



# England Coast Path Stretch: Cleveleys to Pier Head, Liverpool

**Report CPH 1: South Promenade (Kingsway), Cleveleys to Brades Lane, Freckleton**

## Part 1.1: Introduction

<b>Start Point:</b>	<b>South Promenade (Kingsway), Cleveleys (Grid reference SD 3122 4267)</b>
<b>End Point:</b>	<b>Brades Lane, Freckleton (Grid reference SD 4374 2939)</b>
<b>Relevant Maps:</b>	<b>CPH 1a to CPH 1r</b>

1.1.1 This is one of a series of linked but legally separate reports published by Natural England under section 51 of the National Parks and Access to the Countryside Act 1949, which make proposals to the Secretary of State for improved public access along and to this stretch of coast between Cleveleys to Pier Head, Liverpool.

1.1.2 This report covers length CPH 1 of the stretch, which is the coast between South Promenade (Kingsway), Cleveleys to Brades Lane, Freckleton. It makes free-standing statutory proposals for this part of the stretch, and seeks approval for them by the Secretary of State in their own right under section 52 of the National Parks and Access to the Countryside Act 1949.

1.1.3 The report explains how we propose to implement the England Coast Path (“the trail”) on this part of the stretch, and details the likely consequences in terms of the wider ‘Coastal Margin’ that will be created if our proposals are approved by the Secretary of State. Our report also sets out:

- any proposals we think are necessary for restricting or excluding coastal access rights to address particular issues, in line with the powers in the legislation; and
- any proposed powers for the trail to be capable of being relocated on particular sections (“roll-back”), if this proves necessary in the future because of coastal change.

1.1.4 There is also a single Overview document for the whole of this stretch of coast, explaining common principles and background. **This and the other individual reports relating to the stretch should be read in conjunction with the Overview. The Overview explains, among other things, how we have considered any potential environmental impacts of improving public access to this part of the coast, and this report, and other separately published assessments we refer to, then provides more detail on these aspects where appropriate.**

## Part 1.2: Proposals Narrative

### The trail:

1.2.1 Generally follows existing walked routes, including public rights of way, pavements and promenades along most of this length.

1.2.2 Mainly follows the coastline quite closely and maintains good views of the sea.

1.2.3 Includes five sections of new path, in one location at Brook Cottage. See map CPH 1n and associated tables below for details.

1.2.4 Is aligned on the beach or foreshore in two locations at Lytham St. Anne's. In both cases we believe this to be the best option. See Table 1.3.1 and maps CPH 1i and 1j for details.

1.2.5 Follows a route similar to the existing Lancashire Coastal Way but departs from this in places in order to maintain good views of the sea.

### Protection of the environment:

In this part of the report, we explain how we have taken account of environmental protection objectives in developing our proposals for improved coastal access.

1.2.6 The following designated sites affect this length of coast:

- Liverpool Bay Special Protection Area (SPA)
- Ribble and Alt Estuaries SPA
- Ribble and Alt Estuaries Ramsar site
- Ribble Estuary Site of Special Scientific Interest (SSSI), for its wildlife interest
- Lytham St Anne's Dunes SSSI, for its wildlife interest
- Lytham Coastal Changes SSSI, for its geological interest
- Ribble Estuary Marine Conservation Zone (MCZ)

Map C in the Overview shows the extent of designated areas along this stretch of coast, including SPAs, SSSIs and Ramsar sites.

The following table brings together design features included in our access proposals to help to protect the environment along this length of the coast.

#### 1.2.7 Measures to protect the environment

Map(s)	Route section number(s)	Design features of the access proposals	Reason included
CPH 1r	CPH-1-S132 to CPH-1-143	The following design features are described elsewhere in this report: <ul style="list-style-type: none"><li>■ The trail is aligned inland (see table 1.3.3).</li></ul>	To reduce the risk of disturbance of roosting, breeding and feeding waterbirds.

Map(s)	Route section number(s)	Design features of the access proposals	Reason included
		<ul style="list-style-type: none"> <li>■ Coastal access rights would be excluded over the land to the east of Warton Aerodrome at Freckleton for a combination of reasons. (See restrictions and exclusions section starting at paragraph 1.2.16 and associated Direction Maps).</li> </ul> <p>In addition, we will install:</p> <ul style="list-style-type: none"> <li>■ A sign at Marsh Gates, Freckleton, explaining the sensitivity and showing a map of the access exclusion.</li> </ul>	

1.2.8 Natural England is satisfied that the proposals for coastal access in this report are made in accordance with relevant environmental protection legislation. For more information about how we came to this conclusion; see the following assessments of the access proposals that we have published separately:

- A Habitats Regulations Assessment relating to any potential impact on the conservation objectives of European sites.
- Our Nature Conservation Assessment, in which we document our conclusions in relation to other potential impacts on nature conservation.

**Part 6b of the Overview includes some contextual information about protecting the environment along this length of coast.**

### Accessibility:

1.2.9 There are few artificial barriers to accessibility on the proposed route. However, the natural coastal terrain is often challenging for people with reduced mobility and this is the case on sections of our proposed route because:

- The trail would follow an uneven grass or bare soil path along the cliff tops and embankments, through agricultural land, marsh areas and along sections of beach or foreshore.
- There are places where it would be necessary to ascend/descend steps over steeper ground. Any new steps will be installed before the new access rights come into force as part of the physical establishment work described below.

1.2.10 All existing stiles will be replaced with gates, so as to make them easier to use. We envisage this happening before the new access rights come in as part of the physical establishment work described below.

1.2.11 It is possible to avoid some of the barriers mentioned above by following other existing walked routes or public rights of way. Where possible, we will aim to work with local authorities to indicate other paths that might be followed by those with reduced mobility, in order to bypass steps or other obstacles.

**See part 6a of the Overview - 'Recreational issues' - for more information.**

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## Where we have proposed exercising statutory discretions:

1.2.12 **Estuary:** This report proposes that the trail should contain sections aligned on the estuary of the River Ribble, extending upstream from the open coast. Natural England proposes to exercise its functions as if the sea included the estuarial waters of that river as far as Penwortham New Road Bridge, Preston, as indicated by the extent of the trail shown on maps CPH 1j to 1r. The trail covered in this report includes part of the Ribble Estuary, the remaining part of the Ribble Estuary route is covered in reports CPH 2, CPH 3 and CPH 4.

**See part 5 of the Overview for a detailed analysis of the options considered for this estuary and our resulting proposals.**

1.2.13 **Landward boundary of the coastal margin:** We have used our discretion on some sections of the route to map the landward extent of the coastal margin to an adjacent physical boundary such as a wall, fence line, hedge or edge of pavement, promenade, path or track to make the extent of the new access rights clearer. See Table 1.3.1 below.

1.2.14 Along the promenade from North Shore to Starr Gate at Blackpool, at Lytham Green and communal areas running parallel to Victory Boulevard at Lytham St. Anne's, we have used our discretion to propose the inclusion of additional, more extensive landward areas within the coastal margin, to secure or enhance public enjoyment of this part of the coast. The owners of these areas of land are content for us to propose this.

1.2.15 The Proposals Tables show where we are proposing to alter the default landward boundary of the coastal margin. These proposals are set out in columns 5b and 5c of table 1.3.1. Where these columns are left blank, we are making no such proposals, so the default landward boundary applies. See the note relating to Columns 5b & 5c [above Table 1.3.1] explaining what this means in practice.

**See also part 3 of the Overview - 'Understanding the proposals and accompanying maps', for a more detailed explanation of the default extent of the coastal margin and how we may use our discretion to adjust the margin, either to add land or to provide clarity.**

1.2.16 **Restrictions and/or exclusions:** We have proposed to exclude access by direction under the Countryside and Rights of Way Act (2000) in certain places along this section of coast.

*Exclusion of access for land management purposes.*

1.2.17 Land to the east of Warton Aerodrome at Freckleton is within the flight path of the main runway. This area is known to be a haven for wild birds, including birds that are features of the designated sites and feral birds (Canada geese in particular). Airfield Operations staff monitor bird numbers to ensure they will not impact on the operation of the airfield. If there were to be a significant increase in access to this area, we believe that people and dogs could disturb the birds and pose an air safety risk to the operation of the airfield. Therefore the following exclusion is proposed:

1.2.18 Access to the land in the coastal margin seaward of route sections CPH-1-S132 to CPH-1-S143 is to be excluded all-year round by direction, under section 24 of the Countryside and Rights of Way Act (2000), for the purpose of land management. The exclusion does not affect the route itself and will have no legal effect on land where coastal access rights do not apply. See Directions Map CPH 1A.

1.2.19 From early spring to early autumn, cattle (up to 40 beef heifers and steers) are grazed in fields adjacent to the marsh at Naze Point. From early autumn to early spring the fields are a refuge for over 200 sheep (with lambs) which graze on the marsh at time of high tides. Due to the location of the fields, their size and the potential for an increase in access, we believe that people and dogs could cause disruption to the current farm management practices and use of the land throughout the year.

Therefore the following exclusion is proposed:

1.2.20 Access to the land in the coastal margin seaward of route sections CPH-1-S114 and CPH-1-S115 is to be excluded all-year round by direction, under section 24 of the Countryside and Rights of Way Act (2000), for the purpose of land management. The exclusion does not affect the route itself and will have no legal effect on land where coastal access rights do not apply. See Directions Map CPH 1B.

*Exclusion of access to the saltmarsh/flat.*

1.2.21 Areas of saltmarsh at Lytham, Warton Bank, and Warton Aerodrome have deep channels and creeks, some of which would not be readily apparent to walkers and can pose a significant risk. The mudflats at Fairhaven, Lytham, Warton Bank, Warton Aerodrome and Freckleton are soft and sinking in nature. The saltmarshes and flats do not provide a safe walking surface and are subject to frequent tidal inundation. RNLI and Coastguard data indicates incidents of people being rescued from these areas. Therefore the following exclusions are proposed:

1.2.22 Access to the saltmarsh and mudflat in the coastal margin seaward of route sections CPH-1-S042 to CPH-1-S132 is to be excluded all-year round by direction under section 25A of the Countryside and Rights of Way Act (2000), as these areas are unsuitable for public access. These exclusions do not affect the route itself and will have no legal effect on land where coastal access rights do not apply. See Direction Maps CPH 1C and 1D.

1.2.23 These directions will not prevent or affect:

- any existing local use of the land by right where such use is not covered by coastal access rights;
- any other use people already make of the land locally by formal agreement with the landowner, or by informal permission or traditional toleration; or
- use of any registered rights of common or any rights at common law or by Royal Charter etc.

Any such use is not prohibited or limited by these arrangements.

**See part 8 of the Overview - 'Restrictions and exclusions' - for a summary for the entire stretch.**

1.2.24 **Optional alternative routes:** An optional alternative route is to operate as an optional diversion from the ordinary route between St. Anne's HM Coastguard Station and St. Anne's Pier (route sections CPH-1-S030 to CPH-1-S035 when it is subject to exceptionally high tides. The optional alternative route is to be at the centre of the line shown as route sections CPH-1-OA001 to CPH-1-OA011 on map CPH 1i. It would not have the effect of creating any additional spreading room on either the seaward or the landward side.

1.2.25 An optional alternative route is to operate as an optional diversion from the ordinary route between St. Anne's miniature golf course and Fairhaven Lake (route sections CPH-1-S038 to CPH-1-S042) when it is subject to exceptionally high tides. The optional alternative route is to be at the centre of the line shown as route sections CPH-1-OA012 to CPH-1-OA018 on maps CPH 1j and 1k. It would not have the effect of creating any additional spreading room on either the seaward or the landward side.

1.2.26 An optional alternative route is to operate as an optional diversion from the ordinary route on the marsh at Jubilee House, Lytham St. Anne's (route sections CPH-1-S044 to CPH-1-S049 when it is subject to exceptionally high tides. The optional alternative route is to be at the centre of the line shown as route sections CPH-1-OA019 to CPH-1-OA025 on map CPH 1m. It would not have the effect of creating any additional spreading room on either the seaward or the landward side.

1.2.27 By default, an optional alternative route covers the land two metres either side of the approved line. However, by virtue of s55D(2) of the National Parks and Access to the Countryside Act 1949, where the optional alternative route follows an existing path corridor, we may propose that the trail should adopt a variable width as dictated by the existing physical features on either side. Columns 5a and 5b of table 1.3.2 describe the boundaries of the alternative route strips on any route sections where we have proposed use of this discretion in order to clarify the extent of the access strip.

1.2.28 **Coastal erosion:** Natural England is able to propose that the route of the trail would be able to change in the future, without further approval from the Secretary of State, in response to coastal change. This would happen in accordance with the criteria and procedures for 'roll-back' set out in part 7 of the Overview.

Natural England may only propose the use of this roll-back power:

- as a result of coastal erosion or other geomorphological processes or encroachment by the sea, or
- in order to link with other parts of the route that need to roll back in direct response to such changes.

1.2.29 Column 4 of tables 1.3.1 and 1.3.2 indicates where roll-back has been proposed in relation to a route section. Where this is the case, the route, as initially determined at the time the report was prepared, is to be at the centre of the line shown on maps CPH 1a to 1r as the proposed route of the trail.

1.2.30 If at any time in the future any part of a route section upon which roll-back has been specified needs, in Natural England's view, to change in order for the overall route to remain viable, the new route for the part in question will be determined by Natural England without further reference to the Secretary of State. This will be done in accordance with the criteria and procedures described under the title 'Roll-back' in part 7 of the Overview and section 4.10 of the Coastal Access Scheme. If this happens, the new route will become the approved route for that section for the purposes of the Order which determines where coastal access rights apply.

On sections for which roll-back is not proposed in tables 1.3.1 or 1.3.2, the route is to be at the centre of the line shown on maps CPH 1a to 1r as the proposed route of the trail.

### Other future change:

1.2.31 At this point we do not foresee any need for future changes to the access provisions that we have proposed within this report.

**See parts 7 - 'Future changes' of the Overview for more information.**

### Establishment of the trail:

1.2.32 Below we summarise how our proposed route for the trail would be physically established to make it ready for public use before any new rights come into force.

Establishment works will only start on this length of coast once these proposals have been approved by the Secretary of State. The works may therefore either precede or follow the start of establishment works on other lengths of coast within the stretch, and detailed in their separate reports.

## Lancashire County Council area:

1.2.33 Our estimate of the capital costs for physical establishment of the trail on the proposed route is £269,678 and is informed by:

- information already held by the access authority
- the conclusions of our deliberations in relation to potential impacts on the environment; and
- information gathered while visiting affected land and talking to the people who own and manage it about the options for the route.

1.2.34 There are several main elements to the overall cost:

- A significant number of new signs and information boards would be needed on the trail.
- A significant number of sleeper bridges and a foot bridge will need to be installed along a very wet part of the route at Warton Airfield.
- Some of the surfaces and access furniture of the existing paths and footways on the proposed route are generally of a suitable standard for the trail, but there are some places where new steps, boardwalks and important ground works and resurfacing would enhance the convenience of the trail. More significant items of establishment works are shown on the relevant maps accompanying this report.

Table 1 shows our estimate of the capital cost for each of the main elements of physical establishment described above.

**Table 1: Estimate of capital costs**

<b>Item</b>	<b>Cost</b>
Signs and interpretation	£29,947
Bridges	£161,264
Fencing	£1,450
Steps	£1,760
Boundary crossings	£12,941
Surfacing works	£37,800
Project management	£24,516
<b>Total</b>	<b>£269,678 (Exclusive of any VAT payable)</b>

## Blackpool Council area:

1.2.35 Our estimate of the capital costs for physical establishment of the trail on the proposed route is £4,076 and is informed by:

- information already held by the access authority
- the conclusions of our deliberations in relation to potential impacts on the environment; and

- information gathered while visiting affected land and talking to the people who own and manage it about the options for the route.

1.2.36 There is one main element to the overall cost:

- A number of new signs and information boards would be needed on the trail.

Table 2 shows our estimate of the capital cost for each of the main elements of physical establishment described above.

**Table 2: Estimate of capital costs**

Item	Cost
Signs and interpretation	£3,706
Project management	£370
<b>Total</b>	<b>£4,076 (Exclusive of any VAT payable)</b>

1.2.37 Once the Secretary of State’s decision on our report has been notified, and further to our conversations with land managers during the route planning stage, Lancashire County Council and Blackpool Council will liaise with affected land owners and occupiers about relevant aspects of the design, installation and maintenance of the new signs and infrastructure that are needed on their land. Prior to works being carried out on the ground, all necessary permissions, authorisations and consents will be obtained. All such works would conform to the published standards for National Trails and the other criteria described in our Coastal Access Scheme.

**Maintenance of the trail:**

1.2.38 Because the trail on this length of coast will form part of the National Trail being created around the whole coast of England called the England Coast Path, we envisage that it will be maintained to the same high quality standards as other National Trails in England (see The New Deal; Management of National Trails in England from April 2013: details at Annex A of the Overview).

1.2.39 We estimate that the annual cost to maintain the trail will be **£9,072** (exclusive of any VAT payable). In developing this estimate we have taken account of the formula used to calculate Natural England’s contribution to the maintenance of other National Trails.



## Part 1.3: Proposals Tables

See Part 3 of Overview for guidance on reading and understanding the tables below

### 1.3.1 Section Details: Maps CPH 1a to CPH 1r – South Promenade (Kingsway), Cleveleys to Brades Lane, Freckleton.

Key notes on table:

1. Column 2 – an asterisk (\*) against the route section number means see also table 1.3.3: Other options considered.
2. Column 4 – ‘No’ means no roll-back is proposed for this route section.
3. Column 4 – ‘Yes – see table 1.3.4’ means roll-back is proposed, but refer to that table below about our likely approach to implementing it for this route section. This is because a more complex situation exists in this case and consideration must be given to how roll-back may happen in relation to excepted land, a protected site etc.
4. Column 5a - Certain coastal land types are included automatically in the coastal margin where they fall landward of the trail if they touch it at some point. The relevant land type (foreshore, cliff, bank, barrier, dune, beach, flat or section 15 land – see Glossary) is shown in this column where appropriate. “No” means none present on this route section.
5. Columns 5b and 5c – Any entry in these columns means we are proposing to align the landward boundary of the coastal margin on this route section with the physical feature(s) shown in 5b, for the reason in 5c. No text here means that for this route section the landward edge of the margin would be that of the trail itself - or if any default coastal land type is shown in 5a, that would be its landward boundary instead.

1	2	3	4	5a	5b	5c	6
Map(s)	Route section number(s)	Current status of route section(s)	Roll-back proposed? (See Part 7 of Overview)	Landward margin contains coastal land type?	Proposal to specify landward boundary of margin (See maps)	Reason for landward boundary proposal	Explanatory notes
CPH 1a	CPH-1-S001	Public footway (pavement)	No	No	Landward edge of promenade	Clarity and cohesion	
CPH 1a	CPH-1-S002 and CPH-1-S003	Public footway (pavement)	No	No	Landward edge of pavement	Clarity and cohesion	
CPH 1a and 1b	*CPH-1-S004	Public footway (pavement)	No	No	Various	Clarity and cohesion	Landward margin will follow edge of tram track and pavement edge
CPH 1c	*CPH-1-S005	Public footway (pavement)	No	No	Fence line of tram tracks	Clarity and cohesion	
CPH 1c	*CPH-1-S006	Public footway (pavement)	No	No	Various	Clarity and cohesion	Landward margin will follow wall and edge of tram track

1	2	3	4	5a	5b	5c	6
Map(s)	Route section number(s)	Current status of route section(s)	Roll-back proposed? (See Part 7 of Overview)	Landward margin contains coastal land type?	Proposal to specify landward boundary of margin (See maps)	Reason for landward boundary proposal	Explanatory notes
CPH 1d	*CPH-1-S007	Public footway (pavement)	No	No	Landward edge of promenade	Clarity and cohesion	
CPH 1d to 1g	*CPH-1-S008 to CPH-1-S019	Public footway (pavement)	No	No	Various	Additional landward area	Landward margin will follow edge of tram track and wall to include additional promenade area
CPH 1h	CPH-1-S020 and CPH-1-S021	Public footway (pavement)	No	No	Wall	Clarity and cohesion	
CPH 1h	*CPH-1-S022*	Byway open to all traffic	No	No	Landward edge of road	Clarity and cohesion	
CPH 1h	*CPH-1-S023*	Public highway	No	No			
CPH 1h	*CPH-1-S024*	Public footway (pavement)	No	No	Landward edge of pavement	Clarity and cohesion	
CPH 1h	*CPH-1-S025	Public highway	No	No			
CPH 1h	*CPH-1-S026	Public footway (pavement)	No	No	Landward edge of pavement	Clarity and cohesion	
CPH 1h	*CPH-1-S027*	Other existing walked route	Yes - See table 1.3.4	Yes - dune			
CPH 1h	CPH-1-S028	Public footway (pavement)	No	No	Landward edge of pavement	Clarity and cohesion	
CPH 1h	CPH-1-S029	Public highway	No	No			
CPH 1h and 1i	*CPH-1-S030*	Other existing walked route	Yes - See table 1.3.4	Yes - dune			
CPH 1i	CPH-1-S031	Public highway		No			
CPH 1i	CPH-1-S032	Public highway		No	Landward edge of road	Clarity and cohesion	
CPH 1i	CPH-1-S033	Other existing	Yes - See table 1.3.4	Yes - dune			

1	2	3	4	5a	5b	5c	6
Map(s)	Route section number(s)	Current status of route section(s)	Roll-back proposed? (See Part 7 of Overview)	Landward margin contains coastal land type?	Proposal to specify landward boundary of margin (See maps)	Reason for landward boundary proposal	Explanatory notes
		walked route					
CPH 1i	CPH-1-S034	Public footway (pavement)	No	No	Landward edge of pavement	Clarity and cohesion	
CPH 1i	CPH-1-S035	Public footway (pavement)	No	No			
CPH 1j	CPH-1-S036	Public footway (pavement)	No	No	Landward of edge promenade	Clarity and cohesion	
CPH 1j	CPH-1-S037	Public footway (pavement)	No	No			
CPH 1j	CPH-1-S038	Public footway (pavement)	No	No	Landward edge of promenade	Clarity and cohesion	
CPH 1j to 1k	CPH-1-S039 to CPH-1-S041	Other existing walked route	No	Yes - dune			
CPH 1k	CPH-1-S042	Other existing walked route	No	No	Various	Clarity and cohesion	Landward margin will follow the path edge and the tree line
CPH 1k	CPH-1-S043	Public footway (pavement)	No	No	Landward edge of pavement	Clarity and cohesion	
CPH 1l and 1m	CPH-1-S044	Public footway (pavement)	No	No	Various	Additional landward area	Landward margin will follow fence line, wall and seaward edge of road
CPH 1m	CPH-1-S045 and CPH-1-S046	Other existing walked route	No	No	Wall	Clarity and cohesion	
CPH 1m	CPH-1-S047	Public footway (pavement)	No	No	Fence line	Clarity and cohesion	
CPH 1m	CPH-1-S048 and CPH-1-S049	Public footway (pavement)	No	No	Fence line	Additional landward area	

1	2	3	4	5a	5b	5c	6
Map(s)	Route section number(s)	Current status of route section(s)	Roll-back proposed? (See Part 7 of Overview)	Landward margin contains coastal land type?	Proposal to specify landward boundary of margin (See maps)	Reason for landward boundary proposal	Explanatory notes
CPH 1m	CPH-1-S050 to CPH-1-S054	Public footway (pavement)	No	No	Landward edge of pavement	Clarity and cohesion	
CPH 1m	CPH-1-S055	Public highway	No	No			
CPH 1m	*CPH-1-S056 and CPH-1-S057	Public footway (pavement)	No	No	Landward edge of pavement	Clarity and cohesion	
CPH 1m	*CPH-1-S058	Public highway	No	No			
CPH 1m	*CPH-1-S059 to CPH-1-S061	Public footway (pavement)	No	No	Landward edge of pavement	Clarity and cohesion	
CPH 1m	*CPH-1-S062	Public highway	No	No			
CPH 1m	*CPH-1-S063 to CPH-1-S066	Public footway (pavement)	No	No	Landward edge of pavement	Clarity and cohesion	
CPH 1m	CPH-1-S067 and CPH-1-S068	Public footpath	No	Yes - bank			
CPH 1m	CPH-1-S069	Public footpath	No	No	Landward edge of path	Clarity and cohesion	
CPH 1m	*CPH-1-S070 and CPH-1-S071	Public footpath	No	Yes - bank			
CPH 1m	*CPH-1-S072	Other existing walked route	No	Yes - bank			
CPH 1m	CPH-1-S073 and CPH-1-S074	Public footpath	No	No	Fence line	Clarity and cohesion	
CPH 1m	CPH-1-S075	Other existing walked route	No	No	Fence line	Clarity and cohesion	
CPH 1m	CPH-1-S076	Public footway (pavement)	No	No		Clarity and cohesion	
CPH 1n	*CPH-1-S077 and CPH-1-S078	Public footpath	No	No	Fence line	Clarity and cohesion	
CPH 1n	*CPH-1-S079 and CPH-1-S080	Not an existing	No	No	Fence line	Clarity and cohesion	

1	2	3	4	5a	5b	5c	6
Map(s)	Route section number(s)	Current status of route section(s)	Roll-back proposed? (See Part 7 of Overview)	Landward margin contains coastal land type?	Proposal to specify landward boundary of margin (See maps)	Reason for landward boundary proposal	Explanatory notes
		walked route					
CPH 1n	*CPH-1-S081	Not an existing walked route	No	No	Hedge	Clarity and cohesion	
CPH 1n	*CPH-1-S082 and CPH-1-S083	Not an existing walked route	No	No	Fence line	Clarity and cohesion	
CPH 1n & 1o	*CPH-1-S084 and CPH-1-S085	Public footpath	No	No	Fence line	Clarity and cohesion	
CPH 1o	CPH-1-S086 and CPH-1-S087	Public footpath	No	No	Wall	Clarity and cohesion	
CPH 1o	CPH-1-S088	Public bridleway	Yes - See table 1.3.4	No	Fence line	Clarity and cohesion	
CPH 1o	CPH-1-S089 and CPH-1-S090	Public footpath	Yes - See table 1.3.4	No	Fence line	Clarity and cohesion	
CPH 1o	CPH-1-S091	Other existing walked route	Yes - See table 1.3.4	No			
CPH 1o	CPH-1-S092 and CPH-1-S093	Public footpath	Yes - See table 1.3.4	No	Fence line	Clarity and cohesion	
CPH 1o	CPH-1-S094	Other existing walked route	Yes - See table 1.3.4	No			
CPH 1o	CPH-1-S095	Other existing walked route	Yes - See table 1.3.4	No	Fence line	Clarity and cohesion	
CPH 1o	CPH-1-S096 to CPH-1-S101	Public footpath	Yes - See table 1.3.4	No	Fence line	Clarity and cohesion	
CPH 1o	CPH-1-S102	Public footpath	No	No	Fence line	Clarity and cohesion	
CPH 1p	CPH-1-S103 to CPH-1-S109	Public footpath	Yes - See table 1.3.4	No	Fence line	Clarity and cohesion	
CPH 1p	CPH-1-S110 and CPH-1-S111	Public footpath	Yes - See table 1.3.4	No	Landward edge of bridge	Clarity and cohesion	

1	2	3	4	5a	5b	5c	6
Map(s)	Route section number(s)	Current status of route section(s)	Roll-back proposed? (See Part 7 of Overview)	Landward margin contains coastal land type?	Proposal to specify landward boundary of margin (See maps)	Reason for landward boundary proposal	Explanatory notes
CPH 1p	CPH-1-S112	Public footpath	Yes - See table 1.3.4	No			
CPH 1q	*CPH-1-S113	Public footpath	Yes - See table 1.3.4	No	Fence line	Clarity and cohesion	
CPH 1q	*CPH-1-S114	Public footpath	Yes - See table 1.3.4	No			
CPH 1q	*CPH-1-S115	Public footpath	Yes - See table 1.3.4	No	Hedge	Clarity and cohesion	
CPH 1q	*CPH-1-S116	Public footpath	No	No	Seaward edge of ditch	Clarity and cohesion	
CPH 1q	*CPH-1-S117	Public footpath	No	No			
CPH 1q	*CPH-1-S118	Public footpath	No	No	Hedge	Clarity and cohesion	
CPH 1q	*CPH-1-S119	Public footpath	Yes - See table 1.3.4	No			
CPH 1q	*CPH-1-S120 and CPH-1-S121	Public footpath	Yes - See table 1.3.4	No	Fence line	Clarity and cohesion	
CPH 1q	*CPH-1-S122	Public footpath	Yes - See table 1.3.4	No	Fence line	Clarity and cohesion	
CPH 1q	*CPH-1-S123 and CPH-1-S124	Public footpath	Yes - See table 1.3.4	No			
CPH 1q	*CPH-1-S125	Public footpath	Yes - See table 1.3.4	No	Fence line	Clarity and cohesion	
CPH 1q	*CPH-1-S126	Public footpath	No	No	Fence line	Clarity and cohesion	
CPH 1q	*CPH-1-S127 and CPH-1-S128	Public footpath	Yes - See table 1.3.4	No	Hedge	Clarity and cohesion	
CPH 1q	*CPH-1-S129	Public footpath	No	No			
CPH 1q and 1r	*CPH-1-S130 to CPH-1-S134	Public footpath	Yes - See table 1.3.4	No			
CPH 1r	*CPH-1-S135	Public footpath	Yes - See table 1.3.4	No	Hedgerow	Clarity and cohesion	
CPH 1r	*CPH-1-S136	Public footway (pavement)	No	No	Wall	Clarity and cohesion	
CPH 1r	*CPH-1-S137	Public highway	No	No			

1	2	3	4	5a	5b	5c	6
Map(s)	Route section number(s)	Current status of route section(s)	Roll-back proposed? (See Part 7 of Overview)	Landward margin contains coastal land type?	Proposal to specify landward boundary of margin (See maps)	Reason for landward boundary proposal	Explanatory notes
CPH 1r	*CPH-1-S138	Public footway (pavement)	No	No	Wall	Clarity and cohesion	
CPH 1r	*CPH-1-S139	Public highway	No	No			
CPH 1r	*CPH-1-S140	Public footway (pavement)	No	No	Wall	Clarity and cohesion	
CPH 1r	*CPH-1-S141	Public highway	No	No			
CPH 1r	*CPH-1-S142 and CPH-1-S143	Public footpath	No	No	Landward edge of track	Clarity and cohesion	

### 1.3.2 Alternative routes and optional alternative route details: Maps CPH 1a to CPH 1r – South Promenade (Kingsway), Cleveleys to Brades Lane, Freckleton.

Key notes on table:

1. Column 4 – ‘No’ means no roll-back is proposed for this route section.
2. Columns 5a and 5b – An entry in either or both of these columns denotes a proposal to align the seaward or landward boundary (as the case may be) of this section of the alternative route strip with the physical feature(s) shown. No text in the column means no such proposal, meaning that the edge of the alternative route strip would be at the default width of 2 metres on the relevant side of the route’s centre line.

1	2	3	4	5a	5b	6
Map(s)	Route section number(s)	Current status of route section(s)	Roll-back proposed? (See Part 7 of Overview)	Proposal to specify seaward boundary of alternative route strip	Proposal to specify landward boundary of alternative route strip	Explanatory notes
CPH 1i	CPH-1-OA001	Public highway	No	Pavement edge	Pavement edge	
CPH 1i	CPH-1-OA002	Public footway (pavement)	No	Pavement edge	Pavement edge	
CPH 1i	CPH-1-OA003	Public highway	No			
CPH 1i	CPH-1-OA004	Public footway (pavement)	No	Pavement edge	Pavement edge	
CPH 1i	CPH-1-OA005	Public highway	No			
CPH 1i	CPH-1-OA006	Public footway (pavement)	No	Pavement edge	Pavement edge	
CPH 1i	CPH-1-OA007	Public highway	No			
CPH 1i	CPH-1-OA008	Public footway (pavement)	No	Pavement edge	Pavement edge	
CPH 1i	CPH-1-OA009	Public highway	No			
CPH 1i	CPH-1-OA010	Public footway (pavement)	No	Pavement edge	Pavement edge	
CPH 1i	CPH-1-OA011	Other existing walked route	No	Pavement edge	Pavement edge	
CPH 1j	CPH-1-OA012	Other existing walked route	No	Edge of path	Edge of path	
CPH 1j	CPH-1-OA013	Public footway (pavement)	No	Pavement edge	Pavement edge	
CPH 1j	CPH-1-OA014	Public highway	No			
CPH 1j	CPH-1-OA015	Public footway (pavement)	No	Pavement edge	Pavement edge	
CPH 1j	CPH-1-OA016	Public highway	No			



1	2	3	4	5a	5b	6
Map(s)	Route section number(s)	Current status of route section(s)	Roll-back proposed? (See Part 7 of Overview)	Proposal to specify seaward boundary of alternative route strip	Proposal to specify landward boundary of alternative route strip	Explanatory notes
CPH 1j	CPH-1-OA017	Public footway (pavement)	No	Pavement edge	Pavement edge	
CPH 1k	CPH-1-OA018	Other existing walked route	No	Edge of path	Edge of path	
CPH 1m	CPH-1-OA019	Public footway (pavement)	No	Fence line	Path	
CPH 1m	CPH-1-OA020	Public footway (pavement)	No	Pavement edge	Pavement edge	
CPH 1m	CPH-1-OA021	Public highway	No			
CPH 1m	CPH-1-OA022	Public footway (pavement)	No			
CPH 1m	CPH-1-OA023 and CPH-1-OA024	Other existing walked route	No	Fence line	Fence line	
CPH 1m	CPH-1-OA025	Other existing walked route	No	Edge of path	Fence line	

**1.3.3 Other options considered: Maps CPH 1a to CPH 1r – South Promenade (Kingsway), Cleveleys to Brades Lane, Freckleton.**

Map(s)	Route section numbers(s)	Other option(s) considered	Reasons for not proposing this option
CPH1a to CPH 1d	CPH-1-S004 to CPH-1-S008	We considered aligning the trail along the lowest part of the promenade between Little Bispham and North Pier, Blackpool.	<p>We opted for the proposed route because:</p> <ul style="list-style-type: none"> <li>■ it has better accessibility and provides all year round access; and</li> <li>■ we were advised by the Coastguard and Blackpool Council that the lower section of the promenade can be hit by large waves and may flood in bad weather, and therefore would be unsuitable for the trail.</li> </ul> <p>We therefore concluded that, overall, the proposed route struck the best balance in terms of the criteria described in Chapter 4 of the Coastal Access Scheme.</p>
CPH 1h	CPH-1-S022 to CPH-1-S026	<p>We considered aligning the trail along the foreshore, south form Starr Gate.</p> <p>We also considered aligning through the dunes.</p>	<p>We opted for the proposed route because:</p> <ul style="list-style-type: none"> <li>■ the foreshore is frequently and quickly inundated by the tide; and</li> <li>■ the dunes are steep-sided and difficult to walk in, and they are constantly moving in this location, making it difficult to mark a route.</li> </ul> <p>We therefore concluded that, overall, the proposed route struck the best balance in terms of the criteria described in Chapter 4 of the Coastal Access Scheme.</p>
CPH 1h and CPH 1i	CPH-1-S027 and CPH-1-S030	We considered aligning along Clifton Drive North	<p>We opted for the proposed route because:</p> <ul style="list-style-type: none"> <li>■ it takes walkers away from a busy road, giving a more coastal feel amongst the smaller dunes.</li> </ul> <p>We therefore concluded that, overall, the proposed route struck the best balance in terms of the criteria described in Chapter 4 of the Coastal Access Scheme.</p>

Map(s)	Route section numbers(s)	Other option(s) considered	Reasons for not proposing this option
CPH 1m	CPH-1-S057 to CPH-1-S065	We considered aligning along the south bank of Liggard Brook, adjacent to Dock Road.	<p>We opted for the proposed route because:</p> <ul style="list-style-type: none"> <li>■ it has better accessibility and provides all year round access;</li> <li>■ It would be expensive to install infrastructure to allow people access from the embankment, over the bridge and down onto the pavement. The infrastructure would also obstruct the pavement; and</li> <li>■ The embankment is uneven and does not provide a suitable surface for the trail.</li> </ul> <p>We therefore concluded that, overall, the proposed route struck the best balance in terms of the criteria described in chapter 4 of the Coastal Access Scheme.</p>
CPH 1m	CPH-1-S070 to CPH-1-S072	We considered aligning the trail around the outside of the waste land behind Lytham Trade Park.	<p>We opted for the proposed route because:</p> <ul style="list-style-type: none"> <li>■ it has better accessibility and provides all year round access; and</li> <li>■ It is more direct in an area where there are no views of the coast.</li> </ul> <p>We therefore concluded that, overall, the proposed route struck the best balance in terms of the criteria described in chapter 4 of the Coastal Access Scheme.</p>
CPH 1n	CPH-1-S078 to CPH-1-S084	<p>At Brook Cottage we considered aligning the trail along the existing public right of way (PRoW). This heads inland from the embankment onto the A584 and then back to re-join the PRoW on the embankment.</p> <p>At the same location we also considered dropping down off the PRoW onto the marsh and crossing the brook using the existing bridge, then returning to the PRoW on the embankment.</p>	<p>We opted for the proposed route because:</p> <ul style="list-style-type: none"> <li>■ it is closer to the sea than the public footpath and would afford better sea views; and</li> <li>■ it has better accessibility and provides all year round access.</li> </ul> <p>We therefore concluded that, overall, the proposed route struck the best balance in terms of the criteria described in chapter 4 of the Coastal Access Scheme.</p>

Map(s)	Route section numbers(s)	Other option(s) considered	Reasons for not proposing this option
CPH 1q	CPH-1-S113 to CPH-1-S131	At Naze Point we considered aligning the trail on the existing PRow that goes around Naze Point. It follows the west bank of Freckleton Pool as far as Freckleton Boat Yard.	<p>We opted for the proposed route because:</p> <ul style="list-style-type: none"> <li>■ it is a better walking experience and provides longer term sustainability of the route due to erosion of the PRow. It is accessible in bad weather.</li> </ul> <p>We therefore concluded that, overall, the proposed route struck the best balance in terms of the criteria described in chapter 4 of the Coastal Access Scheme</p>
CPH 1q and 1r and Report CPH 2	CPH-1-S132 to CPH-1-S143	Off the access road of Freckleton Boat Yard, we considered crossing an existing culvert onto Freckleton Marsh.	<p>We opted for the proposed route because:</p> <ul style="list-style-type: none"> <li>■ There are ongoing complex nature conservation and land management issues east of the location. For further explanation see CPH Report 2 'Other options considered' table.</li> </ul> <p>We therefore concluded that, overall, the proposed route struck the best balance in terms of the criteria described in chapter 4 of the Coastal Access Scheme</p>

Note: Any public rights of way not forming part of the proposed trail would remain available for people to use under their pre-existing rights.

### 1.3.4 Roll-back implementation – more complex situations: Maps CPH 1a to CPH 1r – South Promenade (Kingsway), Cleveleys to Brades Lane, Freckleton.

Map(s)	Route section number(s)	Feature(s) or site(s) potentially affected	Our likely approach to roll-back
CPH 1h to CPH 1i	CPH-1-S027; CPH-1-S030; and CPH-1-S033	<ul style="list-style-type: none"> <li>■ Liverpool Bay Special Protection Area (SPA)</li> <li>■ Ribble and Alt Estuaries SPA</li> <li>■ Ribble and Alt Estuaries Ramsar site</li> <li>■ Ribble Estuary Site of Special Scientific Interest (SSSI) for its wildlife interest</li> <li>■ Lytham St Annes Dunes SSSI for its wildlife interest</li> <li>■ Lytham Coastal Changes SSSI for its wildlife interest</li> <li>■ Ribble Estuary Marine Conservation Zone (MCZ)</li> </ul>	<p>If it is no longer possible to find a viable route seaward of a designated site (e.g. SSSI, SAC, SPA, SM) whose designated features are sensitive to public access, or where the existing route already passing through such a site must be altered, we will choose a new route after detailed discussions with the relevant experts and with any potentially affected owners or occupiers, which will either (a) [continue to] pass through the site, if appropriate or (b) if necessary, be routed landward of it.</p> <p>Consideration will be given to possible impacts of rollback on the environment, including further assessment under the Habitats Regulations where necessary.</p> <p>In reaching all of the above judgements we will have full regard to the need to seek a fair balance between the interests of potentially affected owners and occupiers and those of the public.</p>
CPH 1o to CPH 1p to 1r	CPH-1-S088 to CPH-1-S101 CPH-1-S103 to CPH-1-S115; CPH-1-S119 to CPH-1-S125; CPH-1-S127 & CPH-1-S128; and CPH-1-S130 to CPH-1-S135	<ul style="list-style-type: none"> <li>■ Warton Airfield</li> </ul>	<p>If it is no longer possible to find a viable route seaward of any excepted land (e.g. buildings, curtilage, gardens etc.), we will choose a route landward of it, following discussions with owners and occupiers.</p> <p>In reaching all of the above judgements we will have full regard to the need to seek a fair balance between the interests of potentially affected owners and occupiers and those of the public.</p>

In relation to all other sections where roll-back has been proposed, any later adjustment of the trail is likely to follow the current feature (e.g. cliff edge/beach) for the foreseeable future as any coastal change occurs.

## Part 1.4: Proposals Maps

### 1.4.1 Map Index

Map reference	Map title
CPH 1a	Kingsway, Cleveleys to Norbreck North Tram Stop
CPH 1b	Norbreck North Tram Stop to Cavendish Road
CPH 1c	Cavendish Road to Glyn Square car park
CPH 1d	Glyn Square car park to North Pier, Blackpool
CPH 1e	North Pier, Blackpool to Central Pier, Blackpool
CPH 1f	Central Pier, Blackpool to Sandcastle Leisure Centre
CPH 1g	Sandcastle Leisure Centre to Starr Gate, Blackpool
CPH 1h	Starr Gate, Blackpool to Clifton Drive North, St. Anne's
CPH 1i	Clifton Drive North, St Anne's to St. Anne's Pier
CPH 1j	St. Anne's Pier to AKS Lytham School
CPH 1k	AKS Lytham School to Granny's Bay
CPH 1l	Granny's Bay to Lytham Green
CPH 1m	Lytham Green to Lytham Dock Bridge
CPH 1n	Lytham Dock Bridge to Seaview Caravan Park, Warton Bank
CPH 1o	Seaview Caravan Park, Warton Bank to Warton Aerodrome
CPH 1p	Warton Aerodrome to Naze Mount Farm
CHP 1q	Naze Mount Farm to Naze Lane East
CPH 1r	Naze Lane East to Brades Lane, Freckleton
Directions Map CPH 1A	Freckleton, Newton and Clifton Marshes: Proposed direction under S24 CROW
Directions Map CPH 1B	Naze Farm, Freckleton: Proposed direction under S24 CROW

Map reference	Map title
Directions Map CPH 1C	Fairhaven to Lytham Dock saltmarsh and mudflats: Proposed direction under S25A CROW
Directions Map CPH 1D	Lytham Dock to Freckleton saltmarsh and mudflats: Proposed direction under S25A CROW

## PROPOSALS

### Trail Sections

- Trail using existing public right of way or highway
- Trail using other existing walked route
- Trail not using existing walked route
- Alternative route
- Trail shown on other maps
- Approved or open England Coast Path

Maps that show sections of the trail that follow the existing South West Coast Path as currently walked and managed use the following trail categories. Information on the existing status and infrastructure is not shown.

- Trail using existing South West Coast Path
- Alternative or optional alternative route using existing South West Coast Path

Trail sections which follow existing public rights of way or highways are indicated by a suffix:

- BW** - Public bridleway
- BY** - Public byway
- CP** - Cycletrack (pedestrian)
- CT** - Cycletrack (cycles only)
- FP** - Public footpath
- FW** - Public footway (Pavement)
- RB** - Restricted byway
- RD** - Public road

### Coastal Margin

Explanatory note

Part 3 of the Overview to the report explains where the landward boundary of the coastal margin falls by default. Our proposals include any suggested variation of this default boundary. The purple wash on the map indicates where as a result of our proposals the coastal margin would extend significantly to the landward side of the proposed route of the trail. The coastal margin may include some areas where coastal access rights do not apply, either seaward or landward of the proposed route of the trail: the Overview explains more about this. The landward boundary of the coastal margin may in due course move inland, if the trail rolls back under proposals in this report to respond to coastal change.

- Coastal margin landward of the trail
- Coastal margin landward of the trail which is existing access land

### Other Information

Other access rights and routes

- Public bridleways
- Public byways
- Public footpaths
- Restricted byways
- South West Coast Path
- Sustrans national routes
- Existing access land

### Infrastructure types

For status of each, where shown on map, see colour codes below

Bridges:

- Clapper bridge
- Footbridge
- Quad bike bridge
- Sleeper bridge
- Vehicle bridge

Stiles:

- Ladder stile
- Lift-up stile
- Squeeze stile
- Step stile
- Stone stile

Gates:

- Bristol gate
- Field gate
- Gateway with no gate
- Kissing gate
- Pedestrian gate
- Wheelchair gate

Miscellaneous:

- Barrier
- Boardwalk
- Bollard
- Cattle grid
- Culvert
- Cycle chicane
- Drainage
- Drop-kerb
- Gap in fence
- Hurdle
- Interpretation panel
- Ramp
- Revetment
- Stepping stones
- Steps

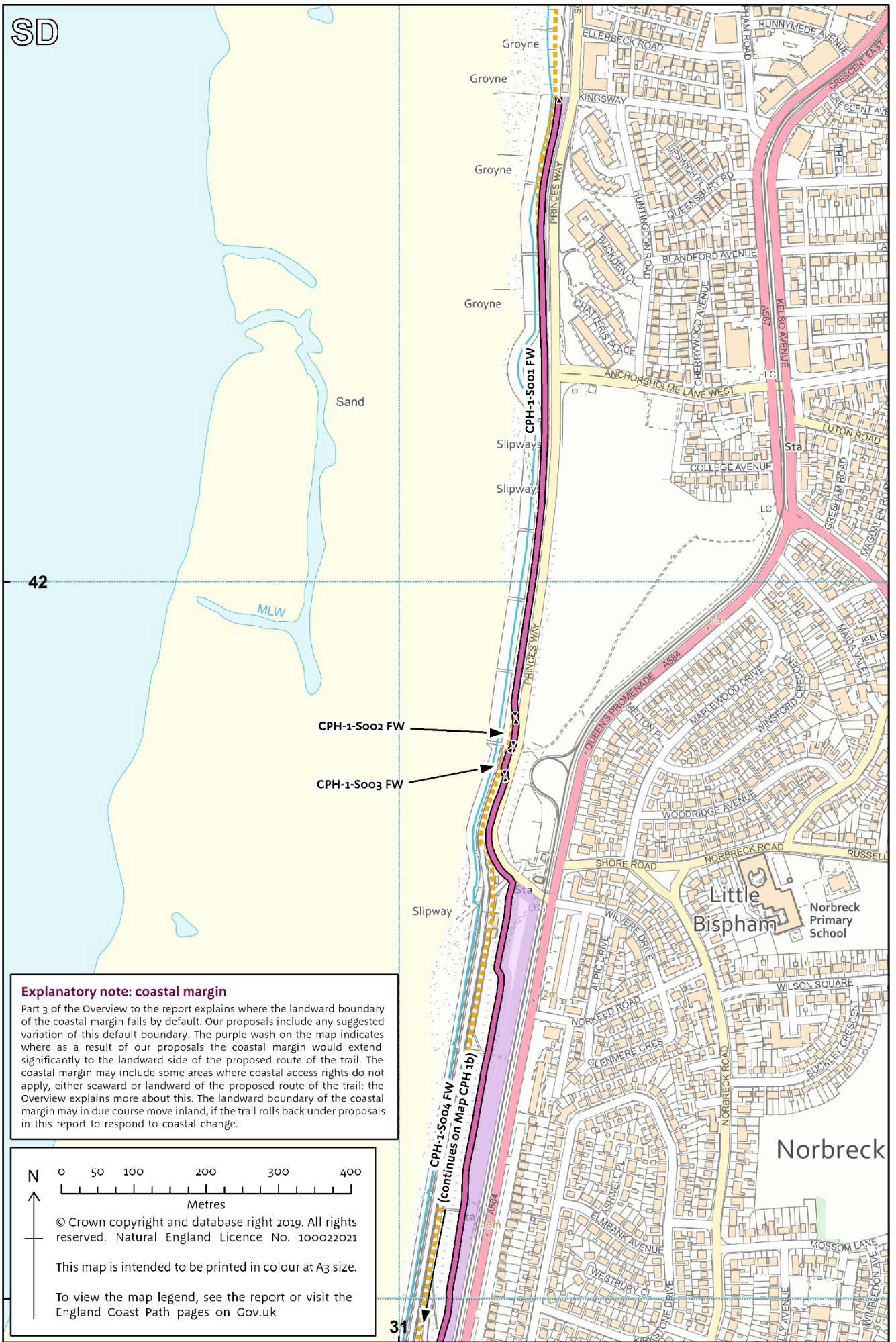
### Infrastructure status

Each symbol shown on the map is colour coded as appropriate, as in this example for a set of steps:

- Existing steps to be retained
- New steps required
- Existing steps to be removed

\* Please note that the items in this legend may not all be present on an individual map or report.



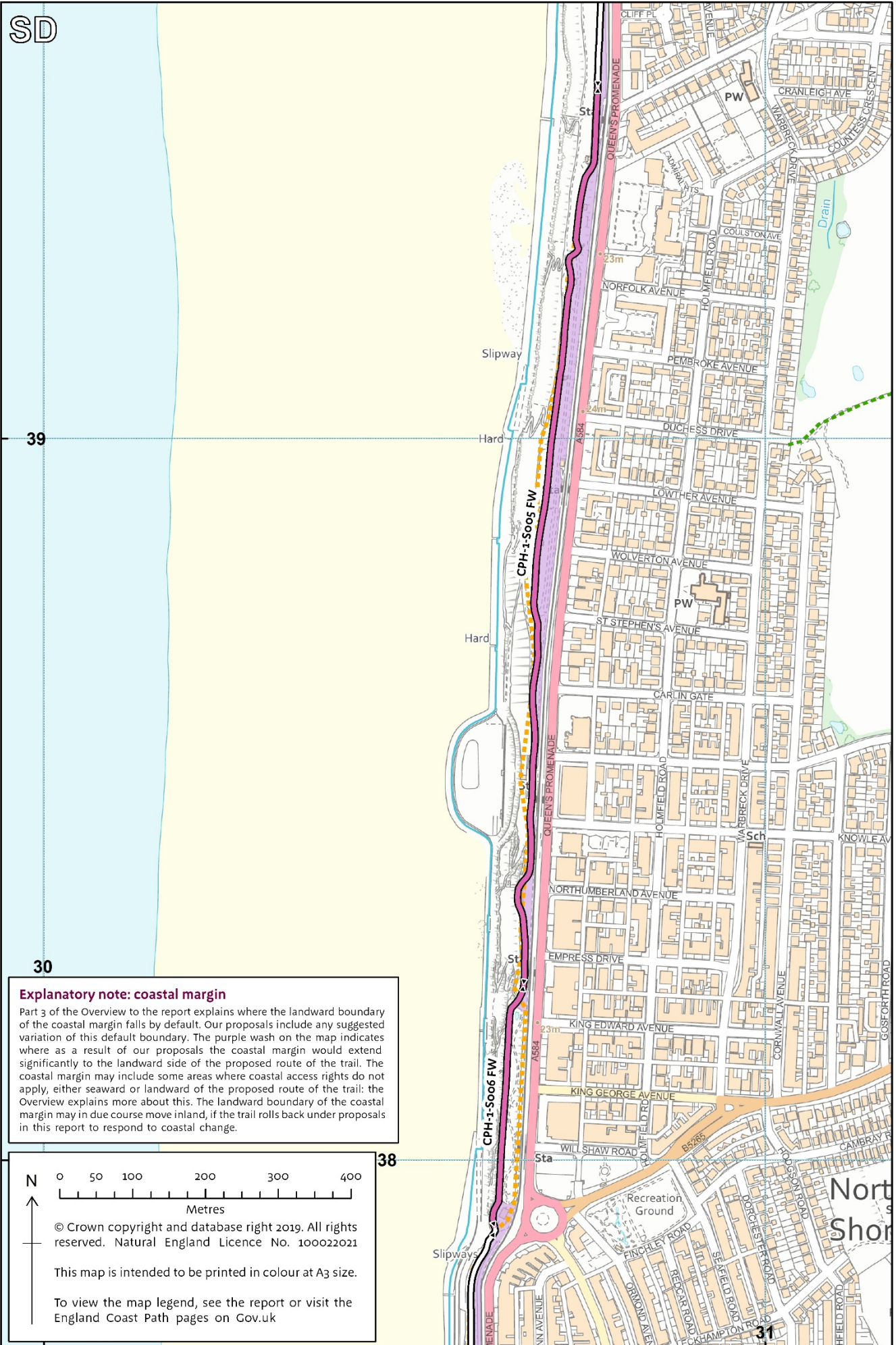


**Explanatory note: coastal margin**  
 Part 3 of the Overview to the report explains where the landward boundary of the coastal margin falls by default. Our proposals include any suggested variation of this default boundary. The purple wash on the map indicates where as a result of our proposals the coastal margin would extend significantly to the landward side of the proposed route of the trail. The coastal margin may include some areas where coastal access rights do not apply, either seaward or landward of the proposed route of the trail: the Overview explains more about this. The landward boundary of the coastal margin may in due course move inland, if the trail rolls back under proposals in this report to respond to coastal change.

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**Explanatory note: coastal margin**

Part 3 of the Overview to the report explains where the landward boundary of the coastal margin falls by default. Our proposals include any suggested variation of this default boundary. The purple wash on the map indicates where as a result of our proposals the coastal margin would extend significantly to the landward side of the proposed route of the trail. The coastal margin may include some areas where coastal access rights do not apply, either seaward or landward of the proposed route of the trail: the Overview explains more about this. The landward boundary of the coastal margin may in due course move inland, if the trail rolls back under proposals in this report to respond to coastal change.

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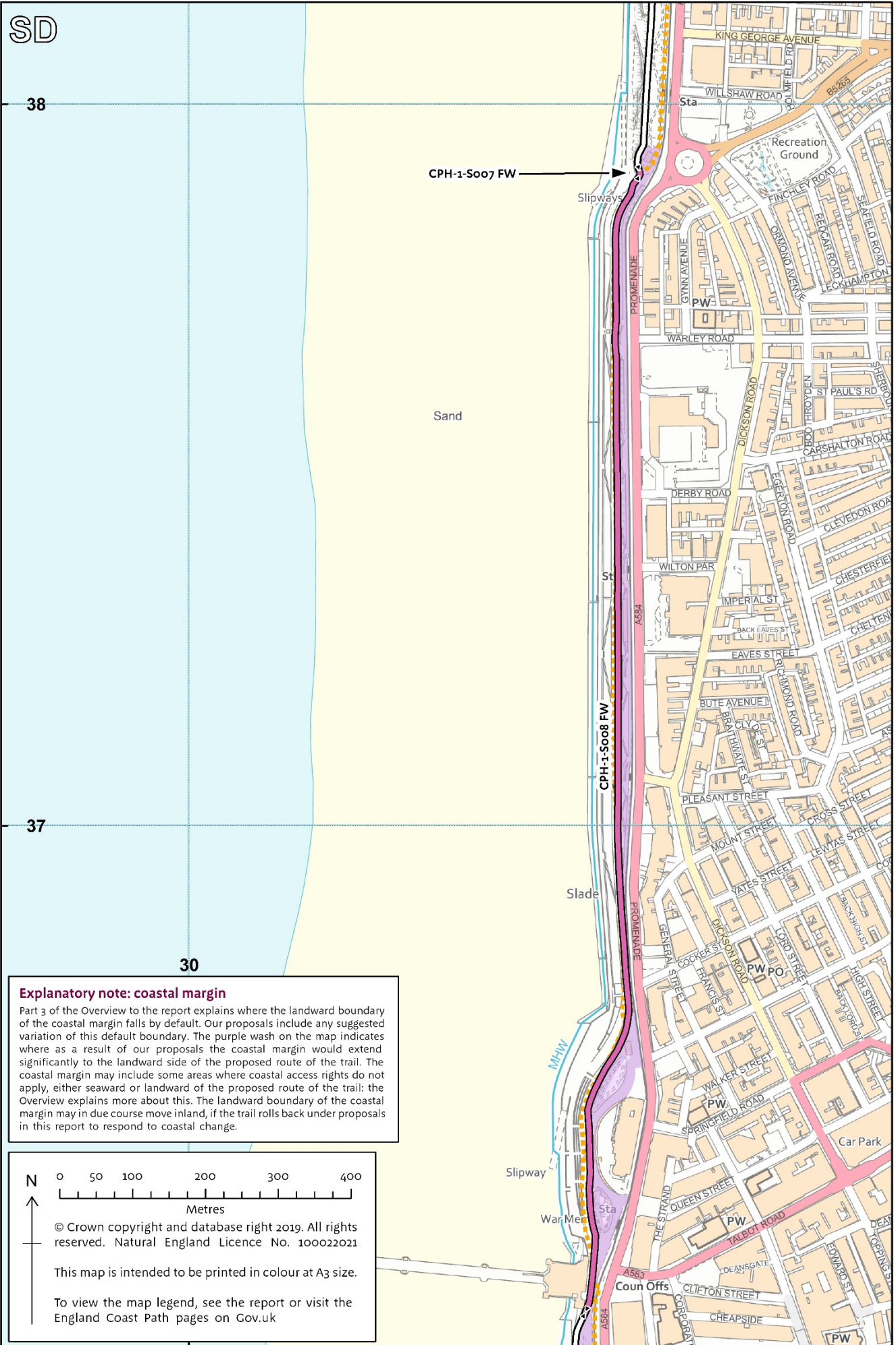
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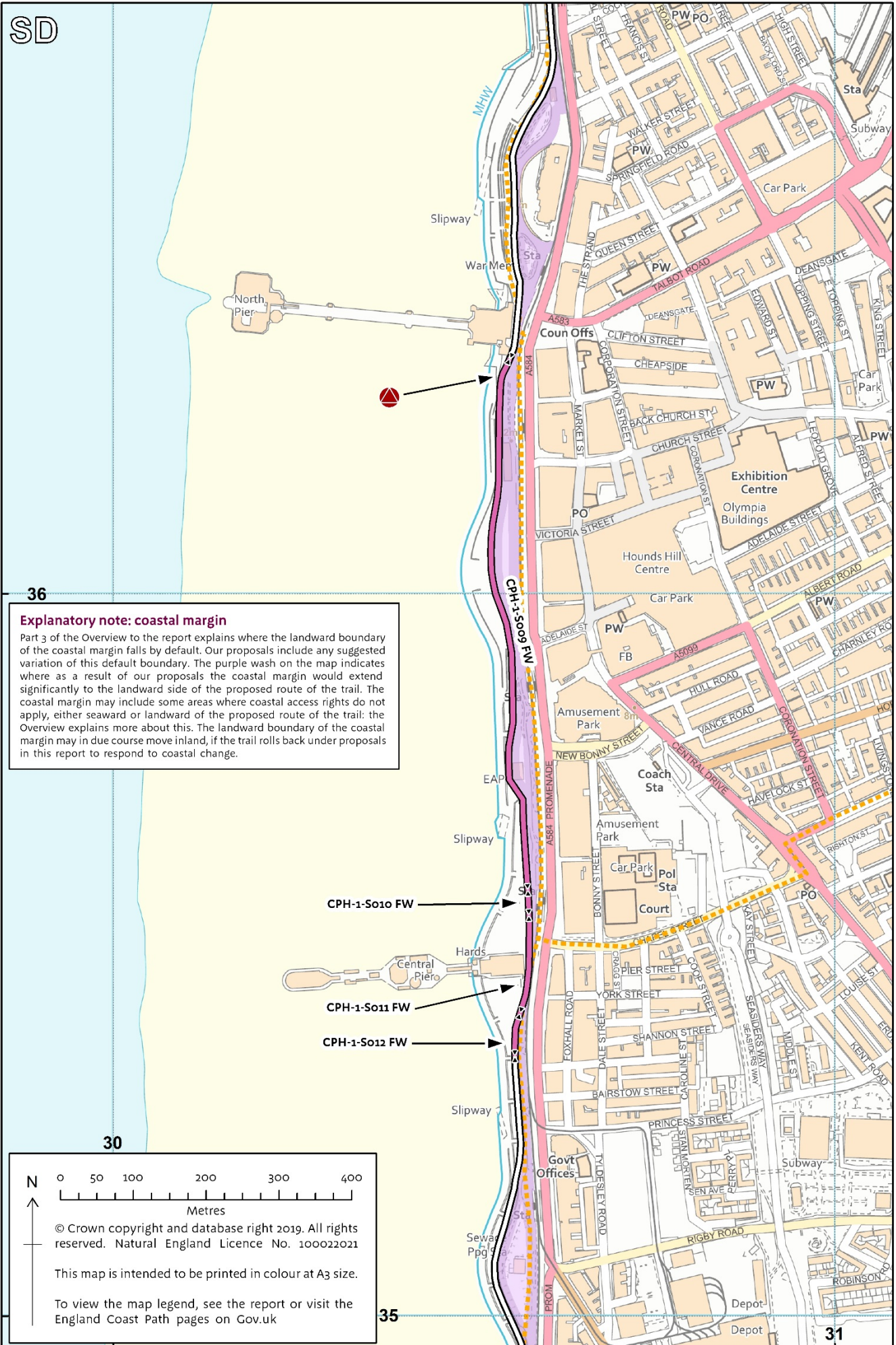
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**Explanatory note: coastal margin**  
 Part 3 of the Overview to the report explains where the landward boundary of the coastal margin falls by default. Our proposals include any suggested variation of this default boundary. The purple wash on the map indicates where as a result of our proposals the coastal margin would extend significantly to the landward side of the proposed route of the trail. The coastal margin may include some areas where coastal access rights do not apply, either seaward or landward of the proposed route of the trail: the Overview explains more about this. The landward boundary of the coastal margin may in due course move inland, if the trail rolls back under proposals in this report to respond to coastal change.

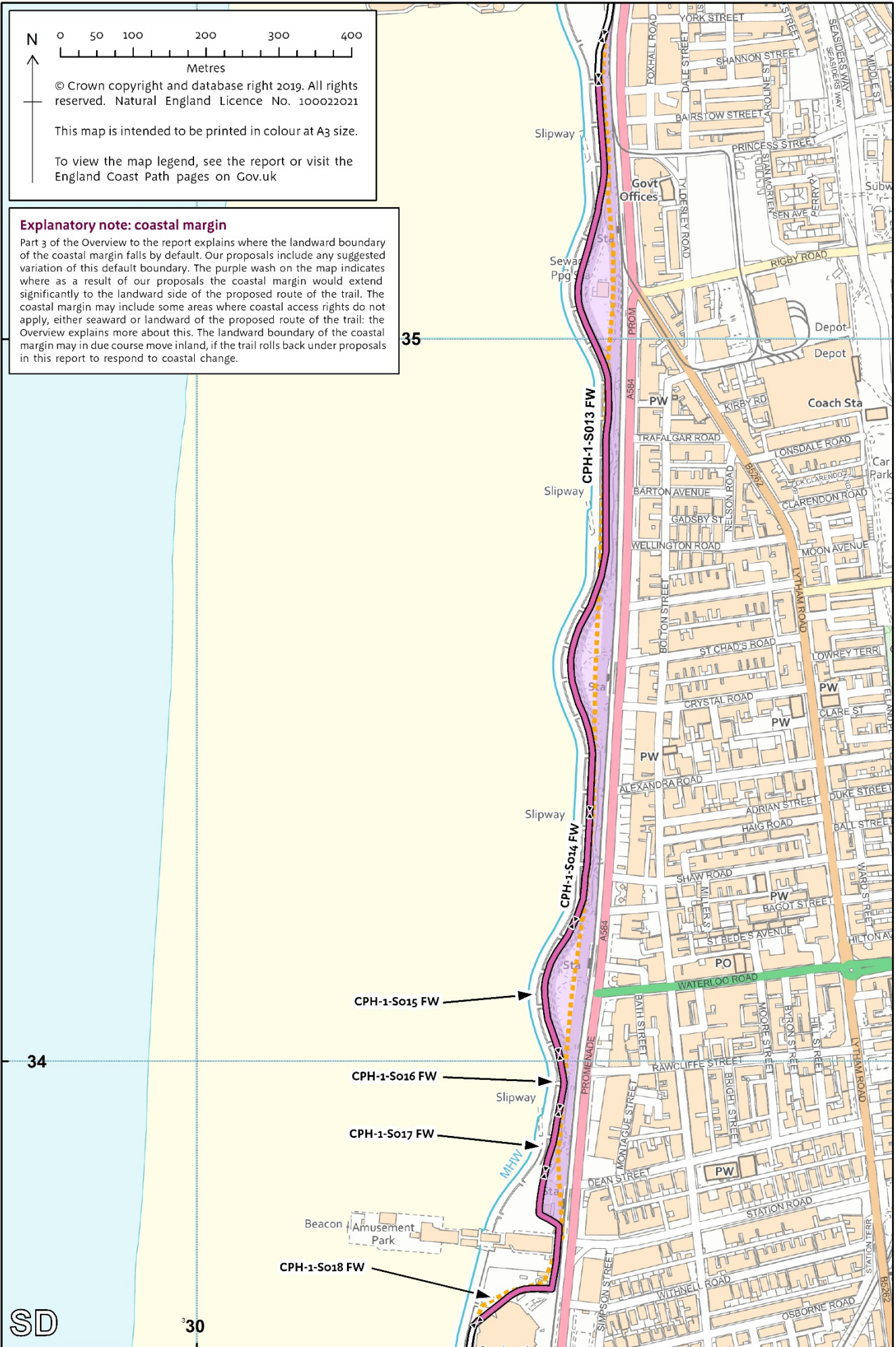
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**Map CPH 1f: Central Pier, Blackpool to Sandcastle Leisure Centre**

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**Explanatory note: coastal margin**  
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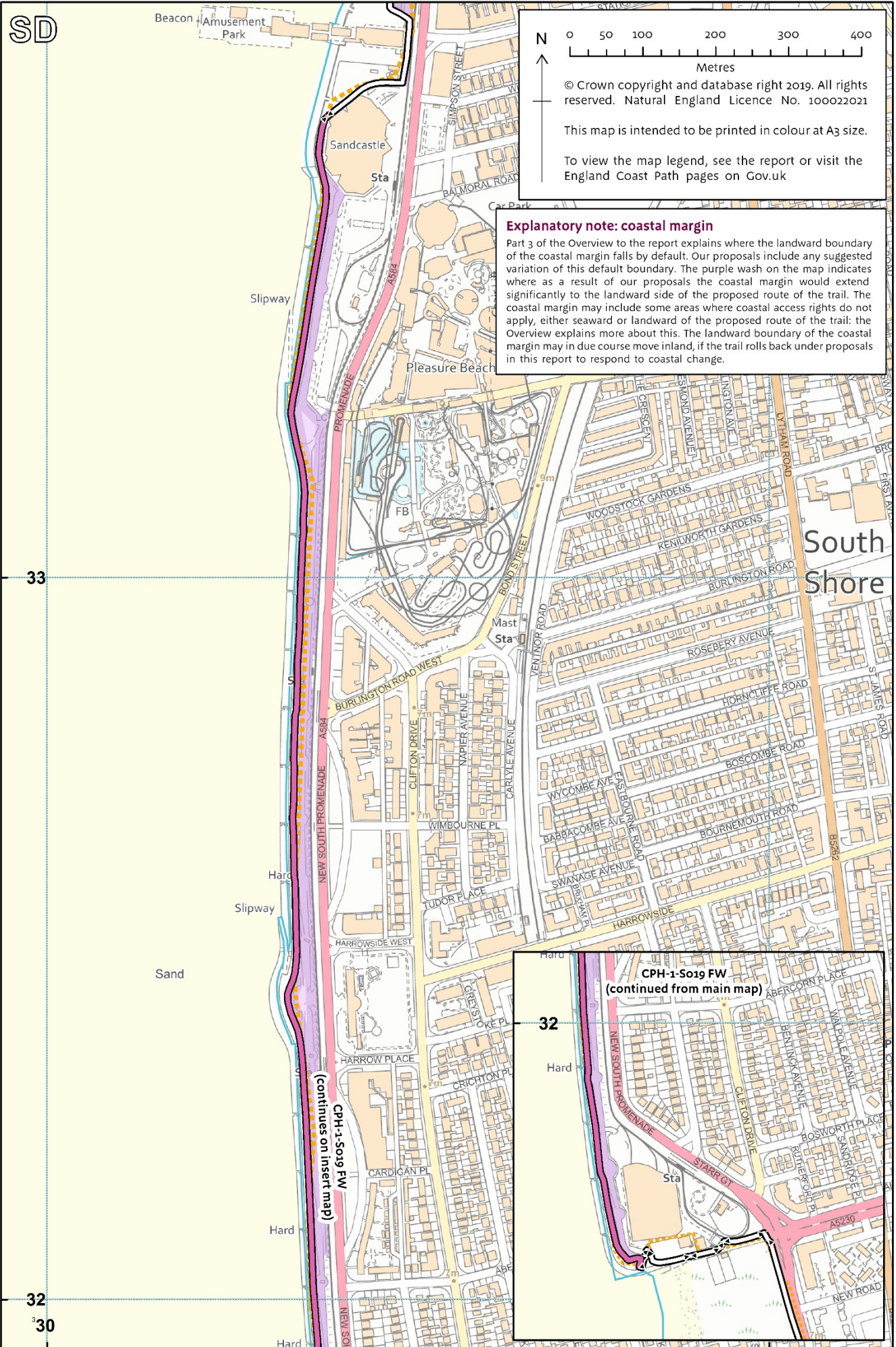


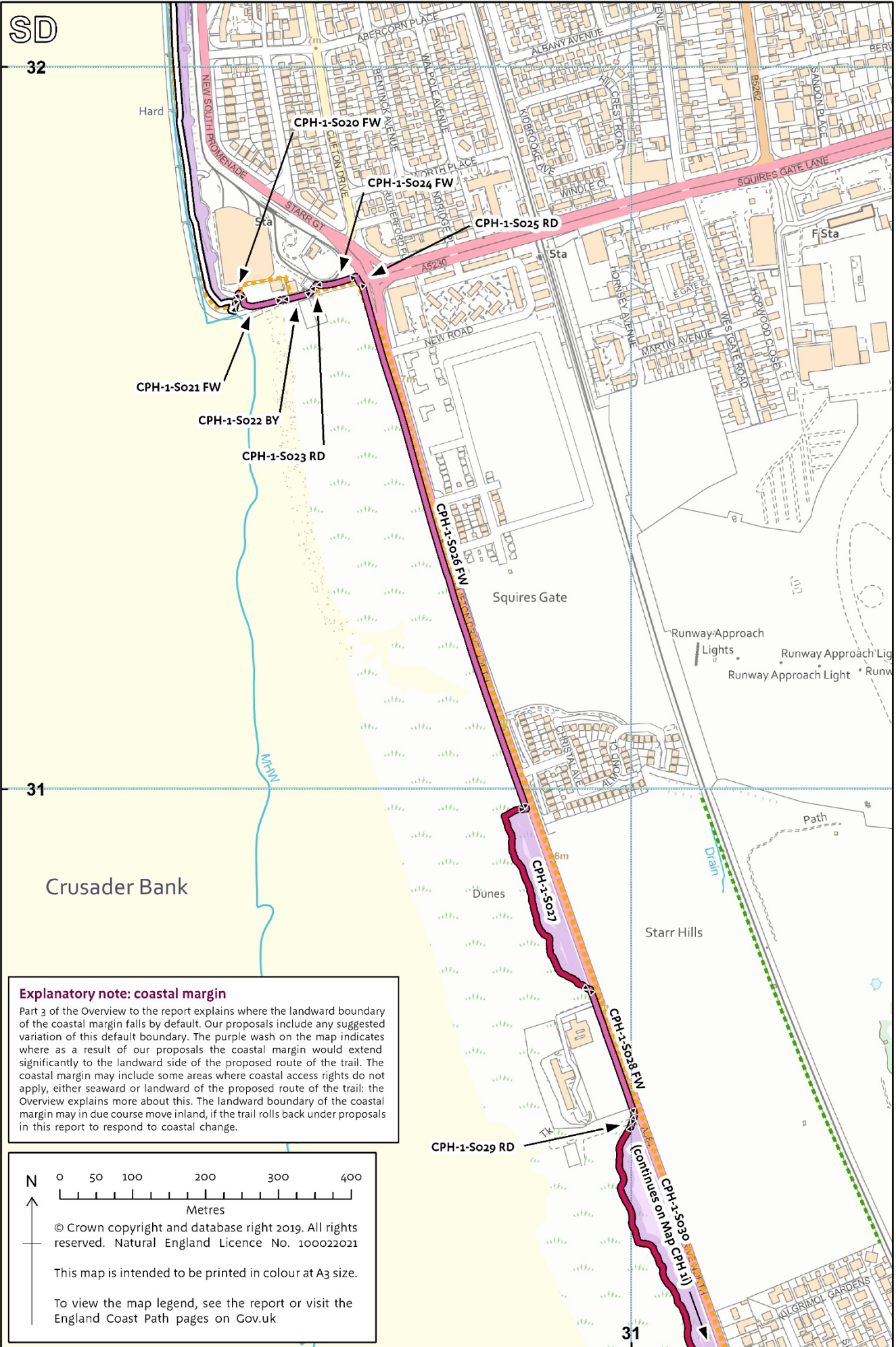
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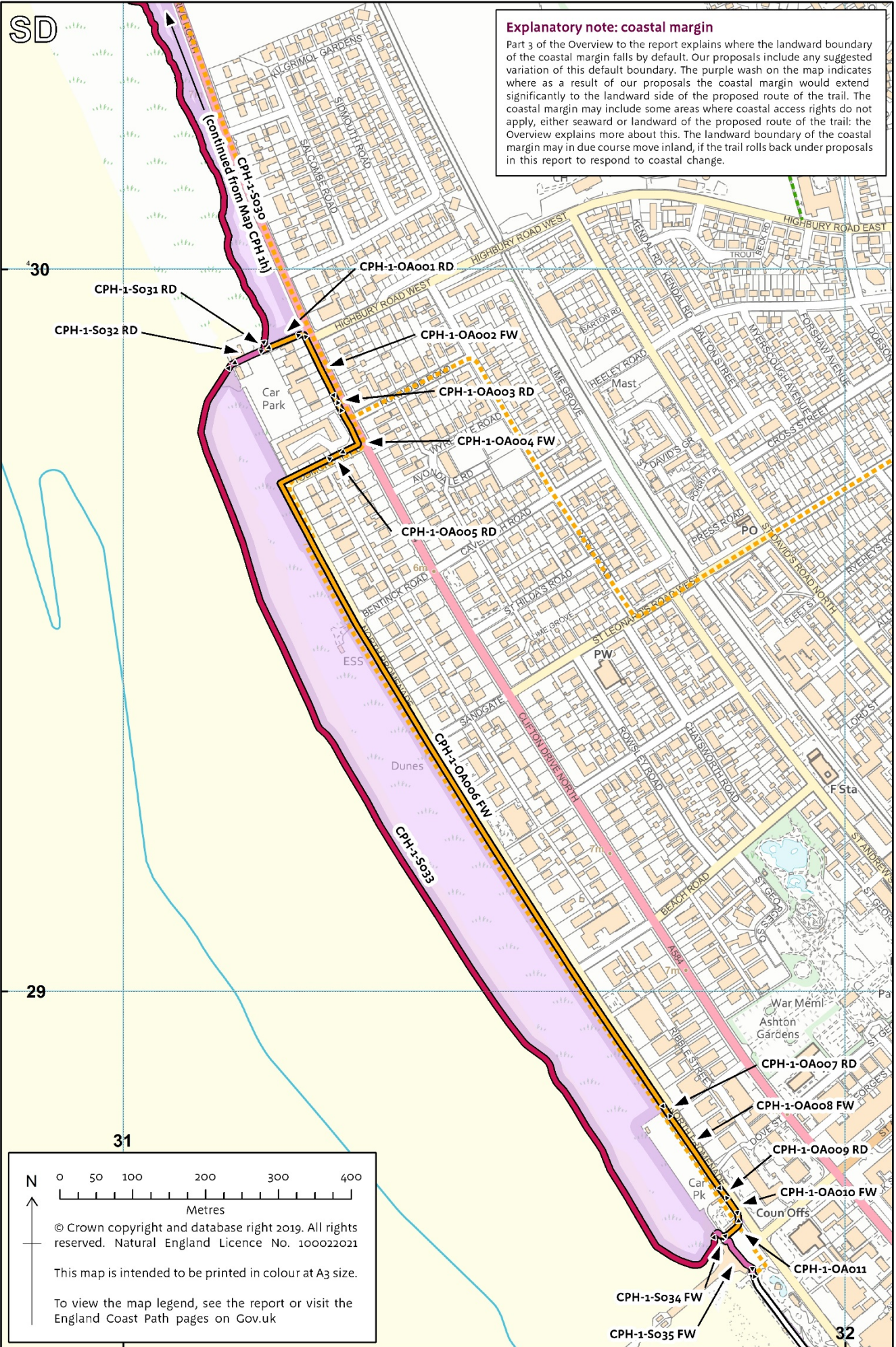


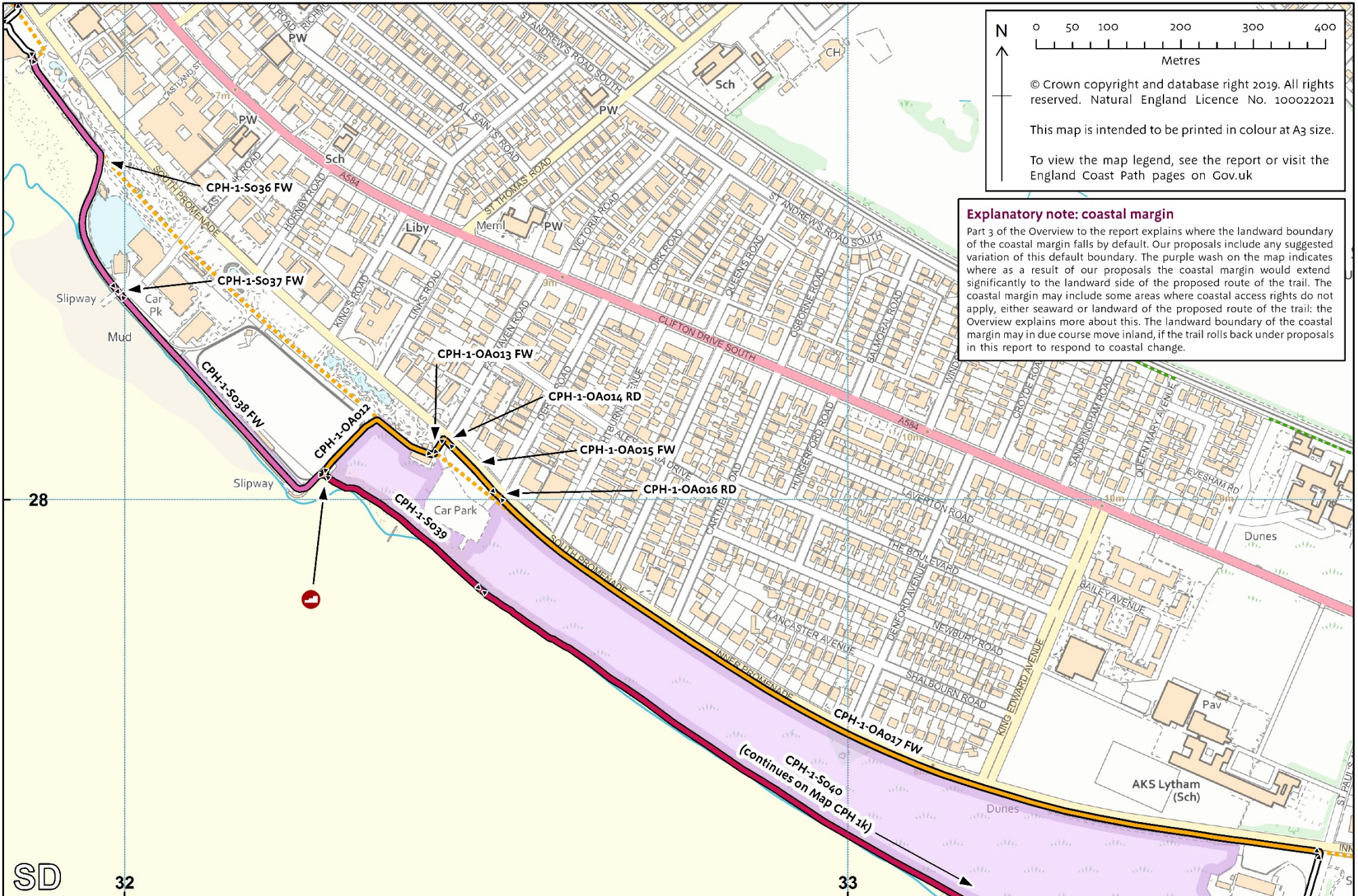


**Explanatory note: coastal margin**  
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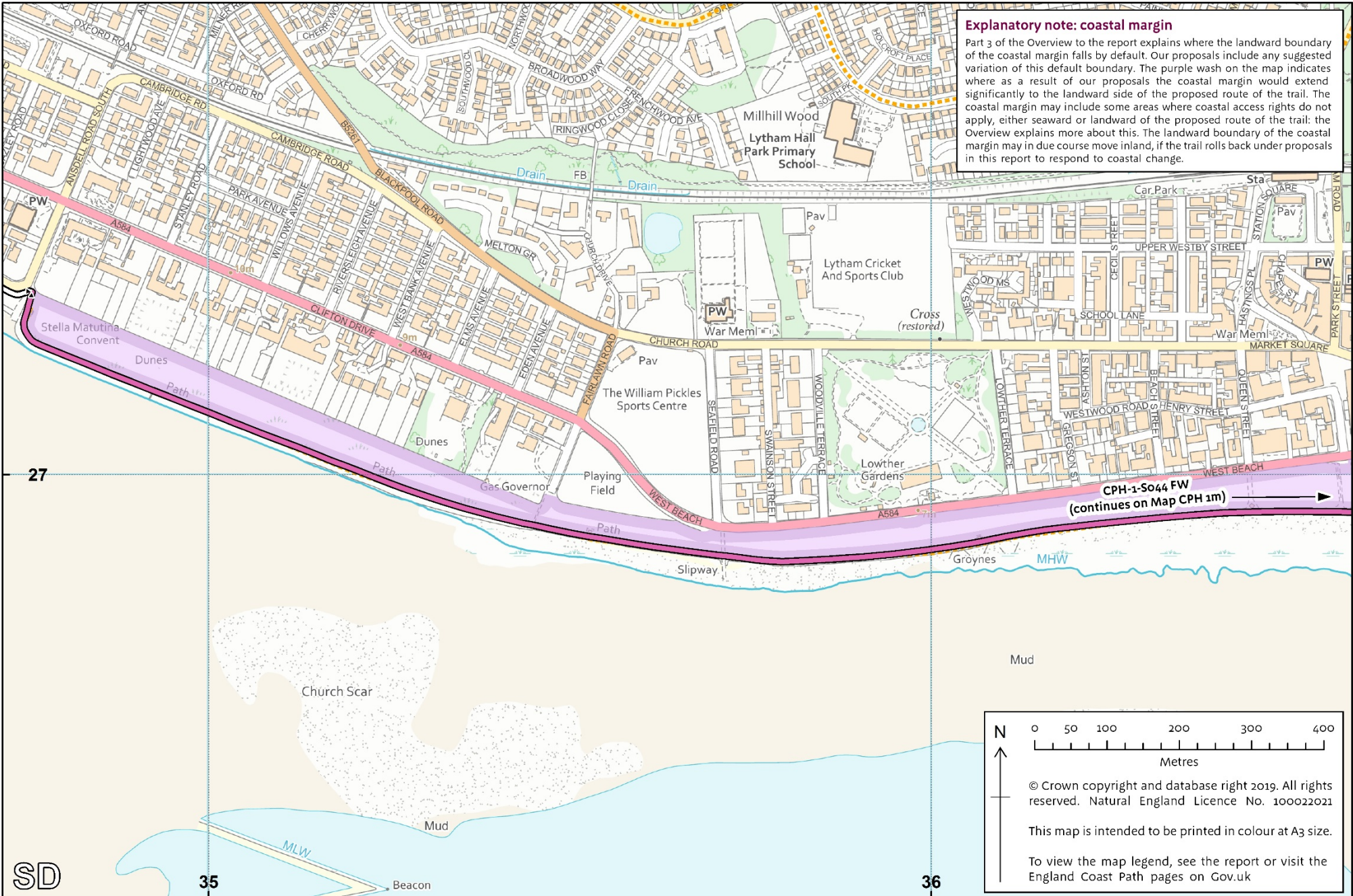
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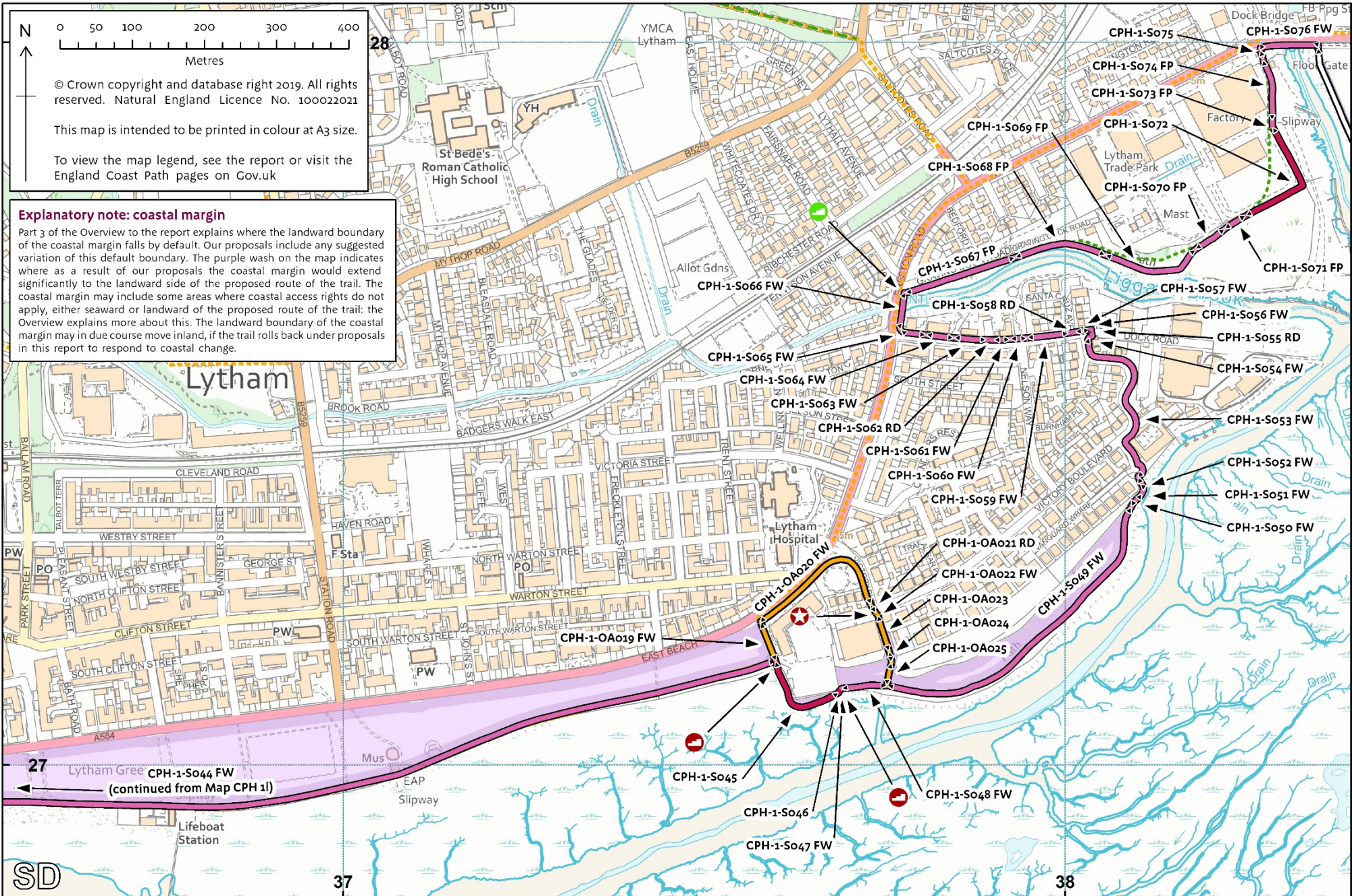




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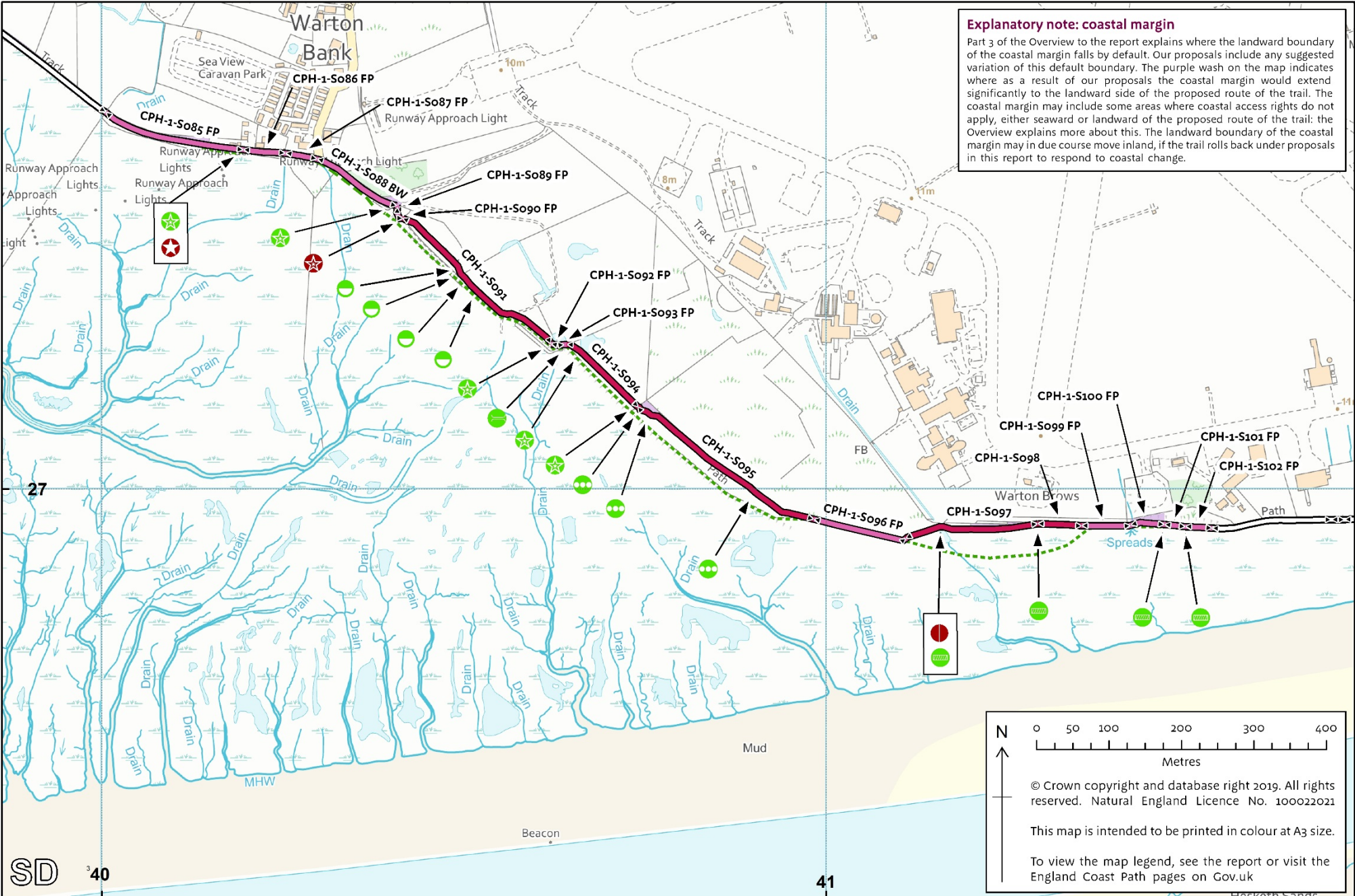


**Explanatory note: coastal margin**

Part 3 of the Overview to the report explains where the landward boundary of the coastal margin falls by default. Our proposals include any suggested variation of this default boundary. The purple wash on the map indicates where as a result of our proposals the coastal margin would extend significantly to the landward side of the proposed route of the trail. The coastal margin may include some areas where coastal access rights do not apply, either seaward or landward of the proposed route of the trail: the Overview explains more about this. The landward boundary of the coastal margin may in due course move inland, if the trail rolls back under proposals in this report to respond to coastal change.



Map CPH 10: Sea View Caravan Park, Warton Bank to Warton Aerodrome



**Explanatory note: coastal margin**

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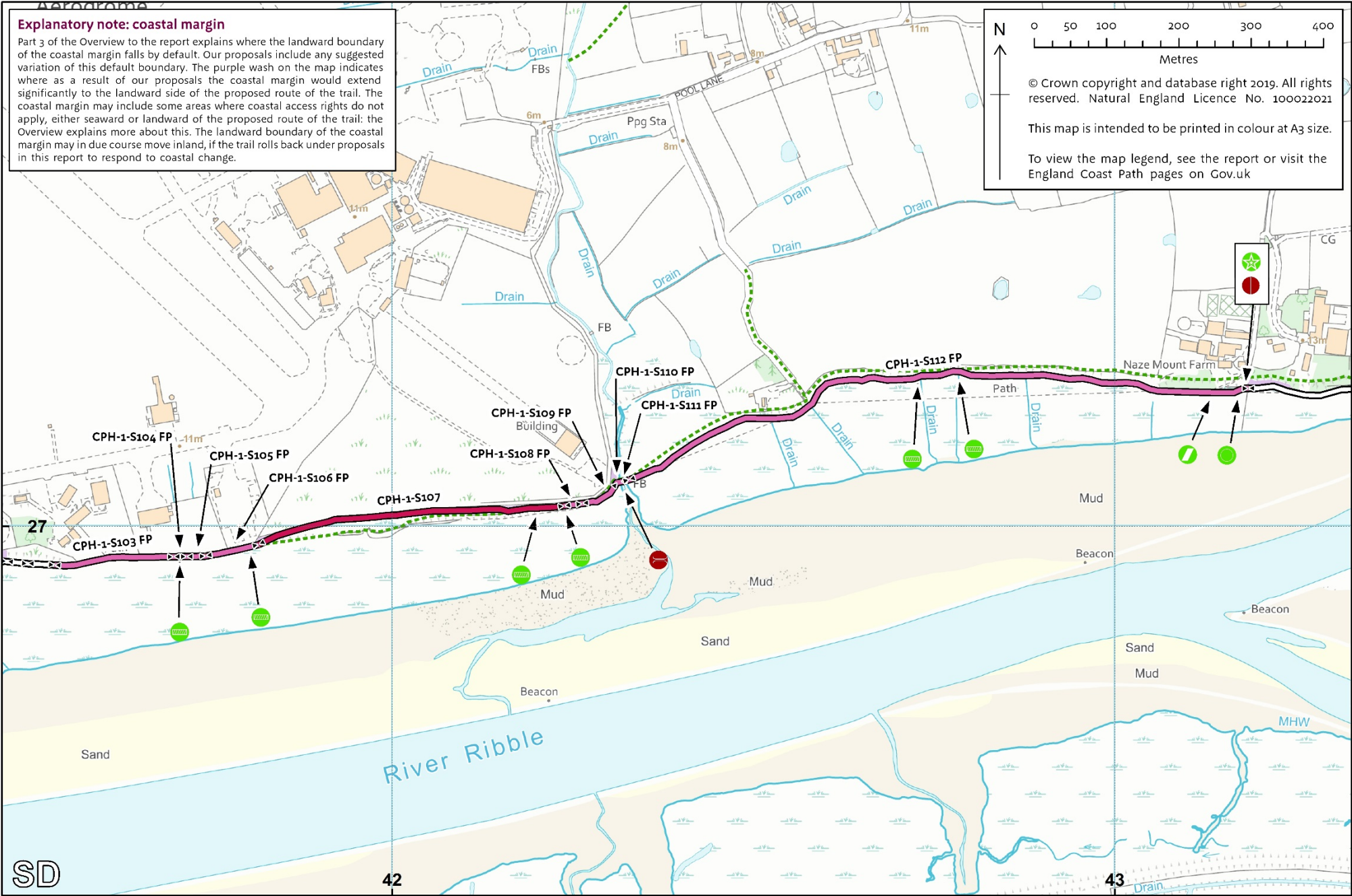
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0 50 100 200 300 400  
Metres

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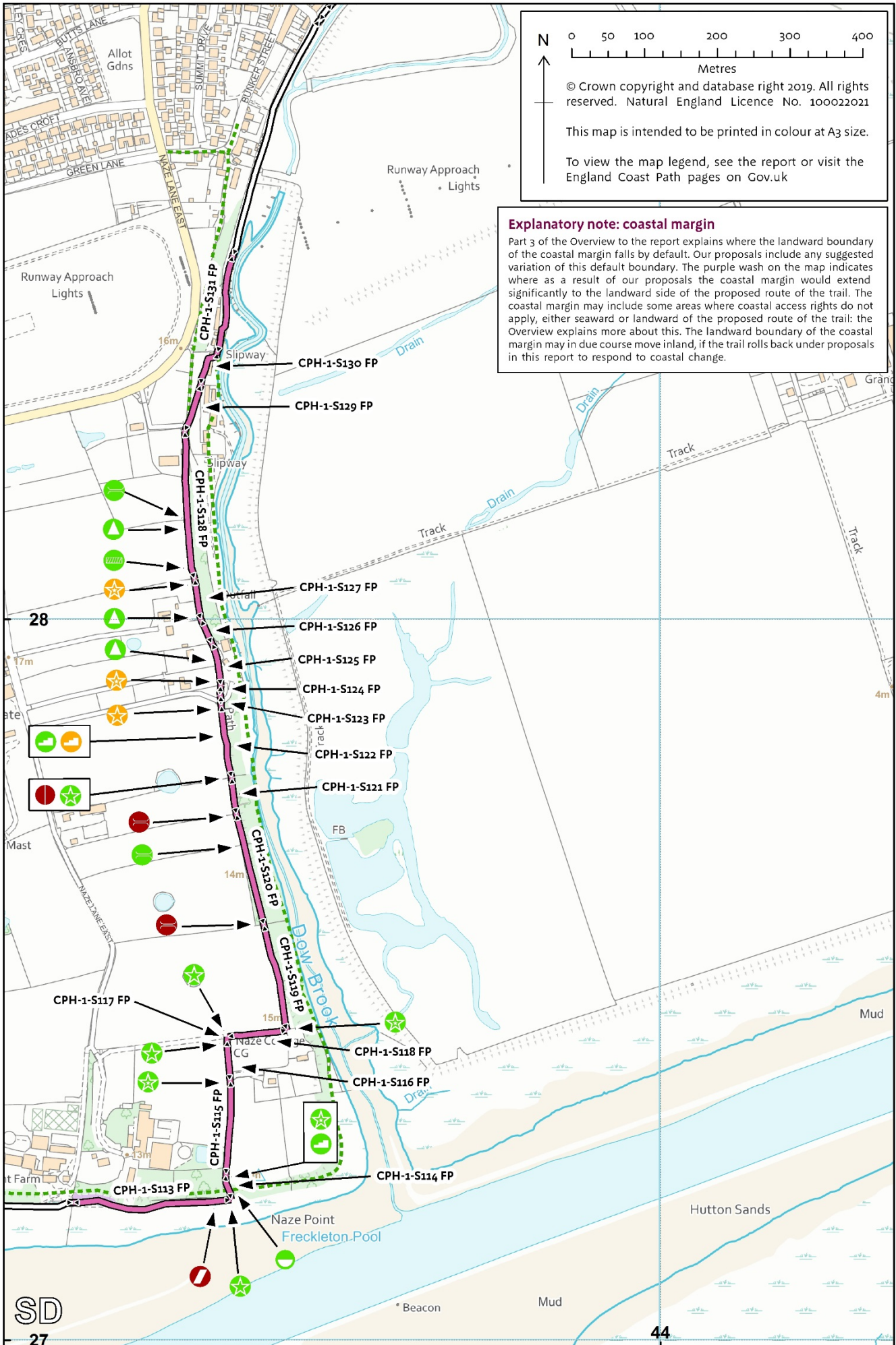
This map is intended to be printed in colour at A3 size.

To view the map legend, see the report or visit the England Coast Path pages on Gov.uk



Map CPH 1p: Warton Aerodrome to Naze Mount Farm

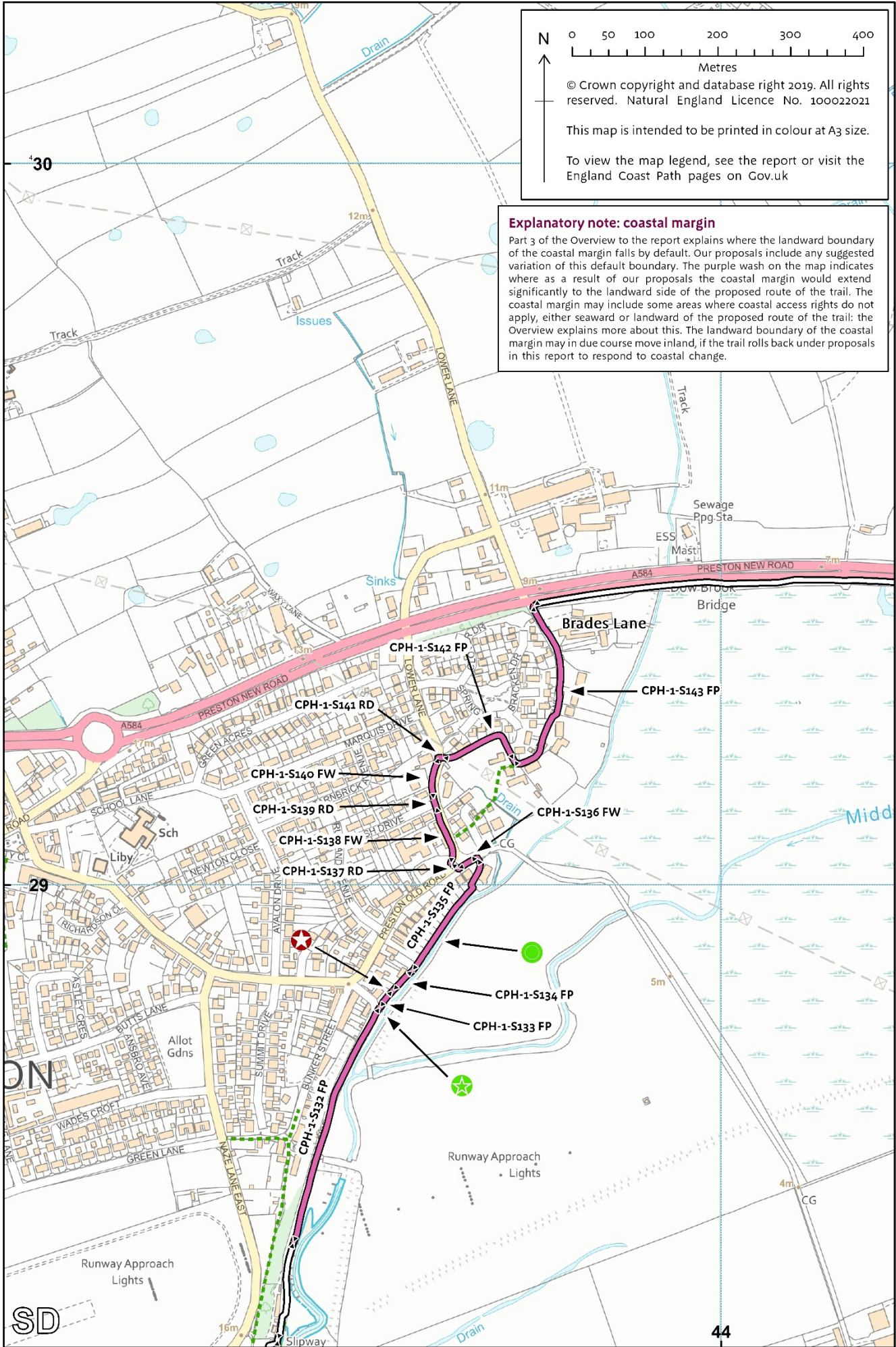




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 To view the map legend, see the report or visit the England Coast Path pages on Gov.uk

**Explanatory note: coastal margin**  
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Map CPH 1q: Naze Mount Farm to Naze Lane East



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 To view the map legend, see the report or visit the England Coast Path pages on Gov.uk

**Explanatory note: coastal margin**  
 Part 3 of the Overview to the report explains where the landward boundary of the coastal margin falls by default. Our proposals include any suggested variation of this default boundary. The purple wash on the map indicates where as a result of our proposals the coastal margin would extend significantly to the landward side of the proposed route of the trail. The coastal margin may include some areas where coastal access rights do not apply, either seaward or landward of the proposed route of the trail: the Overview explains more about this. The landward boundary of the coastal margin may in due course move inland, if the trail rolls back under proposals in this report to respond to coastal change.

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