



England Coast Path Stretch: Southend-on-Sea to Wallasea Island

Report SWI 1: Barge Pier to Landwick Cottages

Part 1.1: Introduction

Start Point:	Barge Pier, Shoebury Ness (Grid reference TQ 9311 8396)
End Point:	Landwick Cottages, Great Wakering (Grid reference TQ 9585 8765)
Relevant Maps:	SWI 1a to SWI 1d

1.1.1 This is one of a series of linked but legally separate reports published by Natural England under section 51 of the National Parks and Access to the Countryside Act 1949, which make proposals to the Secretary of State for improved public access along and to this stretch of coast between Southend-on-Sea and Wallasea Island.

1.1.2 This report covers length SWI 1 of the stretch, which is the coast between Barge Pier and Landwick Cottages. It makes free-standing statutory proposals for this part of the stretch, and seeks approval for them by the Secretary of State in their own right under section 52 of the National Parks and Access to the Countryside Act 1949.

1.1.3 The report explains how we propose to implement the England Coast Path (“the trail”) on this part of the stretch, and details the likely consequences in terms of the wider ‘Coastal Margin’ that will be created if our proposals are approved by the Secretary of State. Our report also sets out:

- any proposals we think are necessary for restricting or excluding coastal access rights to address particular issues, in line with the powers in the legislation; and
- any proposed powers for the trail to be capable of being relocated on particular sections (“roll-back”), if this proves necessary in the future because of coastal change.

1.1.4 There is also a single Overview document for the whole of this stretch of coast, explaining common principles and background. **This and the other individual reports relating to the stretch should be read in conjunction with the Overview. The Overview explains, among other things, how we have considered any potential environmental impacts of improving public access to this part of the coast, and this report, and other separately published assessments we refer to, then provides more detail on these aspects where appropriate.**

Part 1.2: Proposals Narrative

The trail:

1.2.1 Follows existing walked routes, including public rights of way, along most of this length. This length is characterised by contrasting land use types, from the busy urban edges of the Thames Estuary at Southend and Shoeburyness to the lesser populated ancient villages, such as Great Wakering. The strategic military importance of the outer Thames estuary has been long recognised with the establishment of MOD Shoeburyness and Shoebury Garrison. Historic military landmarks are commemorated at Gunners Park, Shoeburyness (public open space).

1.2.2 Between Barge Pier and East Beach (Shoeburyness) follows the coastline quite closely and maintains good views of the sea (SWI-1-S001 to SWI-1-S017, maps SWI 1a and 1b).

1.2.3 Includes a significant inland route between East Beach, Shoeburyness and Oxenham Farm, Great Wakering (SWI-1-S018 to SWI-1-S070, maps SWI 1b to 1d). This is necessary to bypass Ministry of Defence (MOD) Shoeburyness which is used for defence activities. There are no sea views along this part of the route due to the intervening tracts of low-lying land and numerous man-made structures. The MOD land to seaward is automatically excepted from coastal access rights due to military byelaws (see part 5e of the Overview).

1.2.4 Follows part of the existing Sustrans National Cycle Network (Route 16) at Gunners Park (route sections SWI-1-S001 to SWI-1-S011 on map SWI 1a).

Protection of the environment:

1.2.5 In this part of the report, we explain how we have taken account of environmental protection objectives in developing our proposals for improved coastal access.

1.2.6 The following designated sites affect this length of coast (See Overview Maps C1, C2 and D):

- Essex Estuaries Special Area of Conservation (SAC)
- Outer Thames Estuary Special Protection Area (SPA)
- Benfleet and Southend Marshes SPA
- Benfleet and Southend Marshes Ramsar site
- Benfleet and Southend Marshes Site of Special Scientific Interest (SSSI) designated for its wildlife interest.
- Foulness SPA
- Foulness Ramsar site
- Foulness SSSI designated for its wildlife interest
- Danish Camp, Shoeburyness Scheduled Monument (SM)
- Cold War defence boom, Pig's Bay, Shoeburyness SM

The following table brings together design features included in our access proposals to help to protect the environment along this length of the coast.

1.2.7 Measures to protect the environment

Map(s)	Route section number(s)	Design features of the access proposals	Reason included
SWI 1a and SWI 1b	SWI-1-S001 to SWI-1-S017	<p>The following design features are described elsewhere in this report:</p> <ul style="list-style-type: none"> Coastal access rights are excepted by default (military byelaw land) over the intertidal areas at Gunners Park and East Beach. <p>In addition, we will install:</p> <ul style="list-style-type: none"> Notices at the northern and southern ends of East Beach explaining the nature conservation sensitivities in the intertidal area and asking people to avoid them (access controlled by MOD + Qinetiq). 	<p>To reduce the risk of damage to eelgrass beds (growing in the upper intertidal zone) and shingle/sand strandline vegetation being trampled by people walking along the shore.</p> <p>No specific measures are needed to protect Scheduled Monuments, as they are either landward of the coastal margin or within the military byelaw areas (intertidal).</p>

1.2.8 Natural England is satisfied that the proposals for coastal access in this report are made in accordance with relevant environmental protection legislation. In respect of cultural heritage, we have taken advice from Historic England and others before confirming this conclusion. For more information about how we came to this conclusion in respect of the natural environment; see the following assessments of the access proposals that we have published separately:

- A Habitats Regulations Assessment relating to any potential impact on the conservation objectives of European sites.
- Our Nature Conservation Assessment, in which we document our conclusions in relation to other potential impacts on nature conservation.

Part 6b of the Overview includes some contextual information about protecting the environment along this length of coast.

Accessibility:

1.2.9 There are few artificial barriers to accessibility on the proposed route. However the natural coastal terrain is also challenging for people with reduced mobility and this is the case on sections of our proposed route because the trail would follow uneven ground along some existing public rights of way and other walked routes, such as at East Beach (SWI-1-S014 to SWI-1-S017, map SWI 1b) and Great Wakering Common (SWI-1-S064 to SWI-1-S070, map SWI 1d).

See part 6a of the Overview - 'Recreational issues' - for more information.

Where we have proposed exercising statutory discretions:

1.2.10 **Estuary:** This report proposes that the trail should contain sections aligned on the estuary of the River Thames, extending upstream from the open coast. Natural England proposes to exercise its functions as if the sea included the estuarial waters of that river between Barge Pier (TQ 931 839) and Haven Point (TQ 977 877), as indicated by the extent of the trail shown on maps SWI 1a to 1d.

See part 5 of the Overview for a detailed analysis of the options considered for this estuary and our resulting proposals.

1.2.11 **Landward boundary of the coastal margin:** We have used our discretion on some sections of the route to map the landward extent of the coastal margin to an adjacent physical boundary such as a fence line, pavement or track to make the extent of the new access rights clearer. See Table 1.3.1 below.

1.2.12 In one place we have used our discretion to propose the inclusion of additional, more extensive landward areas within the coastal margin, to secure or enhance public enjoyment of this part of the coast. The owner of this land is content for us to propose this.

1.2.13 The Proposals Tables show where we are proposing to alter the default landward boundary of the coastal margin. These proposals are set out in columns 5b and 5c of Table 1.3.1. Where these columns are left blank, we are making no such proposals, so the default landward boundary applies. See the note relating to columns 5b & 5c [above Table 1.3.1] explaining what this means in practice.

See also part 3 of the Overview - 'Understanding the proposals and accompanying maps', for a more detailed explanation of the default extent of the coastal margin and how we may use our discretion to adjust the margin, either to add land or to provide clarity.

1.2.14 **Coastal erosion:** Natural England is able to propose that the route of the trail would be able to change in the future, without further approval from the Secretary of State, in response to coastal change. This would happen in accordance with the criteria and procedures for 'roll-back' set out in part 7 of the Overview.

Natural England may only propose the use of this roll-back power:

- as a result of coastal erosion or other geomorphological processes or encroachment by the sea, or
- in order to link with other parts of the route that need to roll back in direct response to such changes.

1.2.15 We have chosen not to make any such proposal in this report. Accordingly the route is to be at the centre of the line shown on maps SWI 1a to SWI 1d as the proposed route of the trail.

Other future change:

1.2.16 At this point we do not foresee any need for future changes to the access provisions that we have proposed within this report.

Establishment of the trail:

1.2.17 Below we summarise how our proposed route for the trail would be physically established to make it ready for public use before any new rights come into force. Establishment works will only start on this length of coast once these proposals have been approved by the Secretary of State. The works may therefore either precede or follow the start of establishment works on other lengths of coast within the stretch, and detailed in their separate reports.

1.2.18 Our estimate of the capital costs for physical establishment of the trail on the proposed route is £12,200 and is informed by:

- information already held by the access authorities, Essex County Council and Southend-on-Sea Borough Council, in relation to the management of the existing public rights of way and other walked routes;
- the conclusions of our deliberations in relation to potential impacts on the environment; and
- information gathered while visiting affected land and talking to the people who own and manage it about the options for the route.

1.2.19 There are two elements to the overall cost:

- **New Signs:** A number of new signs (way-marking discs and fingerposts) would be needed on the trail, in particular on route sections where the proposed route differs from that of the existing public right of way network or where route-finding is complex (through the urban areas).
- Some initial vegetation clearance will be needed in some areas to make the route more open and accessible.

1.2.20 The surfaces and access furniture of the existing paths and footways on the proposed route are generally of a suitable standard for the trail, no new or replacement access furniture is proposed.

Table 1 shows our estimate of the capital cost for each of the main elements of physical establishment described above.

Table 1: Estimate of capital costs

Item	Cost
Signs & interpretation	£11,600
Hedge cutting	£600
Total	£12,200 (Exclusive of any VAT payable)

1.2.21 Once the Secretary of State's decision on our report has been notified, and further to our conversations with land managers during the route planning stage, there will be suitable liaison with affected land owners and occupiers about relevant aspects of the design, installation and maintenance of the new signs and infrastructure that are needed on their land. Prior to works being carried out on the ground, all necessary permissions, authorisations and consents will be obtained. All such works would conform to the published standards for National Trails and the other criteria described in our Coastal Access Scheme.

Maintenance of the trail:

1.2.22 Because the trail on this length of coast will form part of the National Trail being created around the whole coast of England called the England Coast Path, we envisage that it will be maintained to the same high quality standards as other National Trails in England (see The New Deal; Management of National Trails in England from April 2013: details at Annex A of the Overview).

1.2.23 We estimate that the annual cost to maintain the trail will be £1,882 (exclusive of any VAT payable). In developing this estimate we have taken account of the formula used to calculate Natural England's contribution to the maintenance of other National Trails.

Part 1.3: Proposals Tables

See Part 3 of Overview for guidance on reading and understanding the tables below

1.3.1 Section Details – Maps SWI 1a to SWI 1d: Barge Pier to Landwick Cottages

Key notes on table:

1. Column 2 – an asterisk (*) against the route section number means see also table 1.3.2: Other options considered.
2. Column 4 – ‘No’ means no roll-back is proposed for this route section. ‘Yes – normal’ means roll-back is proposed and is likely to follow the current feature (e.g. cliff edge/beach) for the foreseeable future as any coastal change occurs.
3. Column 5a - Certain coastal land types are included automatically in the coastal margin where they fall landward of the trail if they touch it at some point. The relevant land type (foreshore, cliff, bank, barrier, dune, beach, flat or section 15 land – see Glossary) is shown in this column where appropriate. “No” means none present on this route section.
4. Columns 5b and 5c – Any entry in these columns means we are proposing to align the landward boundary of the coastal margin on this route section with the physical feature(s) shown in 5b, for the reason in 5c. No text here means that for this route section the landward edge of the margin would be that of the trail itself - or if any default coastal land type is shown in 5a, that would be its landward boundary instead.

1	2	3	4	5a	5b	5c	6
Map(s)	Route section number(s)	Current status of route section(s)	Roll-back proposed? (See Part 7 of Overview)	Landward margin contains coastal land type?	Proposal to specify landward boundary of margin (See maps)	Reason for landward boundary proposal	Explanatory notes
1a	SWI-1-S001	Other existing walked route	No	No	Path	Clarity and cohesion	
1a	SWI-1-S002	Other existing walked route	No	No	Fence line	Clarity and cohesion	
1a	SWI-1-S003	Other existing walked route	No	No	Path	Clarity and cohesion	
1a	SWI-1-S004	Other existing walked route	No	No	Path	Clarity and cohesion	
1a	SWI-1-S005*	Other existing walked route	No	No	Path	Clarity and cohesion	
1a	SWI-1-S006*	Other existing walked route	No	No			
1a	SWI-1-S007*	Other existing walked route	No	No	Pavement edge	Clarity and cohesion	
1a	SWI-1-S008*	Other existing walked route	No	No	Pavement edge	Clarity and cohesion	

1	2	3	4	5a	5b	5c	6
Map(s)	Route section number(s)	Current status of route section(s)	Roll-back proposed? (See Part 7 of Overview)	Landward margin contains coastal land type?	Proposal to specify landward boundary of margin (See maps)	Reason for landward boundary proposal	Explanatory notes
1a	SWI-1-S009*	Other existing walked route	No	No			
1a	SWI-1-S010*	Other existing walked route	No	No	Path	Clarity and cohesion	
1a	SWI-1-S011*	Other existing walked route	No	No	Various	Clarity and cohesion	Pavement edge, hedge line and fence
1b	SWI-1-S012	Other existing walked route	No	No			
1b	SWI-1-S013	Other existing walked route	No	No			
1b	SWI-1-S014	Other existing walked route	No	No			
1b	SWI-1-S015	Other existing walked route	No	No			
1b	SWI-1-S016	Other existing walked route	No	No			
1b	SWI-1-S017	Other existing walked route	No	No			
1b	SWI-1-S018	Other existing walked route	No	No			
1b	SWI-1-S019	Other existing walked route	No	No	Various	Clarity and cohesion	Path edge and tall metal fence landward of grass verge
1b	SWI-1-S020	Public footway (pavement)	No	No	Pavement edge	Clarity and cohesion	Pavement runs parallel to road
1b	SWI-1-S021	Byway open to all traffic	No	No			Crossing road entrance to caravan site
1b	SWI-1-S022	Public footway (pavement)	No	No	Pavement edge	Clarity and cohesion	
1b	SWI-1-S023	Public footway (pavement)	No	No	Fence line	Clarity and cohesion	

1	2	3	4	5a	5b	5c	6
Map(s)	Route section number(s)	Current status of route section(s)	Roll-back proposed? (See Part 7 of Overview)	Landward margin contains coastal land type?	Proposal to specify landward boundary of margin (See maps)	Reason for landward boundary proposal	Explanatory notes
1b	SWI-1-S024	Public footway (pavement)	No	No	Pavement edge	Clarity and cohesion	
1b	SWI-1-S025	Byway open to all traffic	No	No			Crossing road entrance to caravan park
1b	SWI-1-S026	Public footway (pavement)	No	No	Pavement edge	Clarity and cohesion	
1b	SWI-1-S027*	Public footway (pavement)	No	No	Pavement edge	Clarity and cohesion	
1b	SWI-1-S028*	Public highway (road)	No	No			
1b	SWI-1-S029*	Public footway (pavement)	No	No	Various	Clarity and cohesion	Pavement edge, wall and short hedge
1b	SWI-1-S030*	Public highway (road)	No	No			
1b	SWI-1-S031*	Public footway (pavement)	No	No	Wall	Clarity and cohesion	
1b	SWI-1-S032*	Public highway (road)	No	No			
1b	SWI-1-S033*	Public footway (pavement)	No	No	Pavement edge	Clarity and cohesion	
1b	SWI-1-S034*	Public footway (pavement)	No	No	Pavement edge	Clarity and cohesion	
1b	SWI-1-S035*	Public footway (pavement)	No	No	Pavement edge	Clarity and cohesion	
1b	SWI-1-S036	Public highway (road)	No	No			
1b	SWI-1-S037	Public footway (pavement)	No	No	Various	Clarity and cohesion	Various low residential property walls
1b	SWI-1-S038	Public footway (pavement)	No	No	Various	Clarity and cohesion	Various low residential property walls

1	2	3	4	5a	5b	5c	6
Map(s)	Route section number(s)	Current status of route section(s)	Roll-back proposed? (See Part 7 of Overview)	Landward margin contains coastal land type?	Proposal to specify landward boundary of margin (See maps)	Reason for landward boundary proposal	Explanatory notes
1b	SWI-1-S039	Public footway (pavement)	No	No	Pavement edge	Clarity and cohesion	
1b	SWI-1-S040	Public footway (pavement)	No	No	Promenade edge	Clarity and cohesion	
1b	SWI-1-S041	Public footway (pavement)	No	No	Hedgerow	Clarity and cohesion	
1c	SWI-1-S042	Public footway (pavement)	No	No			Entrance to business units
1c	SWI-1-S043	Public footway (pavement)	No	No	Hedgerow	Clarity and cohesion	
1c	SWI-1-S044	Public footway (pavement)	No	No	Pavement edge	Clarity and cohesion	
1c	SWI-1-S045	Public footway (pavement)	No	No	Pavement edge	Clarity and cohesion	
1c	SWI-1-S046*	Public highway (road)	No	No			
1c	SWI-1-S047*	Public footpath	No	No			
1c	SWI-1-S048*	Public footpath	No	No			
1c	SWI-1-S049*	Public footpath	No	No			
1c	SWI-1-S050*	Public footpath	No	No	Ditch	Clarity and cohesion	Deep ditch approximately 5m from trail
1c	SWI-1-S051*	Public footpath	No	No	Various	Clarity and cohesion	Post and mesh fence, post and rail and then hedge
1c	SWI-1-S052*	Public footway (pavement)	No	No			
1c	SWI-1-S053*	Public highway (road)	No	No			
1c	SWI-1-S054*	Public footway (pavement)	No	No			

1	2	3	4	5a	5b	5c	6
Map(s)	Route section number(s)	Current status of route section(s)	Roll-back proposed? (See Part 7 of Overview)	Landward margin contains coastal land type?	Proposal to specify landward boundary of margin (See maps)	Reason for landward boundary proposal	Explanatory notes
1c	SWI-1-S055*	Public footpath	No	No	Hedgerow	Clarity and cohesion	
1c	SWI-1-S056*	Public footway (pavement)	No	No	Pavement edge	Clarity and cohesion	
1c	SWI-1-S057*	Public highway (road)	No	No			
1c	SWI-1-S058*	Public footway (pavement)	No	No	Various	Clarity and cohesion	Various walls gateways and house frontages
1c	SWI-1-S059*	Public highway (road)	No	No			
1c	SWI-1-S060*	Public highway (road)	No	No			
1c	SWI-1-S061*	Public footway (pavement)	No	No	Pavement edge	Clarity and cohesion	
1c	SWI-1-S062*	Public highway (road)	No	No			
1c	SWI-1-S063*	Public footway (pavement)	No	No	Pavement edge	Clarity and cohesion	
1d	SWI-1-S064*	Other existing walked route	No	No	Road	Additional landward area	
1d	SWI-1-S065*	Other existing walked route	No	No	Ditch	Additional landward area	Concurrent with boundary of the common which is existing access land
1d	SWI-1-S066*	Public footpath	No	No	Ditch	Additional landward area	Concurrent with boundary of common which is existing access land
1d	SWI-1-S067*	Other existing walked route	No	No	Ditch	Additional landward area	Concurrent with boundary of common which is

1	2	3	4	5a	5b	5c	6
Map(s)	Route section number(s)	Current status of route section(s)	Roll-back proposed? (See Part 7 of Overview)	Landward margin contains coastal land type?	Proposal to specify landward boundary of margin (See maps)	Reason for landward boundary proposal	Explanatory notes
							existing access land
1d	SWI-1-S068*	Public footpath	No	No	Ditch	Additional landward area	Concurrent with boundary of common which is existing access land
1d	SWI-1-S069*	Public footpath	No	No	Ditch	Additional landward area	Concurrent with boundary of common which is existing access land
1d	SWI-1-S070*	Public footpath	No	No	Ditch	Additional landward area	Concurrent with boundary of common which is existing access land

1.3.2 Other options considered: Maps SWI 1a to SWI 1d: Barge Pier to Landwick Cottages

Map(s)	Route section numbers(s)	Other option(s) considered	Reasons for not proposing this option
SWI 1a	SWI-1-S008 to SWI-1-S010	We considered aligning the trail seaward of 1-19 Mess Road at the base of the seaward slope (directly adjacent to the MOD Danger Area). No other options were identified for the trail in relation to this map	<p>We opted for the proposed route because:</p> <ul style="list-style-type: none"> ■ Although alignment seaward of the houses would have better sea views it was convoluted in nature and physically difficult to walk with many barriers to onward progress ■ An alignment at the base of the seaward slope could increase the risk of people straying in to the Danger Area <p>We concluded that overall the proposed route struck the best balance in terms of the criteria described in chapter 4 of the Coastal Access Scheme</p>

Map(s)	Route section numbers(s)	Other option(s) considered	Reasons for not proposing this option
SWI 1a	SWI-1-S005 to SWI-1-S011	We considered aligning the trail further landward to capture other features of interest at Gunners Park (e.g. Horseshoe Crescent and Warrior Square) plus the local amenities in Shoeburyness High Street	<p>We opted for the proposed route because it is closer to the sea, for the most part maintains views of the sea and offers a better coastal feel than a more inland route</p> <p>We concluded that overall the proposed route struck the best balance in terms of the criteria described in chapter 4 of the Coastal Access Scheme</p>
SWI 1b	SWI-1-S027 to SWI-1-S035	We considered aligning the trail along Peel Avenue pavements to follow the MOD security fence	We opted for the proposed route on advice from Southend-on-Sea Borough Council which, following a road safety assessment, concluded that we should use the pelican controlled crossing at the junction of Blackgate Road and Wakering Road rather than the uncontrolled crossing at the junction of Peel Avenue and Wakering Road
SWI 1c	SWI-1-S047 to SWI-1-S060	We considered aligning the trail via the north side of Poynters Lane (verge and pavement) coupled with a new cross-field route to the north-west of Barnfleet Villas to join the pavement on Shoebury Road, north of Seaview Drive	<p>We opted for the proposed route because:</p> <ul style="list-style-type: none"> ■ It makes use of existing public rights of way that are pleasant and convenient ■ Avoids an additional road crossing ■ It passes through the village of Great Wakering, offering walkers easy access to local facilities. <p>We concluded that overall the proposed route struck the best balance in terms of the criteria described in chapter 4 of the Coastal Access Scheme</p>

Map(s)	Route section numbers(s)	Other option(s) considered	Reasons for not proposing this option
SWI 1c and SWI 1d	SWI-1-S046 to SWI-1-S070	We considered aligning the trail closer to the MOD fence along Poynters Lane, Cupids Chase, public footpath FP13, and New Road (Samuel's Corner)	<p>We opted for the proposed route because:</p> <ul style="list-style-type: none"> ■ It passes through the village of Great Wakering, offering walkers easy access to local facilities. ■ It avoids roads for the most part, including two sections of roads with no pavement (Poynters Lane and New Road) ■ It provides more tranquillity and a better walking experience across Great Wakering Common <p>We concluded that overall the proposed route struck the best balance in terms of the criteria described in chapter 4 of the Coastal Access Scheme</p>

Note: Any public rights of way not forming part of the proposed trail would remain available for people to use under their pre-existing rights.

Part 1.4: Proposals Maps

1.4.1 Map Index

Map reference	Map title
SWI 1a	Barge Pier, Shoebury Ness to Rampart Street, Shoeburyness
SWI 1b	Rampart Street, Shoeburyness to Shoeburyness New Range
SWI 1c	Shoeburyness New Range to Great Wakering Common
SWI 1d	Great Wakering Common to Landwick Cottages, Great Wakering

PROPOSALS

Trail Sections

- Trail using existing public right of way or highway
- Trail using other existing walked route
- Trail not using existing walked route
- Alternative route
- Trail shown on other maps
- Approved or open England Coast Path

Maps that show sections of the trail that follow the existing South West Coast Path as currently walked and managed use the following trail categories. Information on the existing status and infrastructure is not shown.

- Trail using existing South West Coast Path
- Alternative or optional alternative route using existing South West Coast Path

Trail sections which follow existing public rights of way or highways are indicated by a suffix:

- BW** - Public bridleway
- BY** - Public byway
- CP** - Cycletrack (pedestrian)
- CT** - Cycletrack (cycles only)
- FP** - Public footpath
- FW** - Public footway (Pavement)
- RB** - Restricted byway
- RD** - Public road

Coastal Margin

Explanatory note

Part 3 of the Overview to the report explains where the landward boundary of the coastal margin falls by default. Our proposals include any suggested variation of this default boundary. The purple wash on the map indicates where as a result of our proposals the coastal margin would extend significantly to the landward side of the proposed route of the trail. The coastal margin may include some areas where coastal access rights do not apply, either seaward or landward of the proposed route of the trail: the Overview explains more about this. The landward boundary of the coastal margin may in due course move inland, if the trail rolls back under proposals in this report to respond to coastal change.

- Coastal margin landward of the trail
- Coastal margin landward of the trail which is existing access land

Other Information

Other access rights and routes

- Public bridleways
- Public byways
- Public footpaths
- Restricted byways
- South West Coast Path
- Sustrans national routes
- Existing access land

Infrastructure types

For status of each, where shown on map, see colour codes below

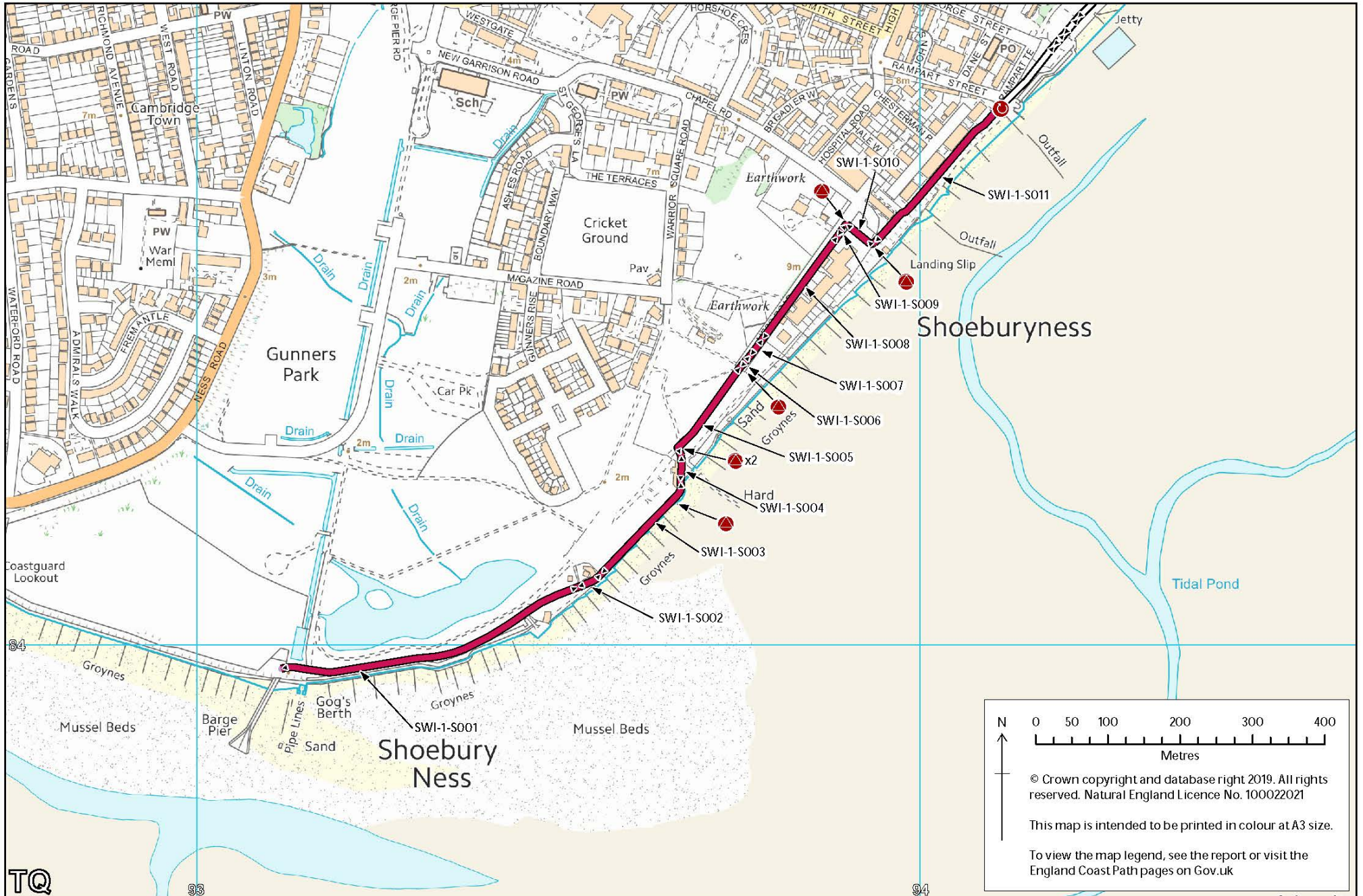
- | Bridges: | Stiles: | Gates: |
|-----------------------|---------------|----------------------|
| Clapper bridge | Ladder stile | Bristol gate |
| Footbridge | Lift-up stile | Field gate |
| Quad bike bridge | Squeeze stile | Gateway with no gate |
| Sleeper bridge | Step stile | Kissing gate |
| Vehicle bridge | Stone stile | Pedestrian gate |
| | | Wheelchair gate |
| Miscellaneous: | | |
| Barrier | Cycle chicane | Interpretation panel |
| Boardwalk | Drainage | Ramp |
| Bollard | Drop-kerb | Revetment |
| Cattle grid | Gap in fence | Stepping stones |
| Culvert | Hurdle | Steps |

Infrastructure status

Each symbol shown on the map is colour coded as appropriate, as in this example for a set of steps:

- Existing steps to be retained
- New steps required
- Existing steps to be removed

* Please note that the items in this legend may not all be present on an individual map or report.

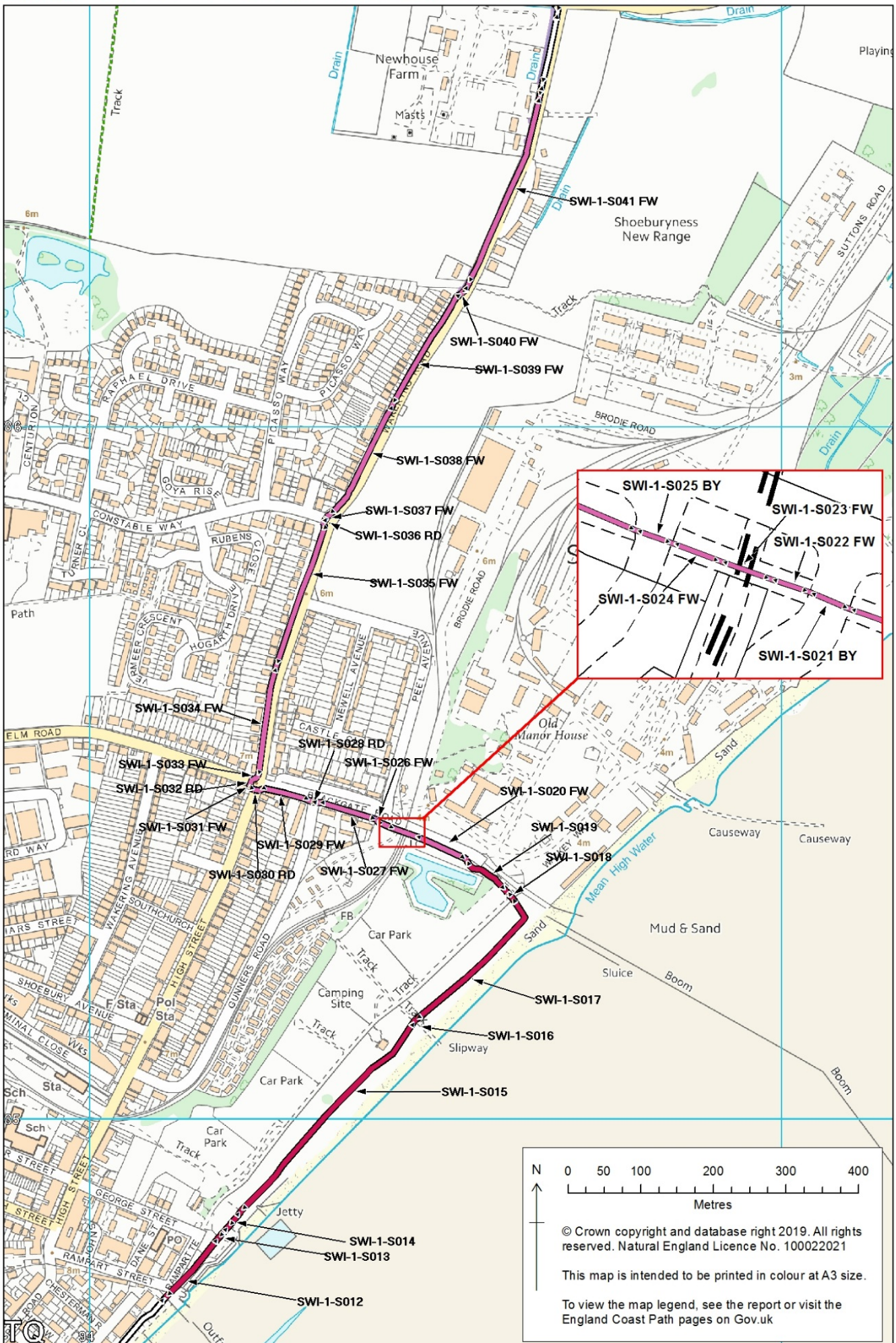


Map SWI 1a - Barge Pier, Shoebury Ness to Rampart Street, Shoeburyness

N
 0 50 100 200 300 400
 Metres
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 To view the map legend, see the report or visit the England Coast Path pages on Gov.uk

Map SWI 1b - Rampart St, Shoeburyness to Shoeburyness New Range

Map SWI 1b - Rampart St, Shoeburyness to Shoeburyness New Range



N
 0 50 100 200 300 400
 Metres

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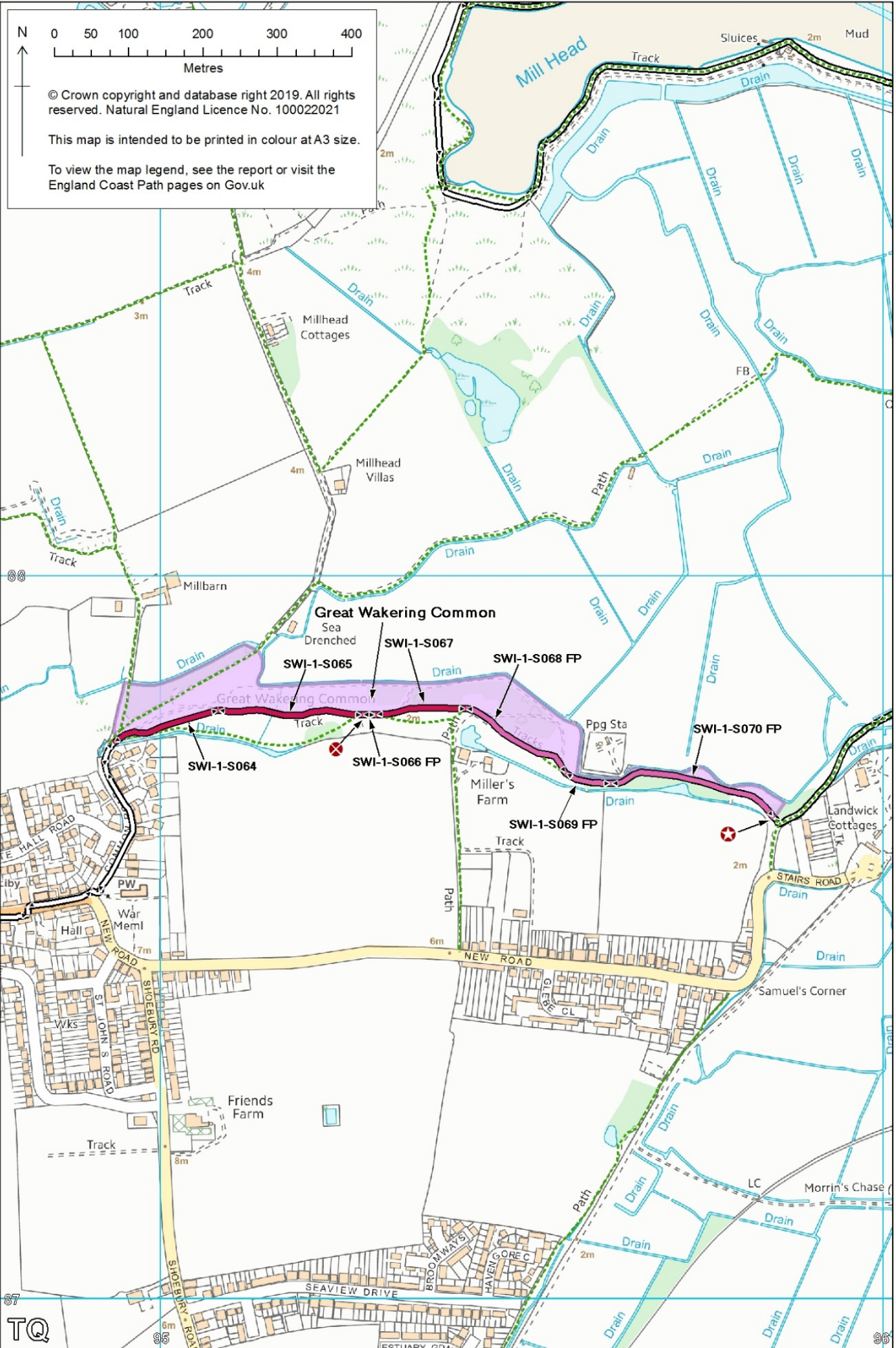
Map SWI 1d - Great Waking Common to Landwick Cottages, Great Waking

N
 0 50 100 200 300 400
 Metres

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Map SWI 1d - Great Waking Common to Landwick Cottages, Great Waking