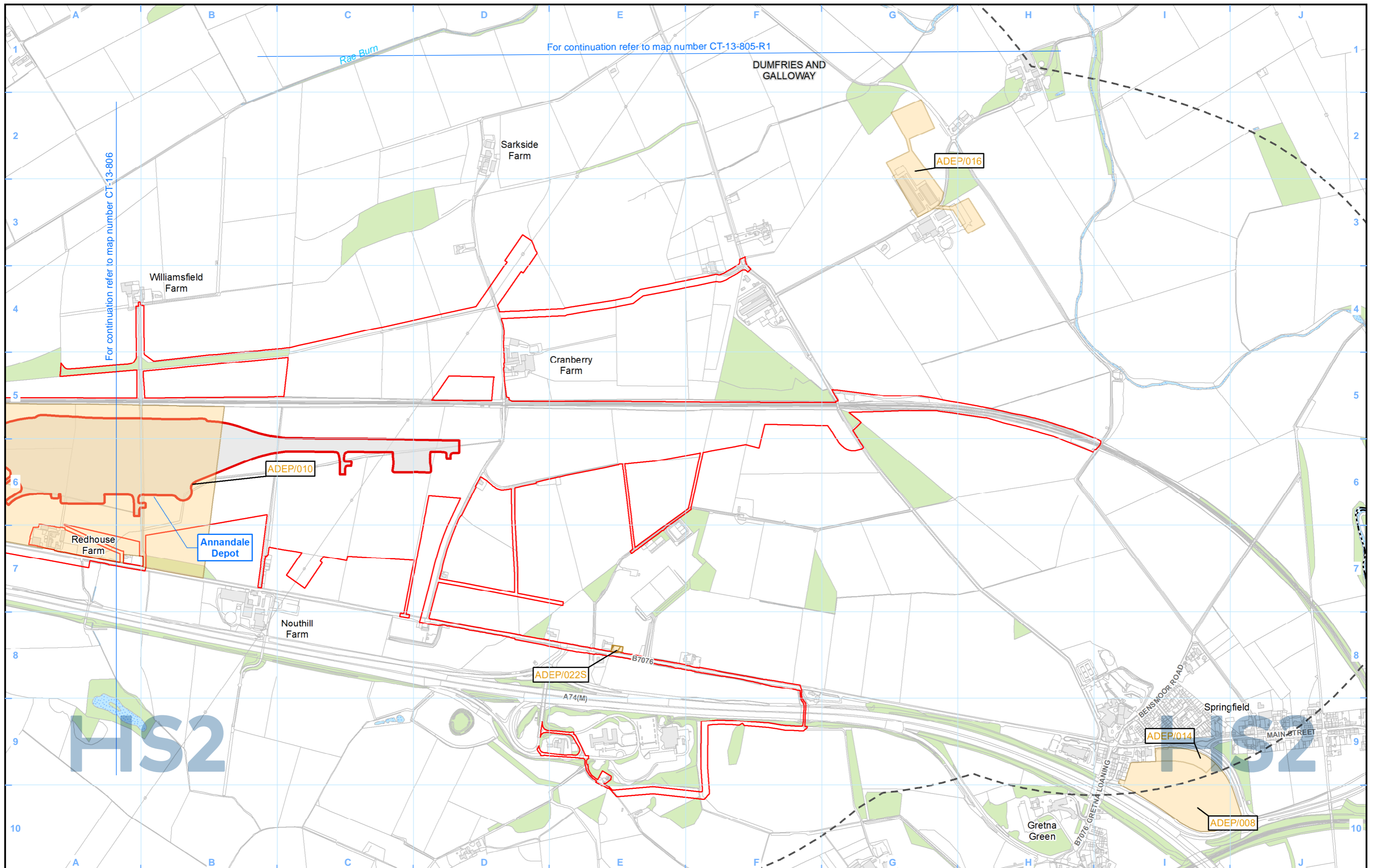
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Metres

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Note: Not all data layers in the legend are represented on every map.



For continuation refer to map number CT-13-805-R1

For continuation refer to map number CT-13-806

- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Community area boundary
  - County/Unitary Authority boundary
  - District/Borough boundary

- Watercourse
- Water body
- Woodland

- Area of assessment
- Committed consent and development allocations:
  - Future Baseline
  - Cumulative Development
- New committed consent and development allocations:
  - Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number: CT-13-805

Map Name: Committed Developments SES2 and AP2 ES

Community Area ADEP: Annandale Depot

**HS2**

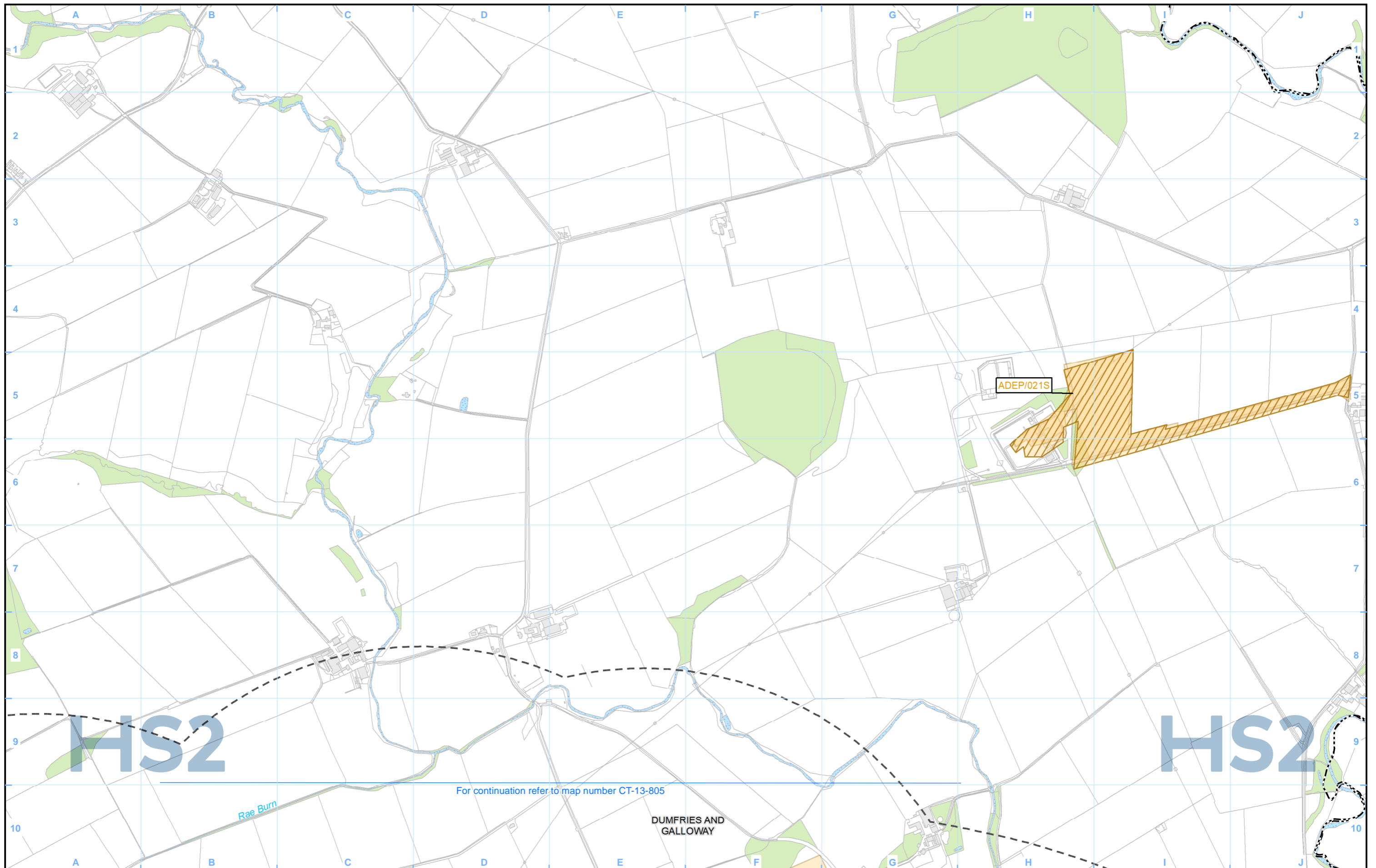
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- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Community area boundary
  - County/Unitary Authority boundary
  - District/Borough boundary

- Watercourse
- Water body
- Woodland

- Area of assessment
- Committed consent and development allocations:**
- Future Baseline
- Cumulative Development
- New committed consent and development allocations:**
- Future Baseline
- Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number **CT-13-805-R1**

Map Name **Committed Developments SES2 and AP2 ES**

Community Area ADEP: Annandale Depot

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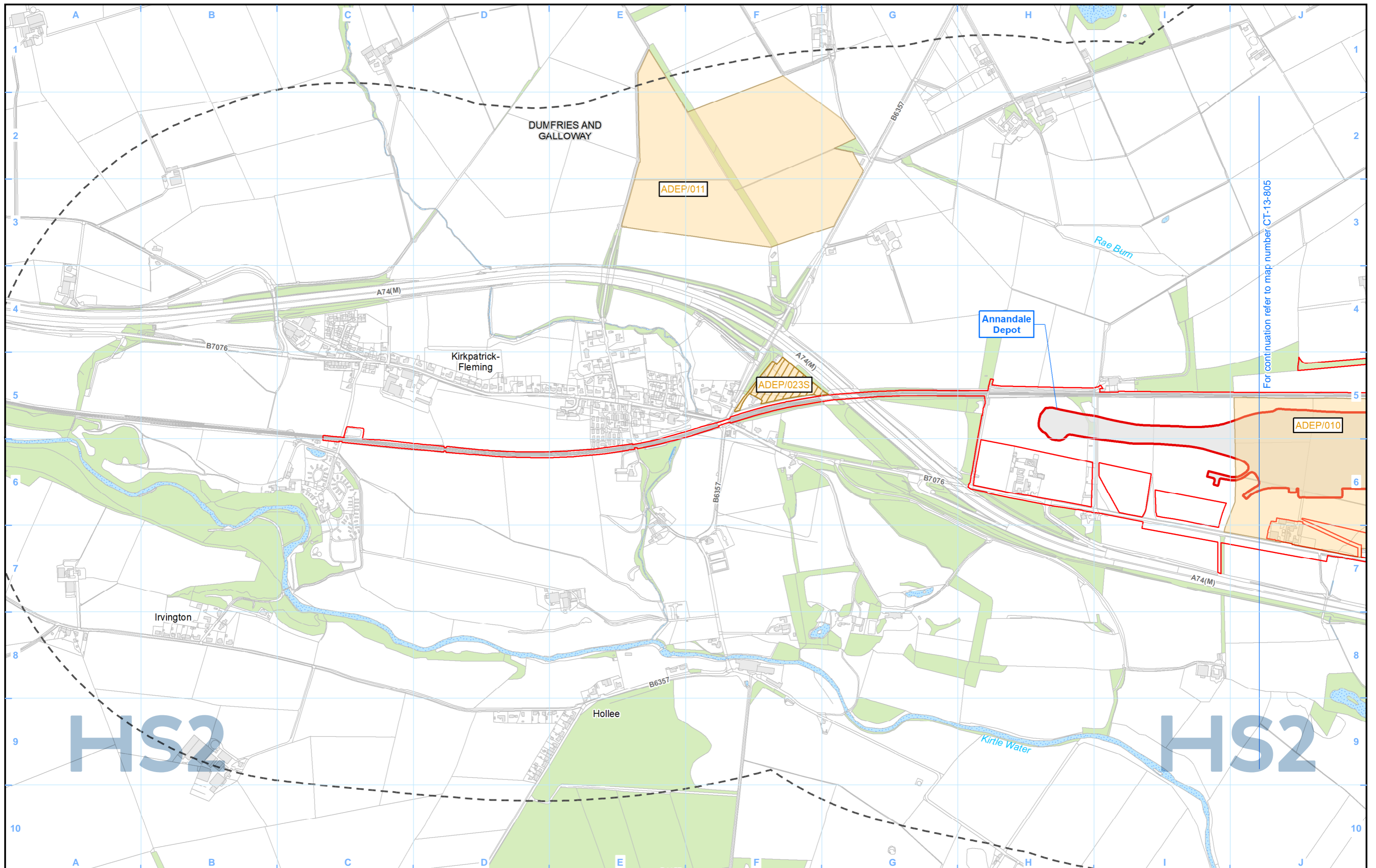
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For continuation refer to map number CT-13-805

- Legend**
- Route in tunnel
  - Route on surface
  - Depot, station, headhouse or portal building
  - Community area boundary
  - County/Unitary Authority boundary
  - District/Borough boundary

- Watercourse
- Water body
- Woodland

- Area of assessment
- Committed consent and development allocations:**
  - Future Baseline
  - Cumulative Development
- New committed consent and development allocations:**
  - Future Baseline
  - Cumulative Development

Future baseline is unimplemented but committed development which is likely to be largely built out prior to commencement of the Proposed Scheme.

Cumulative development is unimplemented but committed development which is likely to be largely built out during implementation of the Proposed Scheme and give rise to potential cumulative effects, when considered alongside the Proposed Scheme.

Map Number  
**CT-13-806**

Map Name  
**Committed Developments  
SES2 and AP2 ES**

Community Area ADEP:  
Annandale Depot

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