

EXHIBIT LIST

Reference No: HOL/10024

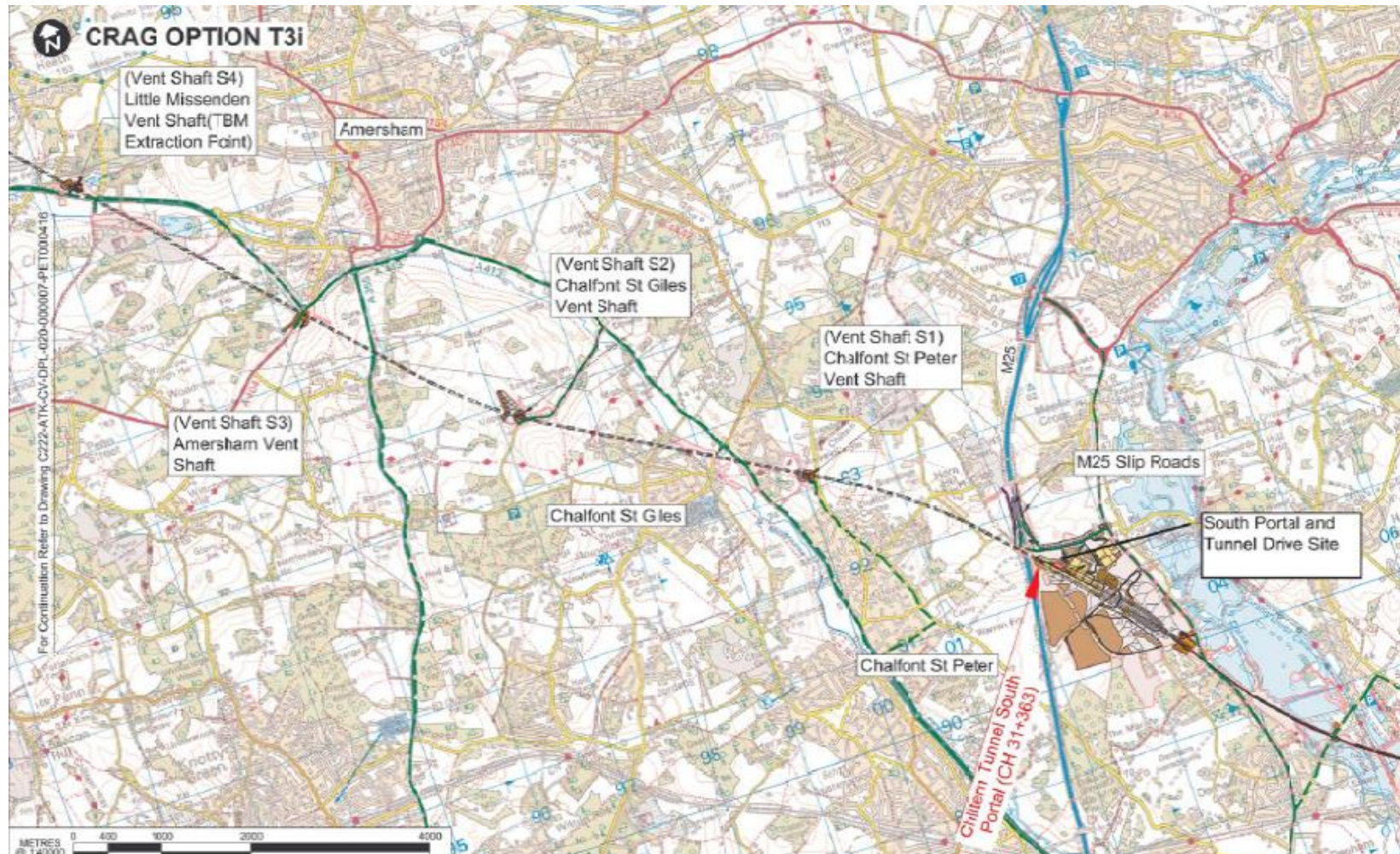
Petitioner: Buckinghamshire Standard Pack

Published to Collaboration Area: Tuesday 18-Oct-2016

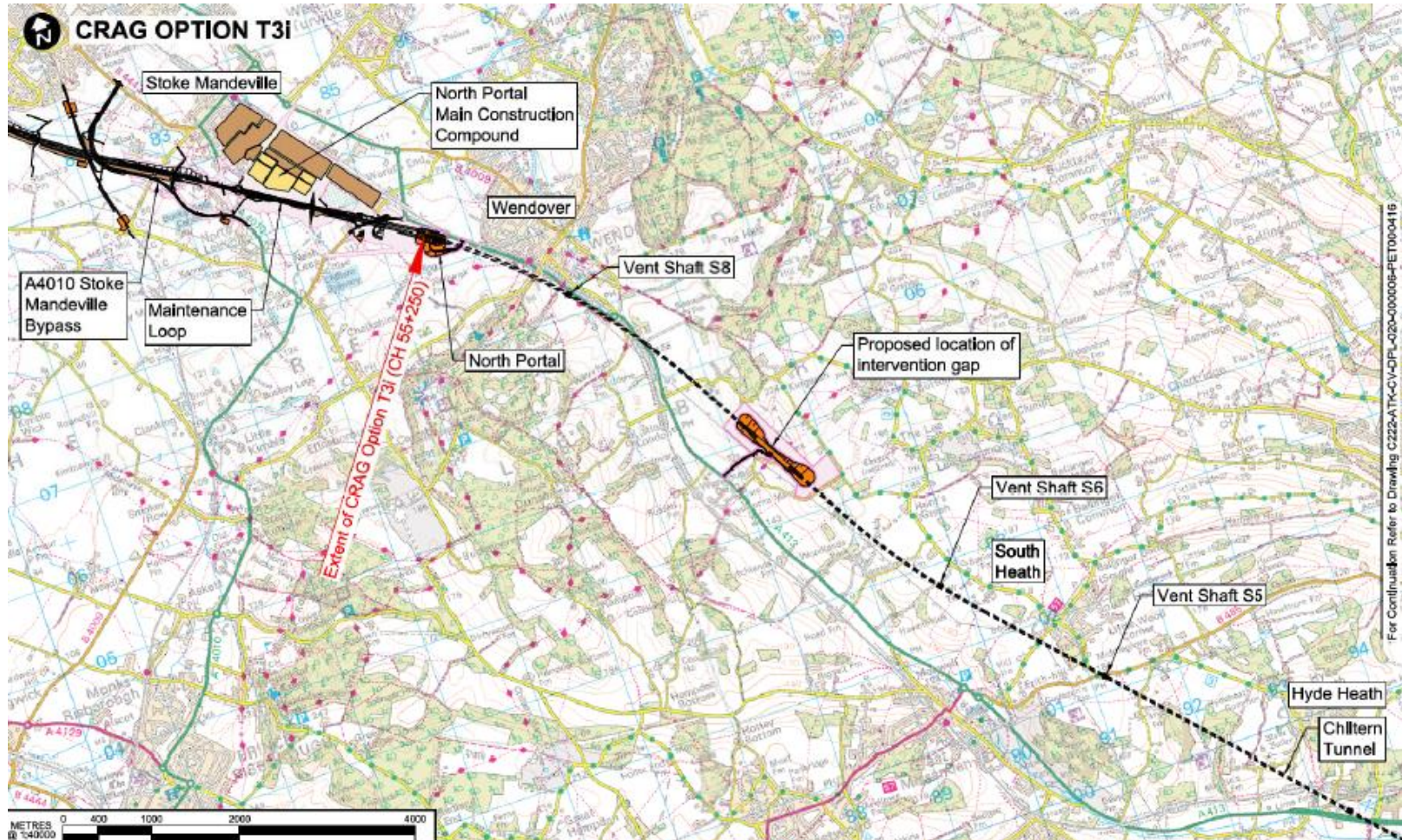
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Petitioner request - CRAG tunnel proposal (drawing 1 of 2)



Petitioner request - CRAG tunnel proposal (drawing 2 of 2)



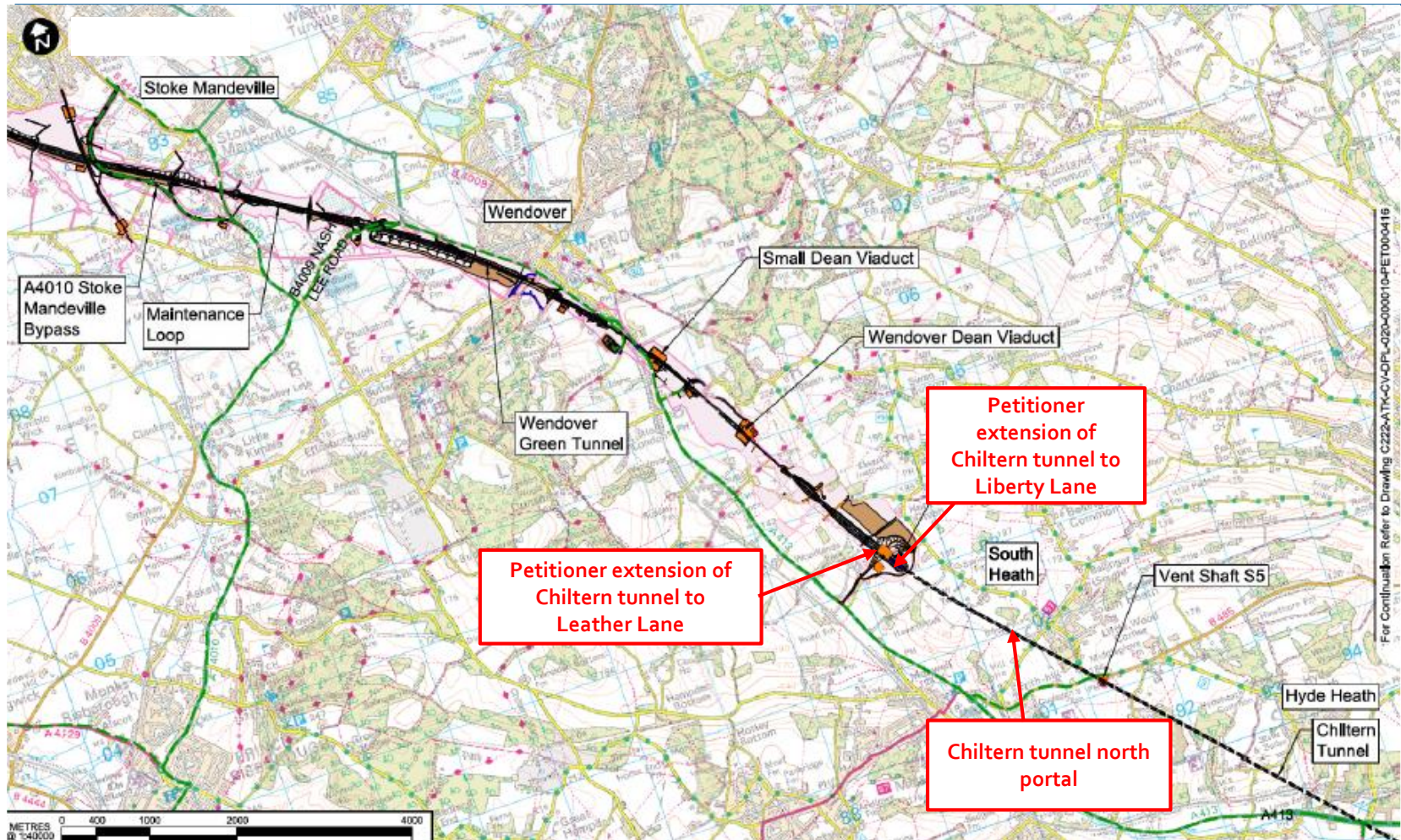
Petitioner request - CRAG tunnel proposal

To extend the Chiltern tunnel from its current Proposed Scheme portal at South Heath to north of Wendover would require:

- permanent new road accesses to and land for new ventilation shafts that would be required;
- an intervention gap on the tunnel alignment in the vicinity of Wendover Dean (consisting of a large open box structure, or retained cutting) to provide appropriate ventilation and vehicular access in accordance with design requirements for long tunnels. Additional land would be required due to the size of this structure, for associated landscape screening and for a permanent new access road for maintenance and emergency services;
- additional new permanent land take at the Wendover north portal to accommodate the wider and deeper cutting that would be required and the road access required to track level at the portal; and
- significant new temporary land take near Stoke Mandeville to provide for the large construction compound that would be required to support tunnel construction and handling of excavated material.

These works would require substantial further land to be brought within Bill limits for which an Additional Provision would be required. There would also be new or different significant environmental impacts arising from the different land take required, revised permanent works and, during construction, from revised materials movements.

Petitioner request – an extended Chiltern tunnel to Leather Lane or Liberty Lane



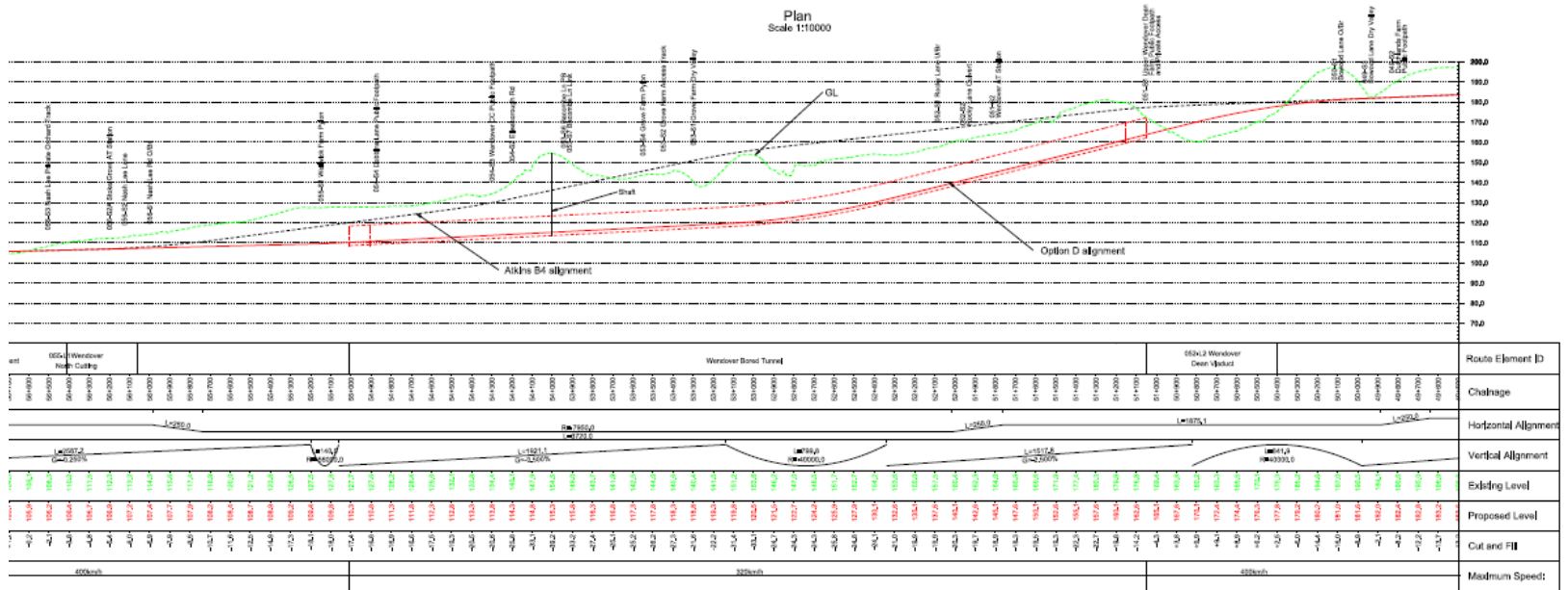
Petitioner request – an extended Chiltern tunnel to Leather Lane or Liberty Lane

Proposals for a further extension of the Proposed Scheme Chiltern tunnel, by up to 1.5 km to either Leather Lane or Liberty Lane, would require additional new land to be acquired in order to provide for:

- a new permanent access road from the A413 to the new portal location for maintenance and emergency access. This road would also be required during construction for access to the portal works and for removal of excavated material;
- appropriate land at the portal for associated portal buildings and equipment, together with vehicular access to track level; and
- temporary rail systems construction areas by the portal.

These works would require substantial further land to be brought within Bill limits for which an Additional Provision would be required. There would also be new or different significant environmental impacts arising from the different land take required, revised permanent works and, during construction, from revised materials movements.

P3747 (6)



Petitioner request – a bored or mined tunnel past Wendover

The proposal for a 3.5 - 4.0km new mined or bored tunnel past Wendover in lieu of the current Proposed Scheme green tunnel and adjacent surface works would require additional land to be acquired in order to:

- provide for a new permanent access road from the A413 up to the new southern portal of the tunnel for maintenance and emergency access. This road would also be required during construction for access to the portal works and for removal of excavated material;
- provide for additional new permanent land take at the Wendover north portal to accommodate the wider and deeper cutting that would be required and allow for permanent vehicular access to track level at the portal;
- allow for a potential ventilation shaft along the tunnel length, which would require a new access road and land for portal facilities; and
- allow for additional temporary land take near Stoke Mandeville to provide for the construction compound required for managing tunnelling works.

These works would require substantial further land to be brought within Bill limits for which an Additional Provision would be required. There would also be new or different significant environmental impacts arising from the different land take required, revised permanent works and, during construction, from revised materials movements.

Petitioner request - further extensions of the Wendover green tunnel

The proposal for further extensions of the Wendover green tunnel, both northwards towards Nash Lee Road and southwards towards the Small Dean viaduct, would require:

- additional new permanent land take at the revised Wendover north portal to allow for revised permanent vehicular access to track level at the portal and for associated portal buildings; and
- additional new permanent land take at the revised Wendover south portal to accommodate the portal buildings area and associated vehicular access.

These works would require substantial further land to be brought within Bill limits for which an Additional Provision would be required. There would also be new or different significant environmental impacts arising from the different land take required, revised permanent works and, during construction, from revised materials movements.



















CESHAM LANE

Chalfont St Peter
Vent Shaft

Chiltern Tunnel

METRES
@ 1:2500

0 20 50 100 250

Legends/Notes:	
	Satellite construction compound
	Main construction compound
	Tunnel portal
	Rail alignment formation
	Tunnels external extent
	Rail alignment
	Engineering earthworks
	Landscape earthworks
	Land potentially required during construction
	Temporary site access route / haul route
	Construction traffic route
	Existing public right of way (PRoW)
	New, diverted or realigned PRoW
	Stopped-up PRoW
	Temporary PRoW
	Temporary material stockpile



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—HOL/10024/0010



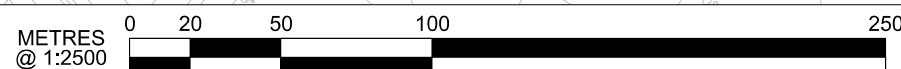
Ecological habitat creation (grassland)

Landscape earthworks
and planting (screening)


Chiltern
Tunnel


Chalfont St Peter
Vent Shaft

Landscape
planting
(screening)



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						Scale with caution as distortion can occur.

 Landscape earthworks

 Rail alignment formation



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Zone	Country South	Project/Contract			Country South Design
Design Stage	DESIGN-FOR-PETITION	Discipline/Function			Environmental
Drawing Title	Chalfont St Peter Vent Shaft	Drawn	Checked	Approved	
		TB			
		Date	Scale	Size	
		03/07/2015	AS SHOWN	A3	
		Drawing No.			Rev.
		C222-ATK-EV-DPL-020-060511-PET000416			P00.1

—HOL/10024/0011

P3748 (2)



Buckinghamshire Chiltern District

Chalfont St Giles Vent Shaft & Express Feeder Auto-transformer Station

Chalfont St Giles
Vent Shaft
Satellite Compound

Roadhead

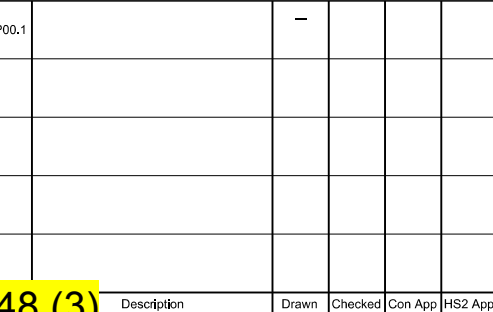
Chiltern Tunnel

Upper Bottom
House Farm

Chalfont Valley Equestrian

Hobbs Hole

Excavated volume	48,232m ³
Volume retained	40,837m ³
Percentage retained	85%
Surplus to be removed	7,395m ³
Lorry loads	870













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Legends/Notes

-  Satellite construction compound
-  Main construction compound
-  Tunnel portal
-  Rail alignment formation
-  Tunnels external extent
-  Rail alignment
-  Engineering earthworks
-  Landscape earthworks
-  Land potentially required during construction
-  UT - Main utility works

-  Temporary site access route / haul route
-  Construction traffic route
-  Existing public right of way (PRoW)
-  New, diverted or realigned PRoW
-  Stopped-up PRoW
-  Temporary PRoW
-  Temporary material stockpile



Registered in England
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Creator/Originator

Atkins

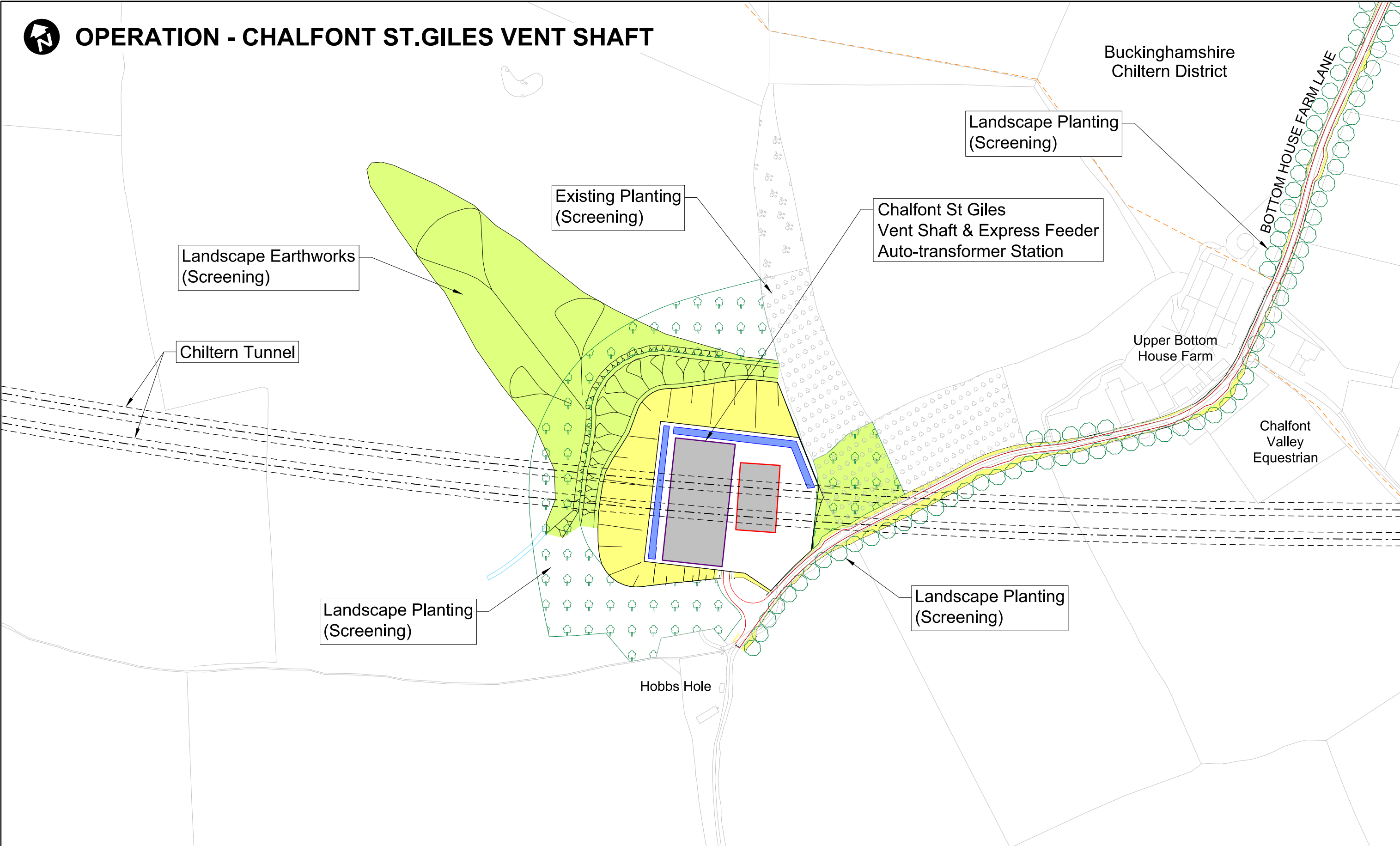
Zone	Country South	Project/Contract			Country South Design	
Design Stage	DESIGN-FOR-PETITION	Discipline/Function				Environmental
Drawing Title	Construction Phase Chalfont St.Giles Vent Shaft	Drawn	Checked	Approved		
		Date	Scale	Size		
		26/08/2015	AS SHOWN	A1		
		Drawing No.			Rev.	
		C222-ATK-EV-DPL-020-059804-PET000000			P00.1	

HOL/10024/0012

P3748 (3)



OPERATION - CHALFONT ST.GILES VENT SHAFT



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CONSTRUCTION - AMERSHAM VENT SHAFT

Buckinghamshire
Chiltern District

A473

A473

Amersham
Hospital

Amersham
Vent Shaft

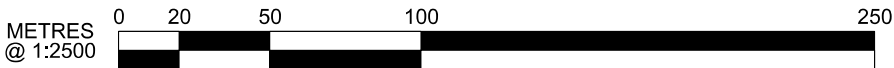
Chiltern
Tunnel

Roadhead

Amersham Vent Shaft
Satellite Compound

A404 WHIELDEN LANE

Excavated volume	45,204m ³
Volume retained	3m ³
Percentage retained	0%
Surplus to be removed	45,201m ³
Lorry loads	5,318



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Legends/Notes:	
Satellite construction compound	Temporary site access route / haul route
Main construction compound	Construction traffic route
Tunnel portal	Existing public right of way (PRoW)
Rail alignment formation	New, diverted or realigned PRoW
Tunnels external extent	Stopped-up PRoW
Rail alignment	Temporary PRoW
Engineering earthworks	Temporary material stockpile
Landscape earthworks	
Land potentially required during construction	

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Creator/Originator
Atkins

Zone	Country South		Project/Contract		Country South Design	
Design Stage	DESIGN-FOR-PETITION		Discipline/Function		Environmental	
Drawing Title	Construction Phase Amersham Vent Shaft		Drawn	Checked	Approved	
			TB			
			Date	Scale	Size	
			03/07/2015	AS SHOWN	A3	
Drawing No.			C222-ATK-EV-DPL-020-050513-PET000416			Rev.
						P00.0



OPERATION - AMERSHAM VENT SHAFT

Buckinghamshire
Chiltern District

A413

Amersham
Hospital

A413

Amersham
Vent Shaft

Chiltern
Tunnel

Landscape planting
(screening)

Landscape planting
(screening)

A404 WHILDEN LANE

METRES
@ 1:2500



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Legends/Notes:

- Depot, station, headhouse or portal building
- Tunnel portal
- Balancing pond
- Land drainage area
- Replacement floodplain storage
- Landscape mitigation planting (scrub / woodland)
- Grassed areas
- Engineering earthworks
- Landscape earthworks
- Rail alignment formation

- Grassland habitat creation
- Tunnels external extent
- Rail alignment
- Ditches - new
- Hedgerow habitat creation
- HS2 Access road
- Existing public right of way (PRoW)
- New, diverted or realigned PRoW
- Stopped-up PRoW



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Zone	Country South	Project/Contract			Country South Design	
Design Stage	DESIGN-FOR-PETITION	Discipline/Function				Environmental
Drawing Title	Amersham Vent Shaft	Drawn	Checked	Approved		
		TB				
		Date	Scale	Size		
		03/07/2015	AS SHOWN	A3		
		Drawing No.			Rev.	
		C222-ATK-EV-DPL-020-060513-PET000416			P00.0	
		UNCL 1400041/020				



CONSTRUCTION - LITTLE MISSENDEN VENT SHAFT

Buckinghamshire
Chiltern District

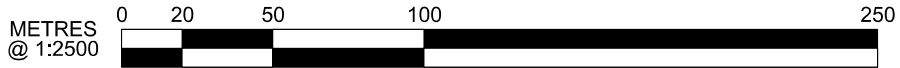
Little Missenden
Vent Shaft and
Auto-transformer
Station

Roadhead

Chiltern
Tunnel

Little Missenden
Vent Shaft
Satellite Compound

Excavated volume	57,246m ³
Volume retained	6,987m ³
Percentage retained	12%
Surplus to be removed	50,259m ³
Lorry loads	5,913



- Legends/Notes:
- Satellite construction compound
 - Main construction compound
 - Tunnel portal
 - Rail alignment formation
 - Tunnels external extent
 - Rail alignment
 - Engineering earthworks
 - Landscape earthworks
 - Land potentially required during construction
 - Temporary site access route / haul route
 - Construction traffic route
 - Existing public right of way (PRoW)
 - New, diverted or realigned PRoW
 - Stopped-up PRoW
 - Temporary PRoW
 - Temporary material stockpile

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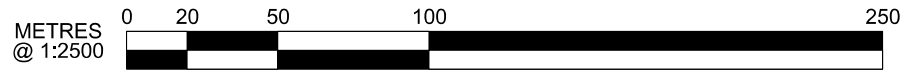
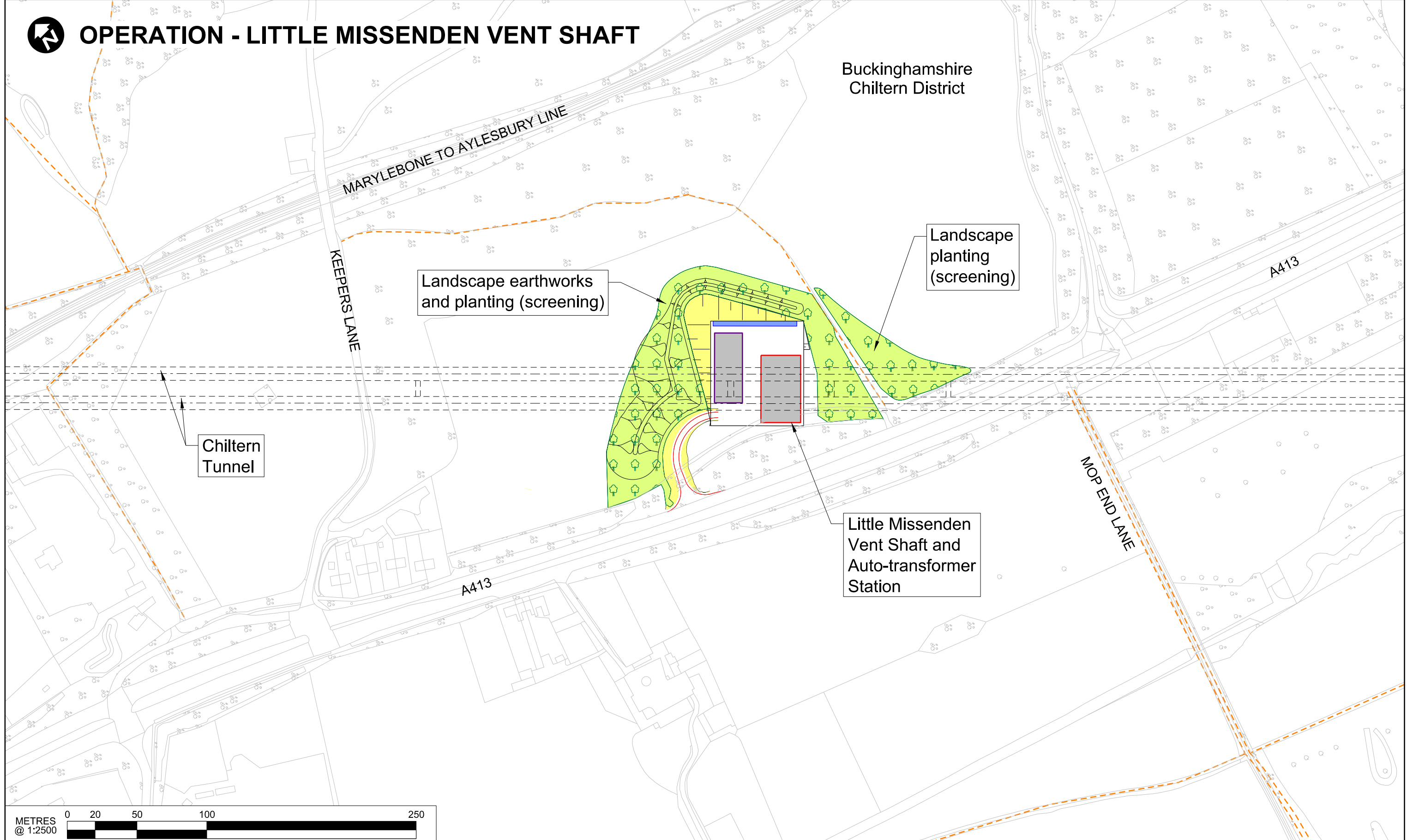
Creator/Originator
Atkins

Zone	Country South	Project/Contract			Country South Design	
Design Stage	DESIGN-FOR-PETITION	Discipline/Function				Environmental
Drawing Title	Construction Phase Little Missenden Vent Shaft	Drawn	Checked	Approved		
		Date	Scale	Size		
		03/07/2015	AS SHOWN	A3		
		Drawing No.			Rev.	
		C222-ATK-EV-DPL-020-050514-PET000416			P00.1	



OPERATION - LITTLE MISSENDEN VENT SHAFT

Buckinghamshire
Chiltern District



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Legends/Notes:

- Depot, station, headhouse or portal building
- Tunnel portal
- Balancing pond
- Land drainage area
- Replacement floodplain storage
- Landscape mitigation planting (scrub / woodland)
- Grassed areas
- Engineering earthworks
- Landscape earthworks
- Rail alignment formation
- Grassland habitat creation
- Tunnels external extent
- Rail alignment
- Ditches - new
- Hedgerow habitat creation
- HS2 Access road
- Existing public right of way (PRoW)
- New, diverted or realigned PRoW
- Stopped-up PRoW



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Registration No. 06791686
Registered office:
One Canada Square,
London, E14 5AB

Creator/Originator
Atkins

Zone	Country South			Project/Contract		
Design Stage	Country South Design			Country South Design		
Drawing Title	DESIGN-FOR-PETITION			Discipline/Function		
	Environmental			Drawn	Checked	Approved
	Little Missenden Vent Shaft			TB		
	Date	03/07/2015	Scale	AS SHOWN	Size	A3
Drawing No.					Rev.	
C222-ATK-EV-DPL-020-060514-PET000416					P00.1	

CONSTRUCTION - CHESHAM ROAD VENT SHAFT

Chesham Road Vent Shaft and Auto-transformer Station

Buckinghamshire
Chiltern District

Sheepcotts
Cottage

Chiltern Tunnel

Chesham Road
Vent Shaft
Satellite Compound

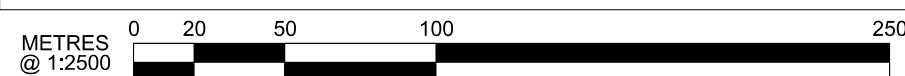
Cudsdens Court

Roadhead

Hyde Farm







Chapel Farm

Excavated volume	56,110m ³
Volume retained	2,356m ³
Percentage retained	4%
Surplus to be removed	53,754m ³
Lorry loads	6,324



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during construction

-  Temporary site access route / haul route
-  Construction traffic route
-  Existing public right of way (PRoW)
- New, diverted or realigned PRoW
-  Stopped-up PRoW
-  Temporary PRoW
-  Temporary material stockpile



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Registration No. 06791686
Registered office:
One Canada Square,
London, E14 5AB

Creator/Originator	
--------------------	--

Atkins

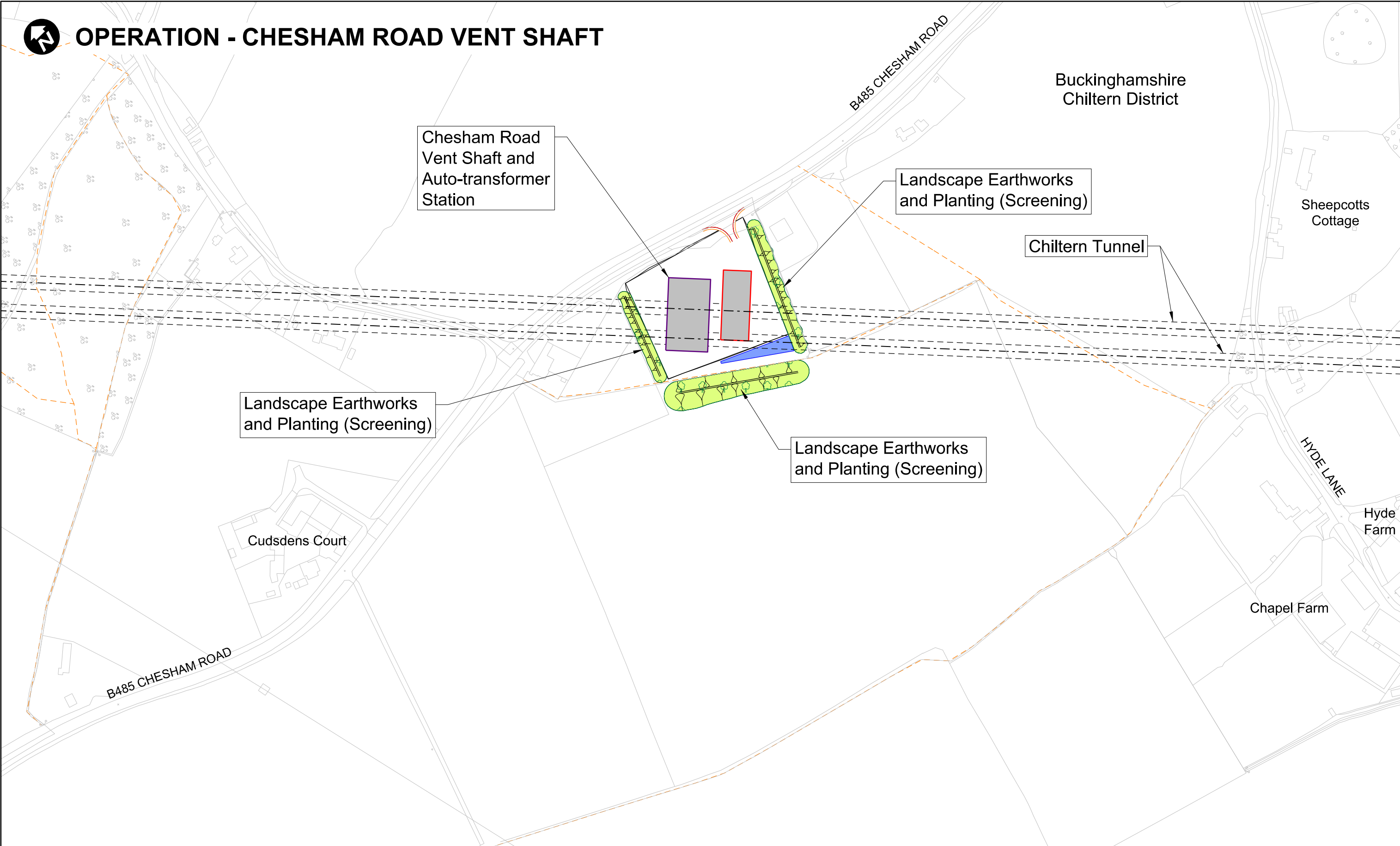
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Design Stage	DESIGN-FOR-PETITION	Discipline/Function			Environmental
Drawing Title	Construction Phase Chesham Road Vent Shaft	Drawn	Checked	Approved	
		Date	Scale	Size	
		26/08/2015	AS SHOWN	A1	
		Drawing No.			Rev.
		C222-ATK-EV-DPL-020-059903-PET000000			P00.1

—HOL/10024/0018

P3748 (9)



OPERATION - CHESHAM ROAD VENT SHAFT



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				Scale with caution as distortion can occur.	
				Legends/Notes: <div><div>Depot, station, headhouse or portal building</div><div>Tunnel portal</div><div>Balancing pond</div><div>Land drainage area</div><div>Replacement floodplain storage</div><div>Landscape mitigation planting (scrub / woodland)</div><div>Grassed areas</div><div>Engineering earthworks</div><div>Landscape earthworks</div><div>Rail alignment formation</div></div>	
				<div><div>Grassland habitat creation</div><div>Tunnels external extent</div><div>Rail alignment</div><div>Noise fence barrier</div><div>Ditches - new</div><div>Hedgerow habitat creation</div><div>HS2 Access road</div><div>Existing public right of way (PROW)</div><div>New, diverted or realigned PROW</div><div>Stopped-up PROW</div></div>	
				<div><div></div><div>Registered in England Registration No. 06791686 Registered office: One Canada Square, London, E14 5AB</div></div> <div>Creator/Originator Atkins</div>	
		Zone Country South		Project/Contract Country South Design	
		Design Stage DESIGN-FOR-PETITION		Discipline/Function Environmental	
		Drawing Title Chesham Road Vent Shaft		Drawn IE	Checked Approved
				Date 26/08/2015	Scale AS SHOWN
				Size A1	Rev. P00.1
				Drawing No. C222-ATK-EV-DPL-020-069903-PET000000	
				Rev. P00.1	

Viaducts – Proposals in the Buckinghamshire area

Sheet 1 of 3

	Viaducts	Location	Design Objective (Why are viaducts necessary?)	Mitigation proposed
1	Wendover Dean Viaduct	Midway between South Heath and Wendover	Viaduct required to cross a natural dry valley, route is in cutting either side of the valley. Alternative of an embankment across the valley discounted on landscape grounds and wide landtake required.	Proposed Scheme vertical alignment requires to balance adjacent cut depths with height of viaduct across the dry valley. Viaduct height varies up to 18m (ground to rail height). Viaduct will be visible from the A413 and from surrounding footpaths and properties/settlements on higher ground. Viaduct is designated a Key Design Element due to visibility on the AONB and will require high quality design and finishes.
2	Small Dean Viaduct	South of Wendover, near Small Dean Lane	Viaduct required to cross over the A413 and adjacent Chiltern Line railway.	Viaduct height will be based on providing appropriate highway clearance to A413 (as railway is in a deeper cutting). Viaduct to have single span across the A413 to minimise impact on road use. Reinstatement of vegetation alongside the road and railway following construction to support integration of structure into road and rail viewpoints

Viaducts – Proposals in the Buckinghamshire area

Sheet 2 of 3

	Viaducts	Location	Design Objective (Why are viaducts necessary?)	Mitigation proposed
3	Thame Valley Viaduct	North west of Aylesbury, south of Waddesdon	Viaduct crosses the River Thame valley and flood plain.	Design will ensure piers avoid main river channel. Viaduct pier spacing and design will aim to support visual appearance of the structure given its low height
4	Twyford Viaduct	East of Twyford	This viaduct carries a length of the Proposed Scheme on embankment over the Padbury Brook and it's floodplain, providing appropriate flood clearance to the watercourse.	The viaduct is approx. 50m long, the design will ensure piers avoid main river channel. A 4m high noise fence barrier or equivalent will be located on the eastern side of Twyford Viaduct
5	Godington East and West Viaducts	Between Twyford and Chetwode	These adjacent viaducts, 80/100m long, carry the Proposed Scheme, which is on embankment in this area, over the Padbury Brook and its flood plain, providing appropriate flood clearance to the watercourse.	Design will ensure piers avoid main river channel. Viaduct pier spacing and design will aim to support visual appearance of the structure given its low height A 1.4m noise absorptive parapet or equivalent will be located on the west side of Godington East and West Viaducts

Viaducts – Proposals in the Buckinghamshire area

Sheet 3 of 3

	Viaducts	Location	Design Objective (Why are viaducts necessary?)	Mitigation proposed
6	Westbury Viaduct	South of Westbury	This 300m long viaduct crosses the River Great Ouse and its adjacent flood plain	A 1.4m noise adsorptive parapet or equivalent will be located on the west side of Westbury Viaduct
7	Turweston Viaduct	Adjacent to Turweston	This 90m long viaduct crosses the River Great Ouse.	A 4m high noise fence barrier or equivalent will be located on the eastern side of the Turweston Viaduct

Viaducts – Design Policy

HS2 Design Vision & Independent Design Panel

The design of HS2 will be developed in line with the HS2 Design Vision.

In addition, the Secretary of State has established an independent Design Panel, so as to ensure that designs of major stations and structures and other related design aspects of the new railway will complement local aspirations and contribute to the natural and built environment. Sadie Morgan has been appointed Chair of the HS2 Design Panel. A pool of panel members covering all design disciplines has been appointed to independently assist the design challenge.

The Design Panel will assist HS2 Ltd through advice, and HS2 Ltd will work in partnership with a range of organisations, including planning authorities. The aim will be to deliver a high standard of design that is also cost-effective and sustainable.



Generic illustration of a high viaduct

*Source: **HS2 Information Paper D1: Design Policy***

Viaducts –Design Policy

Design Development & Public Engagement

The Promoter will engage the public on the design development of Key Design Elements - including main viaducts, depot buildings and key ventilation shafts in sensitive areas.

Viaducts within Buckinghamshire which are classified as Key Design Elements are:

- Colne Valley Viaduct;
- Small Dean Viaduct;
- Wendover Dean Viaduct; and
- Thame Valley Viaduct.

Stoke Mandeville Maintenance Loop

Location Selection

Topic	
Purpose	<p>Maintenance loops are required to enable maintenance trains to reach work sites on the track quickly in the limited night closure period. The Stoke Mandeville Maintenance Loop is to be located at Stoke Mandeville, between London and Calvert.</p> <p>The principal functions of the loops are to:</p> <ul style="list-style-type: none"> • Allow maintenance trains to be kept securely during the day, in readiness for maintenance work during the night • Have a safe stopping location for any passenger train that develops a fault • Have no maintenance functions taking place at the loop
Location/ Layout	<ul style="list-style-type: none"> • Located between Nash Lee Road and the A4010 Risborough Road, approximately mid-way between London and the Infrastructure Maintenance Depot at Calvert. • The loop needs to be approximately 1.25km in length to allow passenger trains to be parked clear of the main line if necessary. • Access tracks are located either side of the maintenance loop with access/egress from Nash Lee Lane on the east side and B4009 Nash Lee Road on the west side of the proposed scheme.
Scheme development	<ul style="list-style-type: none"> • Enhanced screen planting included to screen views of the Maintenance Loop from Coombe Hill • Assurance provided to Stoke Mandeville Parish Council to increase the height of the existing noise fence barriers at the Maintenance Loop height from 3m to 4m, or equivalent.

Stoke Mandeville Maintenance Loop

Location Selection

	Option located at Stoke Mandeville (Proposed Scheme)
Details	<ul style="list-style-type: none"> • Located between Nash Lee Road and the A4010 Risborough Road. • Access tracks either side of the maintenance loop with access/egress from Nash Lee Lane on the east side and B4009 Nash Lee Road on the west side of the Proposed Scheme. • Landscape mitigation and hedgerow planting on either of the maintenance loop to help screen view of the railway and maintenance loop. The inclusion in AP4 of additional areas of woodland planting on both sides to partially screen the sidings from the Chilterns escarpment.
Benefits	<ul style="list-style-type: none"> • Avoids visual impacts on the landscape qualities of AONB and loss of ancient woodland • Limits the maintenance trains' movements during the night closure period, hence reducing noise and visual effects.
Mitigation	<ul style="list-style-type: none"> • Noise fencing is provided either side of the maintenance loops to mitigate noise effects from passenger and maintenance trains. • No adverse noise effects have been assessed in the vicinity of the loop. • Landscape mitigation and hedgerow planting provided on either of the maintenance loops to help screen view of the railway and maintenance loop. • Additional Provision 4 introduced additional areas of woodland planting on both sides to partially screen the sidings from the Chilterns escarpment.
Lighting	<ul style="list-style-type: none"> • Consideration will be given to reducing light pollution resulting from working at night • Low level lighting will be provided to allow safe access for train drivers to the facility • The height of external lighting installations to be designed as low as possible • Using automatic lighting control systems with photocells and time clocks to control their operation • Using LED or low energy lamps

Buckinghamshire
Aylesbury Vale District

MARYLEBONE TO AYLESBURY LINE

Risborough Road
Underpass

Maintenance Loop

Footpath ELL/20
Overbridge

B4009 Nash Lee Road
Overbridge

Stoke Grove
Auto-transformer
Station

Former site of Church
of St. Mary's, Stoke
Mandeville

B4009 Nash Lee Road
Overbridge
Satellite Compound

Risborough Road
Satellite Compound

Roadhead



P00.1	FIRST DRAWN								


















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- Legends/Notes:
-  Satellite construction compound
 -  Main construction compound
 -  Tunnel portal
 -  Rail alignment formation
 -  Tunnels external extent
 -  Rail alignment
 -  Engineering earthworks
 -  Landscape earthworks
 -  Land potentially required during construction
 -  Temporary site access route / haul route
 -  Construction traffic route
 -  Existing public right of way (PRoW)
 -  New, diverted or realigned PRoW
 -  Stopped-up PRoW
 -  Temporary PRoW
 -  Temporary material stockpile
 -  Location of Parish boundary



Registered in England
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Registered office:
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Creator/Originator

Atkins

Zone	Country South	Project/Contract			Country South Design		
Design Stage	DESIGN-FOR-PETITION		Discipline/Function				Environmental
Drawing Title	Stoke Mandeville Maintenance Loop		Drawn	Checked	Approved		
			Date	Scale	Size		
			21/04/2016	AS SHOWN	A1		
Construction Phase			Drawing No.				Rev.
			C222-ATK-EV-DPL-020-050514-PET000000				P00.1



MARYLEBONE TO AYLESBURY LINE

B4009 Nash Lee Road
Overbridge

Risborough
Road
Underpass

Maintenance Loop

Footpath ELL/20
Overbridge

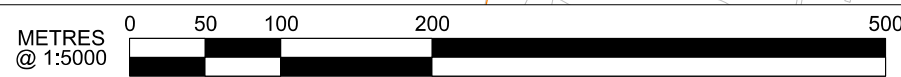
Stoke Grove
Auto-transformer
Station

Former site of Church
of St. Mary's, Stoke
Mandeville










A4010 RISBOROUGH ROAD








0 500


B4009 NASH LEE ROAD



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 Satellite construction compound
 Main construction compound
 Tunnel portal
 Rail alignment formation
 --- --- Tunnels external extent
 --- --- Rail alignment
 Engineering earthworks
 Landscape earthworks
 Land potentially required during construction

 Temporary site access route / haul route
 Construction traffic route
 Existing public right of way (PRoW)
 New, diverted or realigned PRoW
 Stopped-up PRoW
 Temporary PRoW
 Temporary material stockpile

 Location of Parish boundary



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Creator/Originator	
--------------------	--

Atkins

Zone	Country South	Project/Contract			Country South Design		
Design Stage	DESIGN-FOR-PETITION	Discipline/Function			Environmental		
Drawing Title	Stoke Mandeville Maintenance Loop Proposed Scheme	Drawn	Checked	Approved			
		IE					
		Date	Scale	Size			
		21/04/2016	AS SHOWN	A3			
		Drawing No.			Rev.		
		C222-ATK-EV-DPL-020-060514-PET000000			P00.1		

-HOL/10024/0028

P3750 (4)

Construction Compounds - Types

Main construction compounds for civil and rail systems works

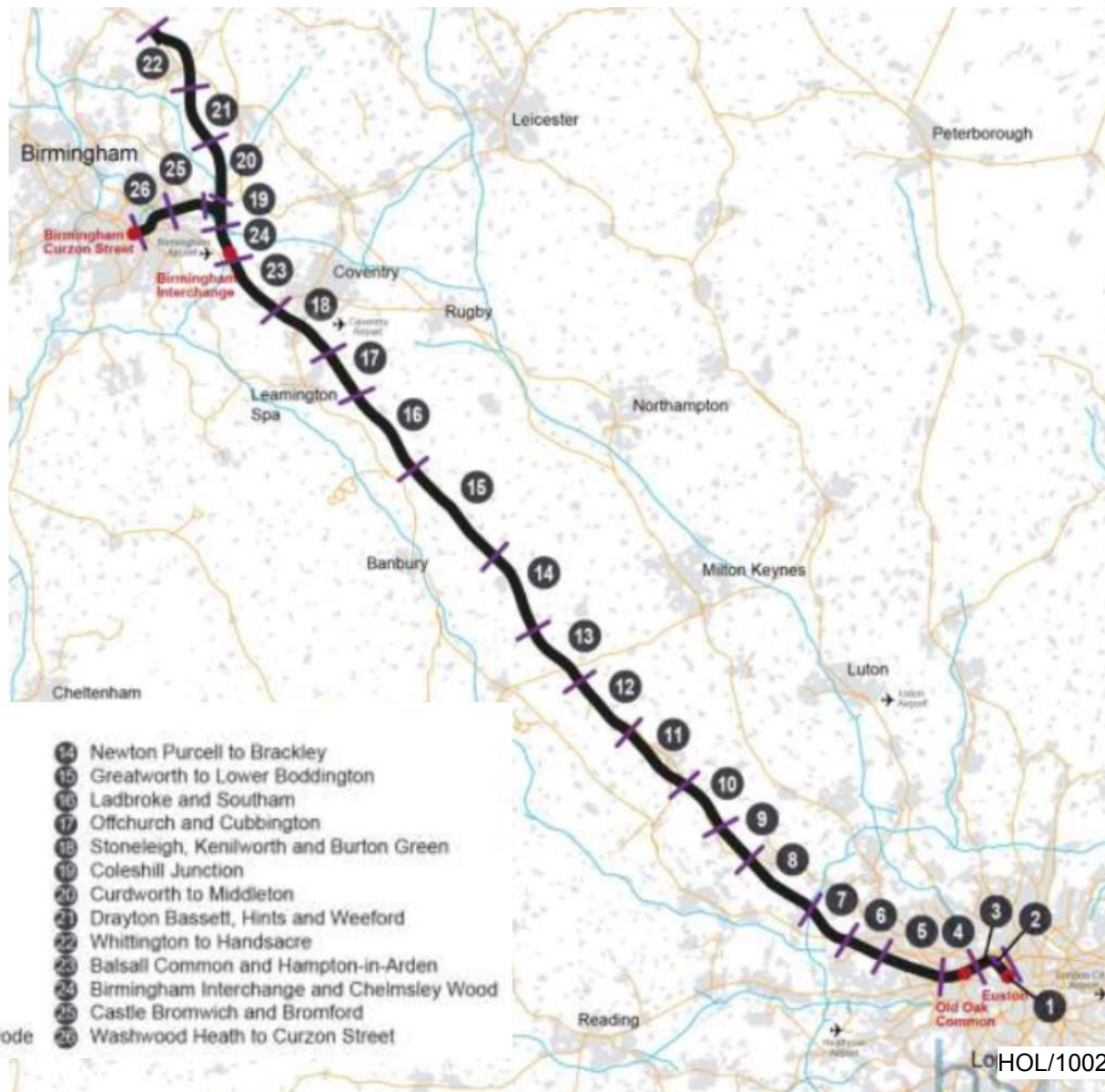
- Will act as strategic hubs for core project management activities (i.e. engineering, planning and construction delivery) and for office based construction personnel.
- Will include offices, storage for materials (such as aggregates, structural steel, steel reinforcement) and laydown areas, maintenance and parking facilities (for site plant, lorries and staff cars), together with the main welfare facilities for construction personnel.
- Workers' sleeping/residential accommodation may be provided at some of these construction compounds, subject to approval under schedule 17 (Conditions of deemed planning permission) of the Bill.
- Will typically require approximately 3ha of land and support up to 260 construction personnel.

Satellite construction compounds for civil and rail systems works

- Will generally be smaller, providing office accommodation for a limited number of construction personnel associated with specific construction activities.
- Will typically require approximately 0.75ha of land, supporting up to 100 construction personnel.
- Will include local storage for plant and materials, welfare facilities and limited car parking.
- Will not include provision for sleeping/residential use.

Ref. HS2 Information Paper D2: Selection of the location of construction compounds

Community Forum Area (CFA) along HS2 Phase 1 route

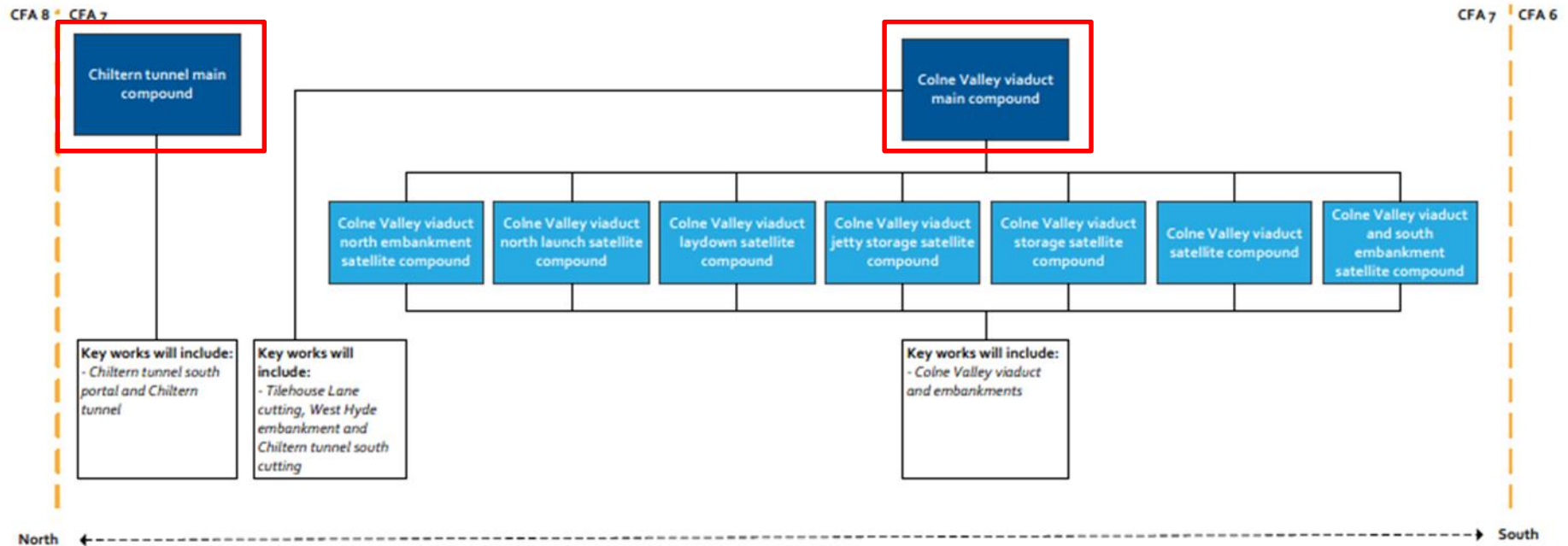


Community forum areas

- 1 Euston - Station and Approach
- 2 Camden Town and HS1 Link
- 3 Primrose Hill to Kilburn (Camden)
- 4 Kilburn (Brent) to Old Oak Common
- 5 Northolt Corridor
- 6 South Ruislip to Ickenham
- 7 Colne Valley
- 8 The Chalfonts and Amersham
- 9 Central Chilterns
- 10 Dunsmore, Wendover and Halton
- 11 Stoke Mandeville and Aylesbury
- 12 Waddesdon and Quainton
- 13 Bert, Steeple Claydon, Twyford and Chetwode
- 14 Newton Purcell to Brackley
- 15 Greatworth to Lower Boddington
- 16 Ladbrooke and Southam
- 17 Offchurch and Cubbington
- 18 Stoneleigh, Kenilworth and Burton Green
- 19 Coleshill Junction
- 20 Curdworth to Middleton
- 21 Drayton Bassett, Hints and Weeford
- 22 Whittington to Handsacre
- 23 Balsall Common and Hampton-in-Arden
- 24 Birmingham Interchange and Chelmsley Wood
- 25 Castle Bromwich and Bromford
- 26 Washwood Heath to Curzon Street

Buckinghamshire Construction Compounds

CFA 7 – Civils works compounds

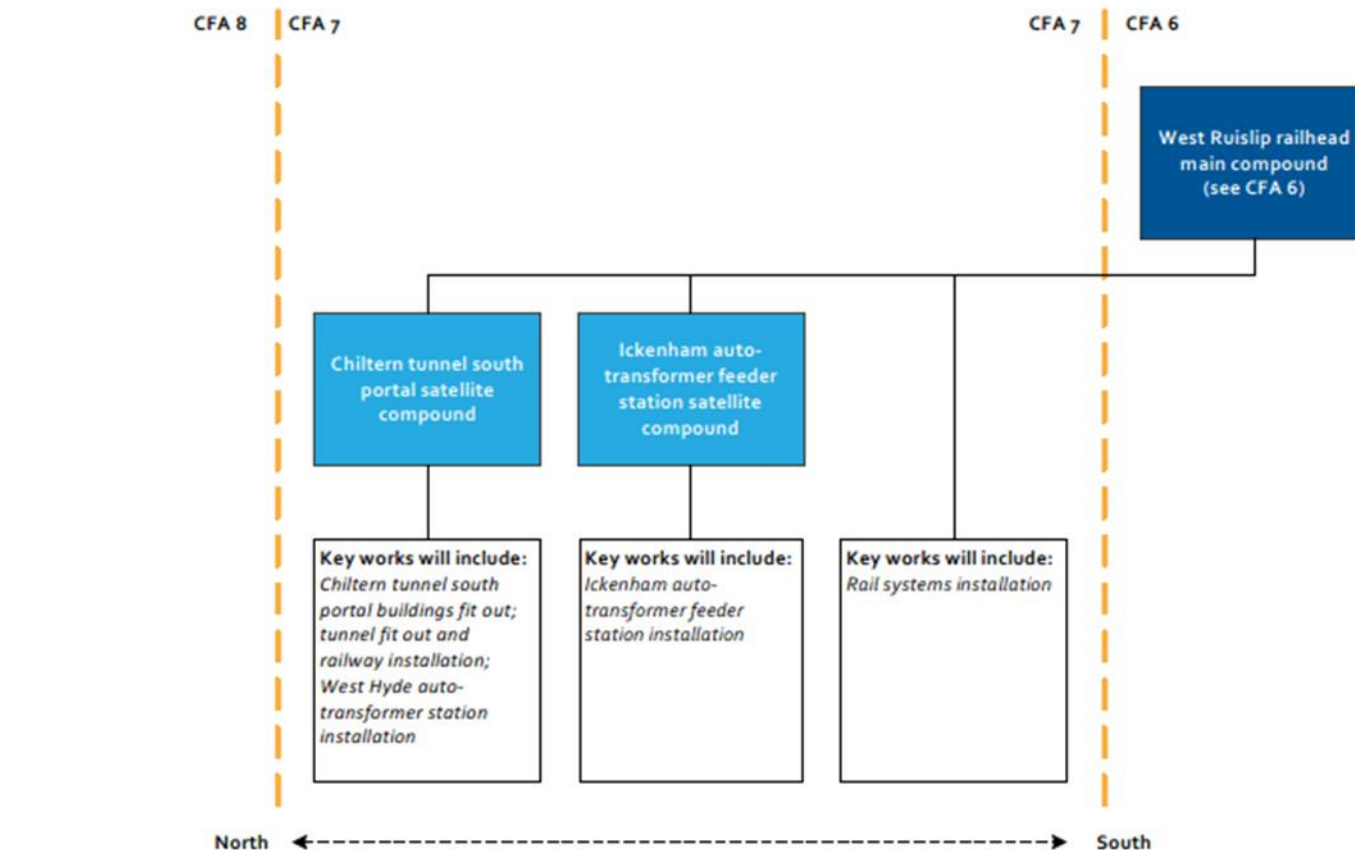


Key




- Main construction compounds
- Satellite construction compounds
- Construction compound(s) with space for workers' accommodation provided

Buckinghamshire Construction Compounds

CFA 7 – Rail systems compounds

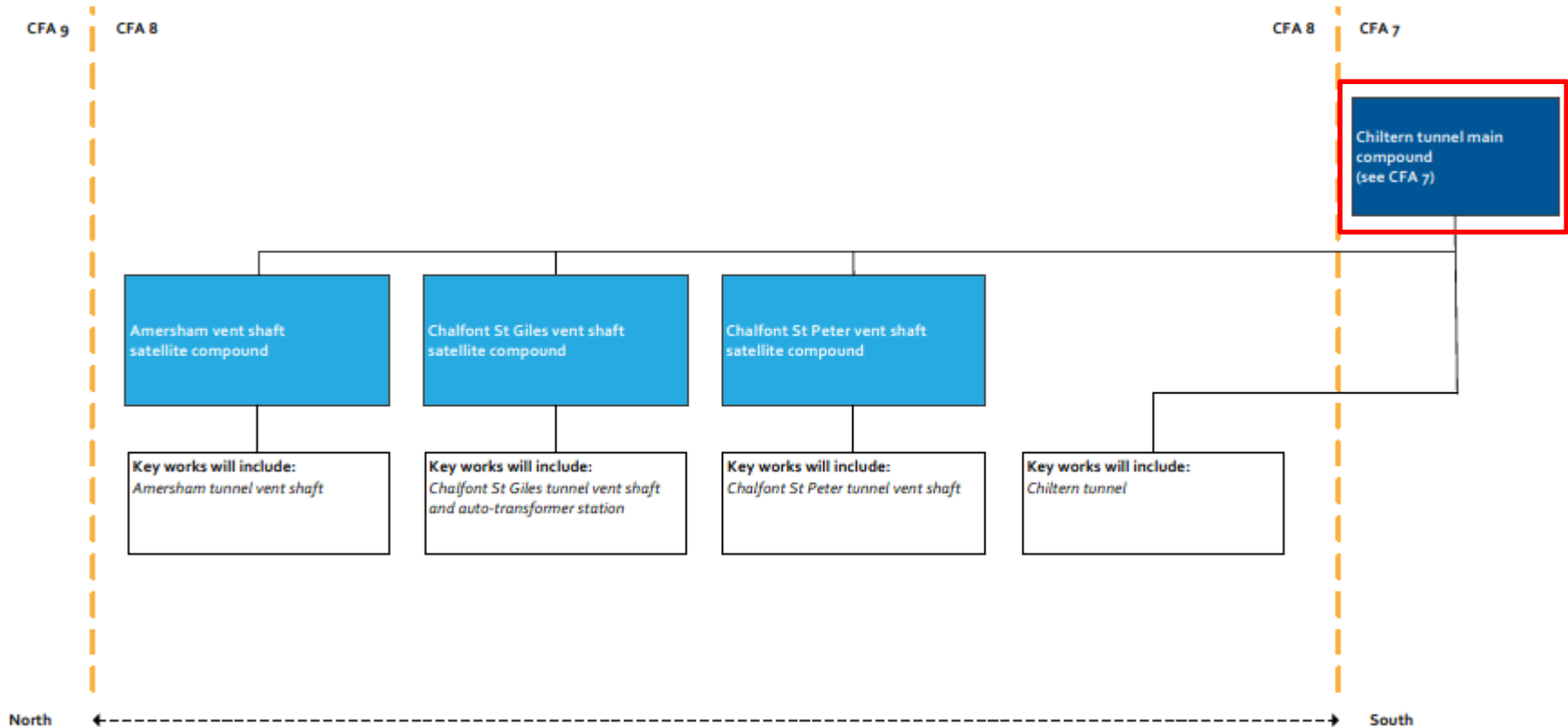


Key




-  Main construction compounds
-  Satellite construction compounds
-  Construction compound(s) with space for workers' accommodation provided

Buckinghamshire Construction Compounds

CFA 8 – Civils works compounds

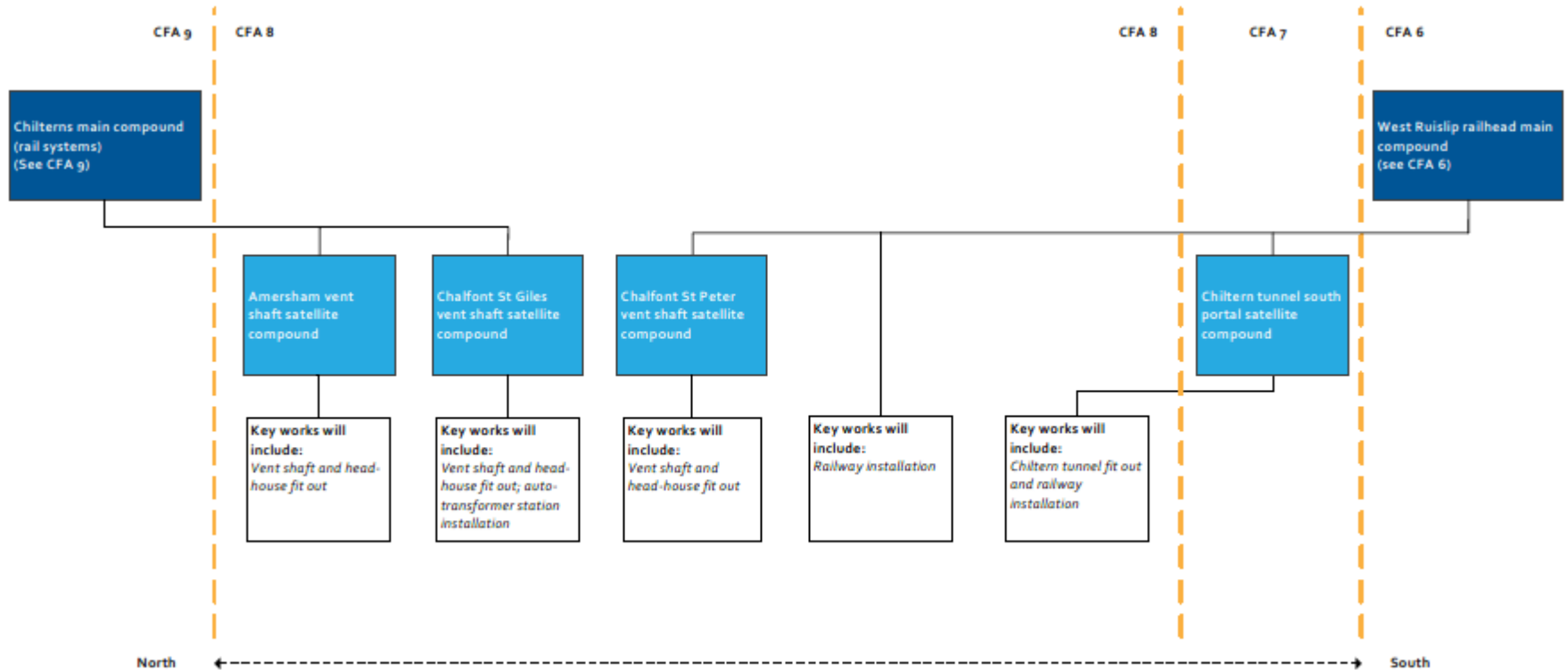


Key

-  Main construction compounds
-  Satellite construction compounds
-  Construction compound(s) with space for workers' accommodation provided

Buckinghamshire Construction Compounds

CFA 8 – Rail systems compounds

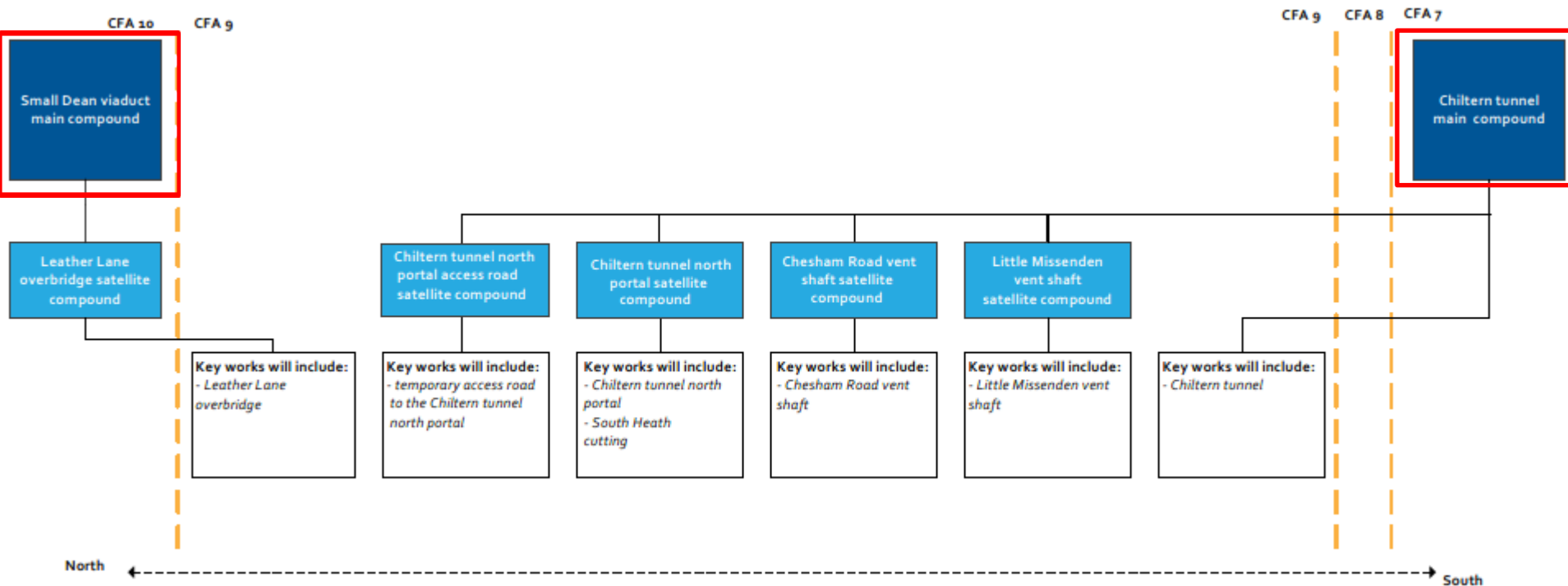


Key




- Main construction compounds
- Satellite construction compounds
- Construction compound(s) with space for workers' accommodation provided

Buckinghamshire Construction Compounds

CFA 9 – Civils works compounds

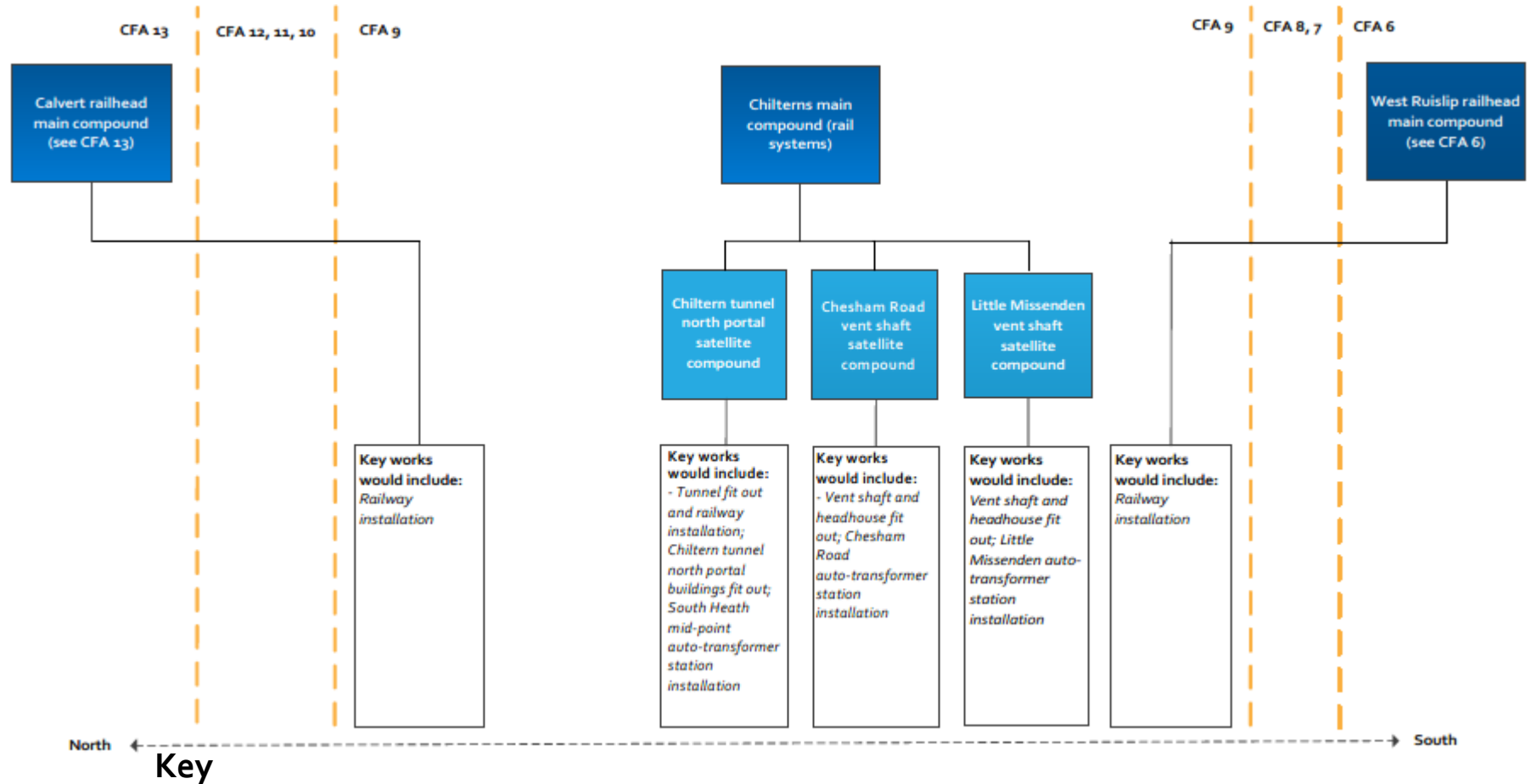


Key

-  Main construction compounds
-  Satellite construction compounds
-  Construction compound(s) with space for workers' accommodation provided

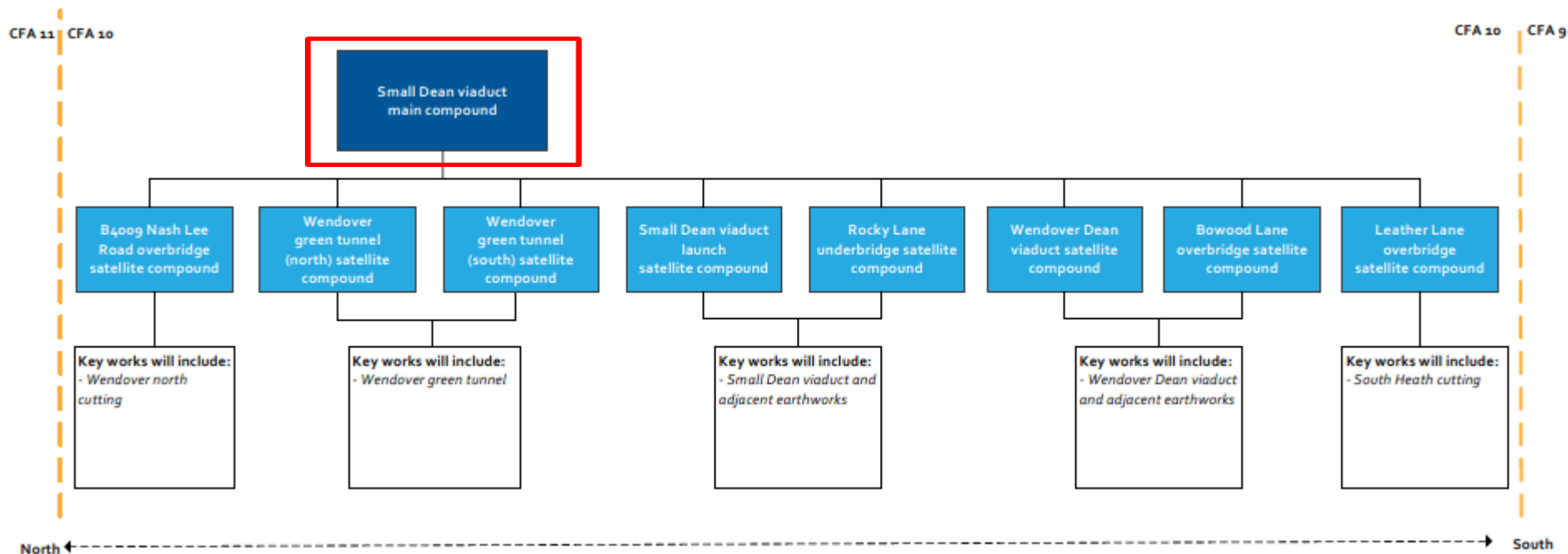
Buckinghamshire Construction Compounds

CFA 9 – Rail systems compounds



Buckinghamshire Construction Compounds

CFA 10 – Civils works compounds

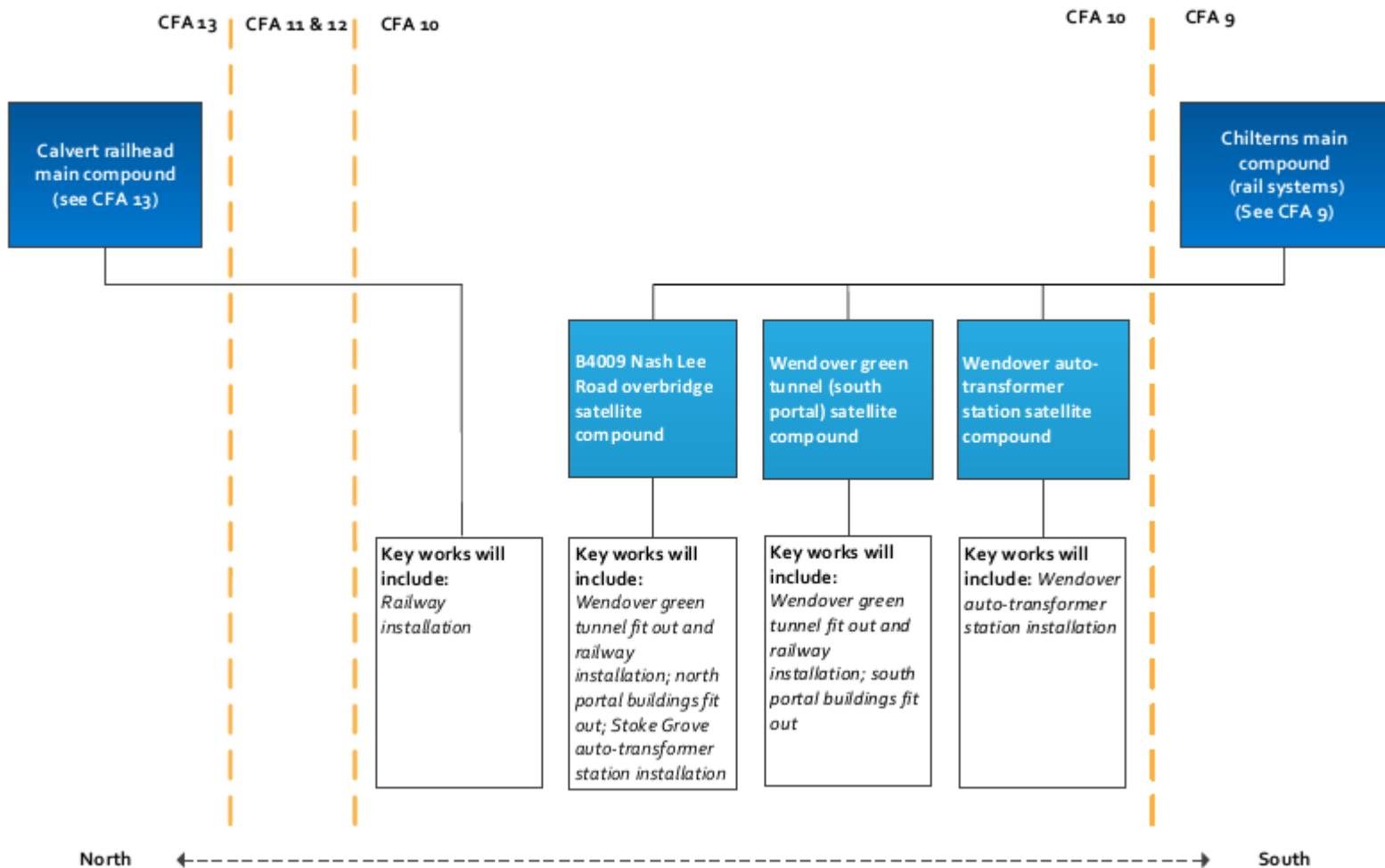


Key

- Main construction compounds
- Satellite construction compounds
- Construction compound(s) with space for workers' accommodation provided

Buckinghamshire Construction Compounds

CFA 10 – Rail systems compounds



Key



Main construction compounds



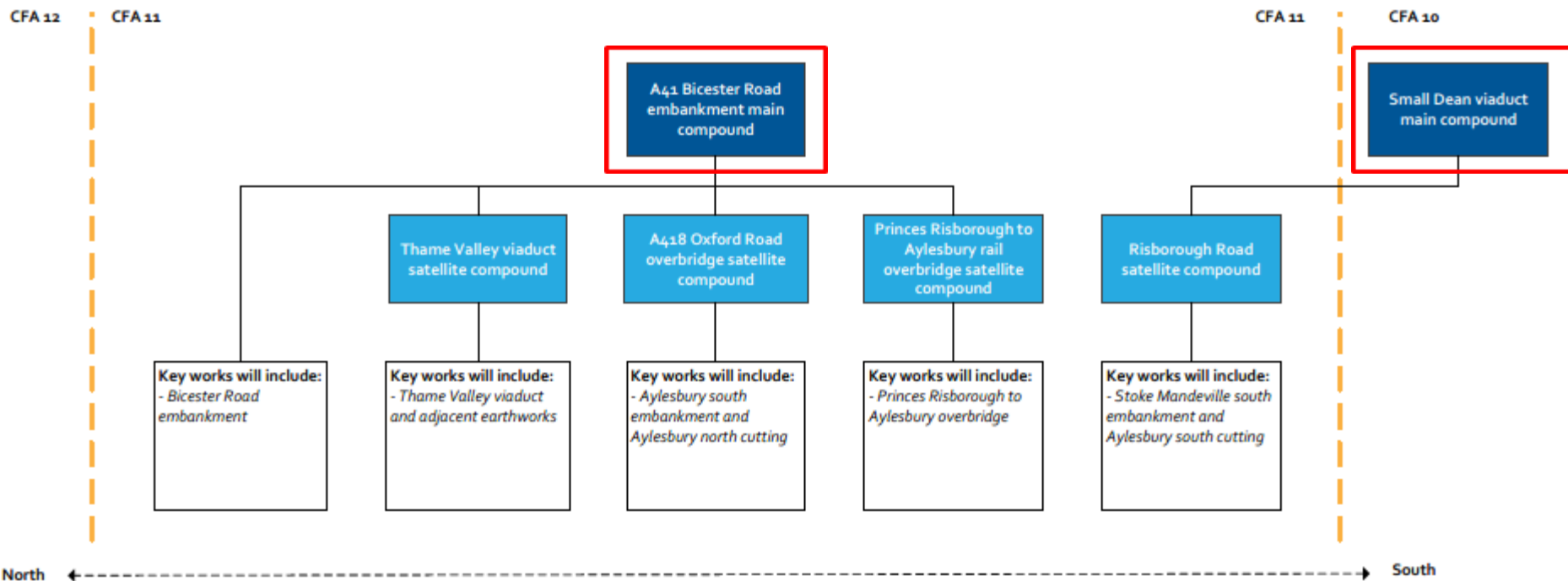
Satellite construction compounds






Construction compound(s) with space for workers' accommodation provided

Buckinghamshire Construction Compounds

CFA 11 – Civils works compounds

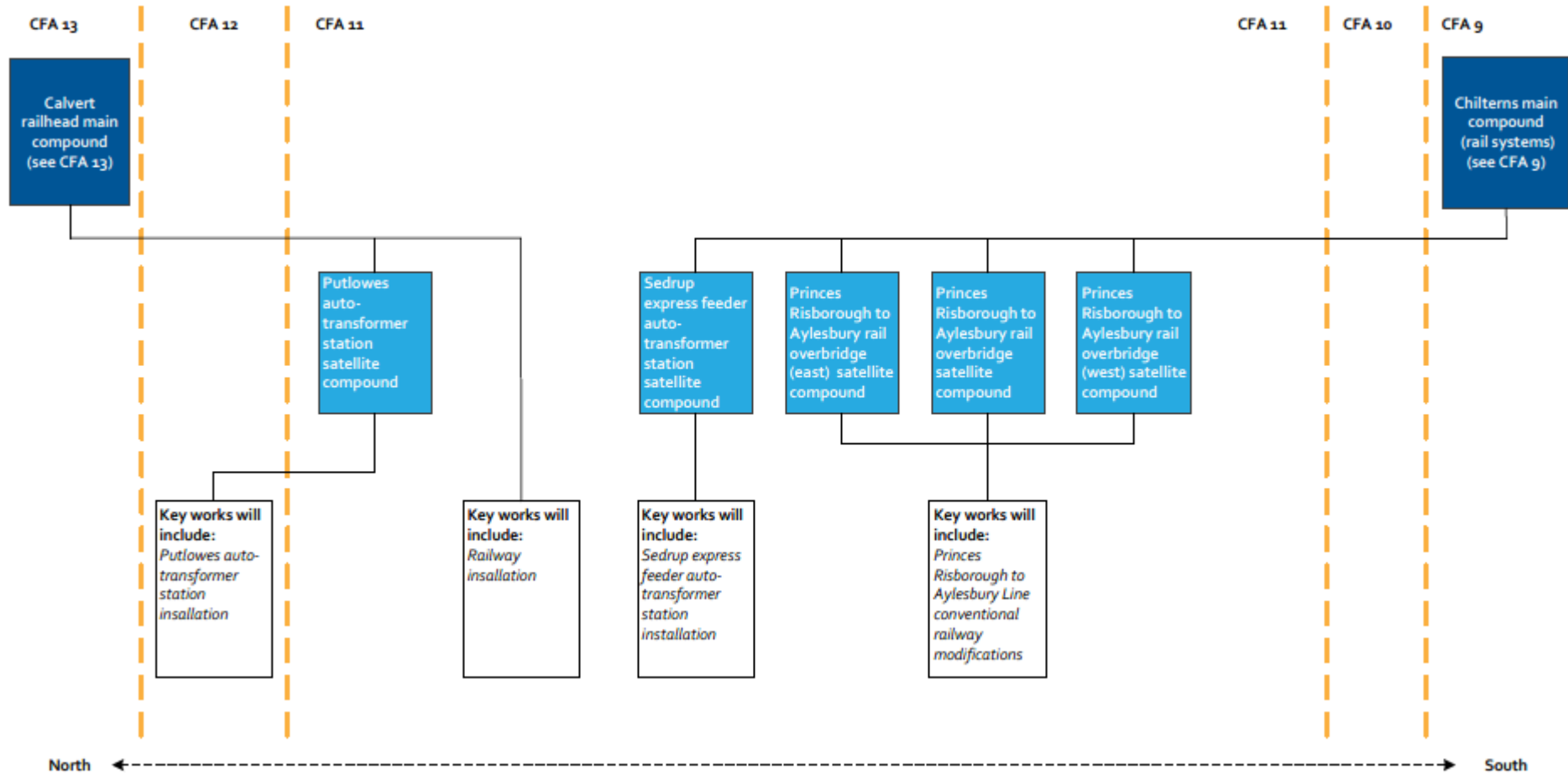


Key

-  Main construction compounds
-  Satellite construction compounds
-  Construction compound(s) with space for workers' accommodation provided

Buckinghamshire Construction Compounds

CFA 11 – Rail systems compounds



Key



Main construction compounds



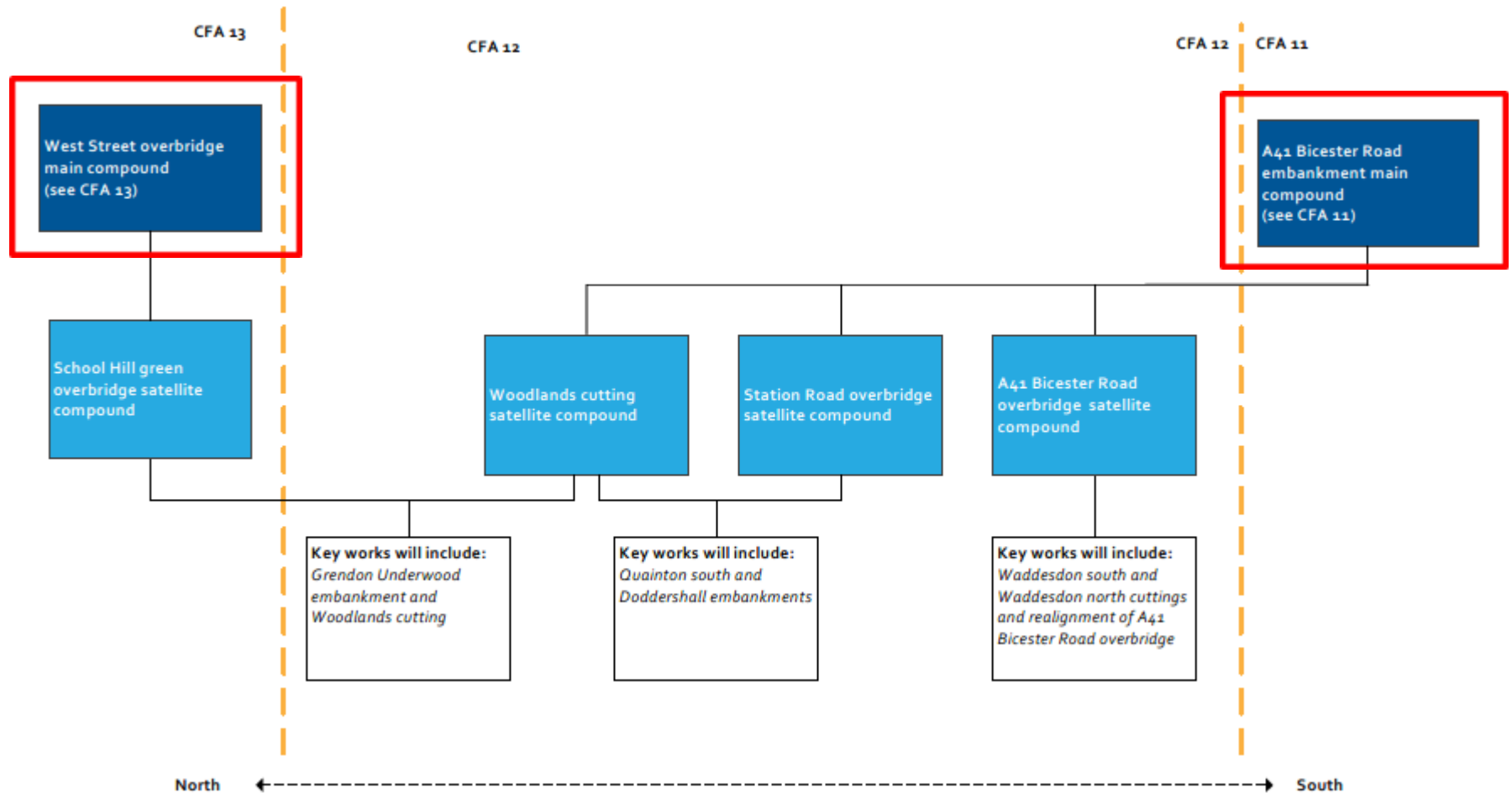
Satellite construction compounds



Construction compound(s) with space for workers' accommodation provided

Buckinghamshire Construction Compounds

CFA 12 – Civils works compounds



Key



Main construction compounds



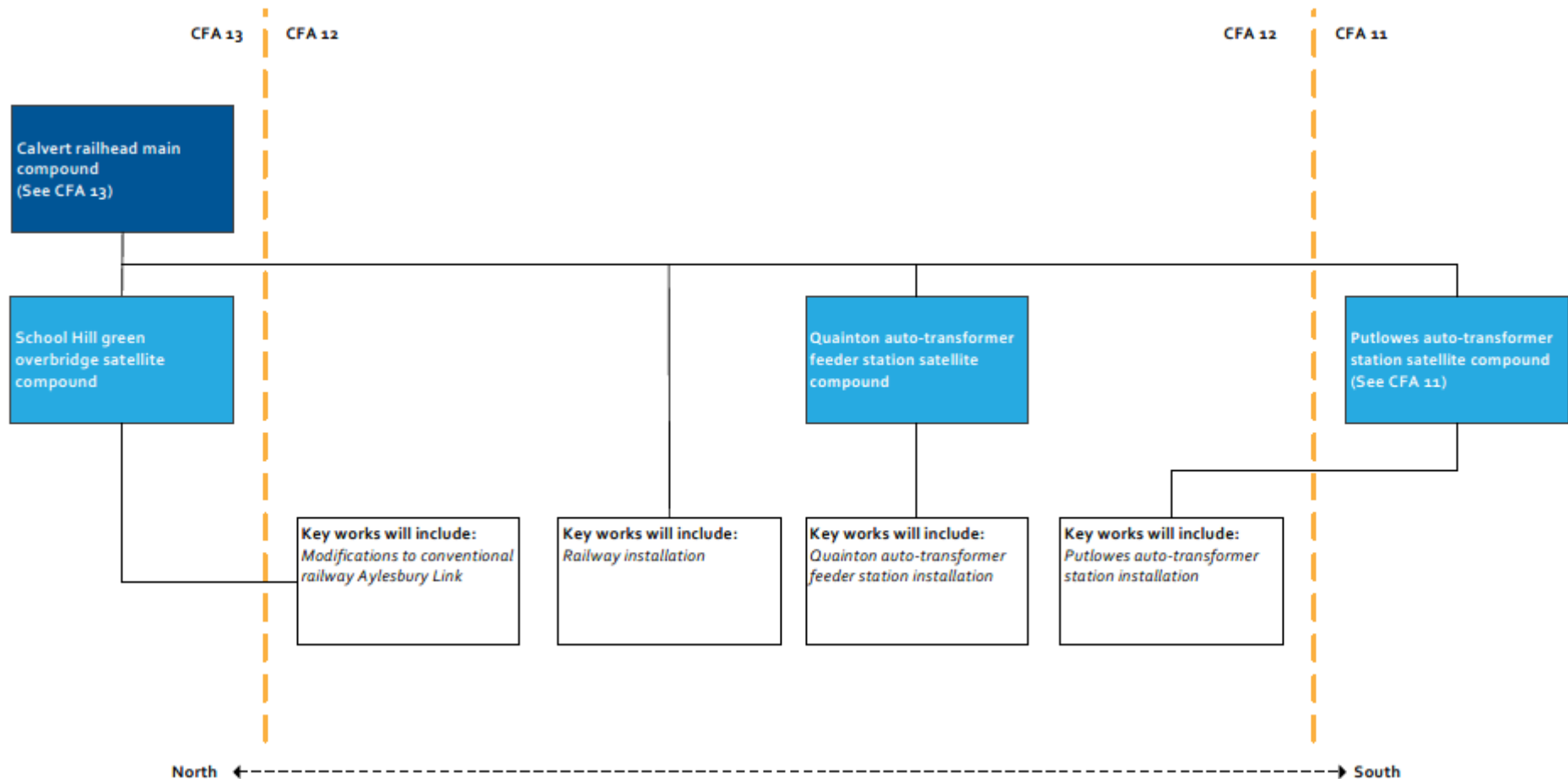
Satellite construction compounds



Construction compound(s) with space for workers' accommodation provided

Buckinghamshire Construction Compounds

CFA 12 – Rail systems compounds



Key



Main construction compounds



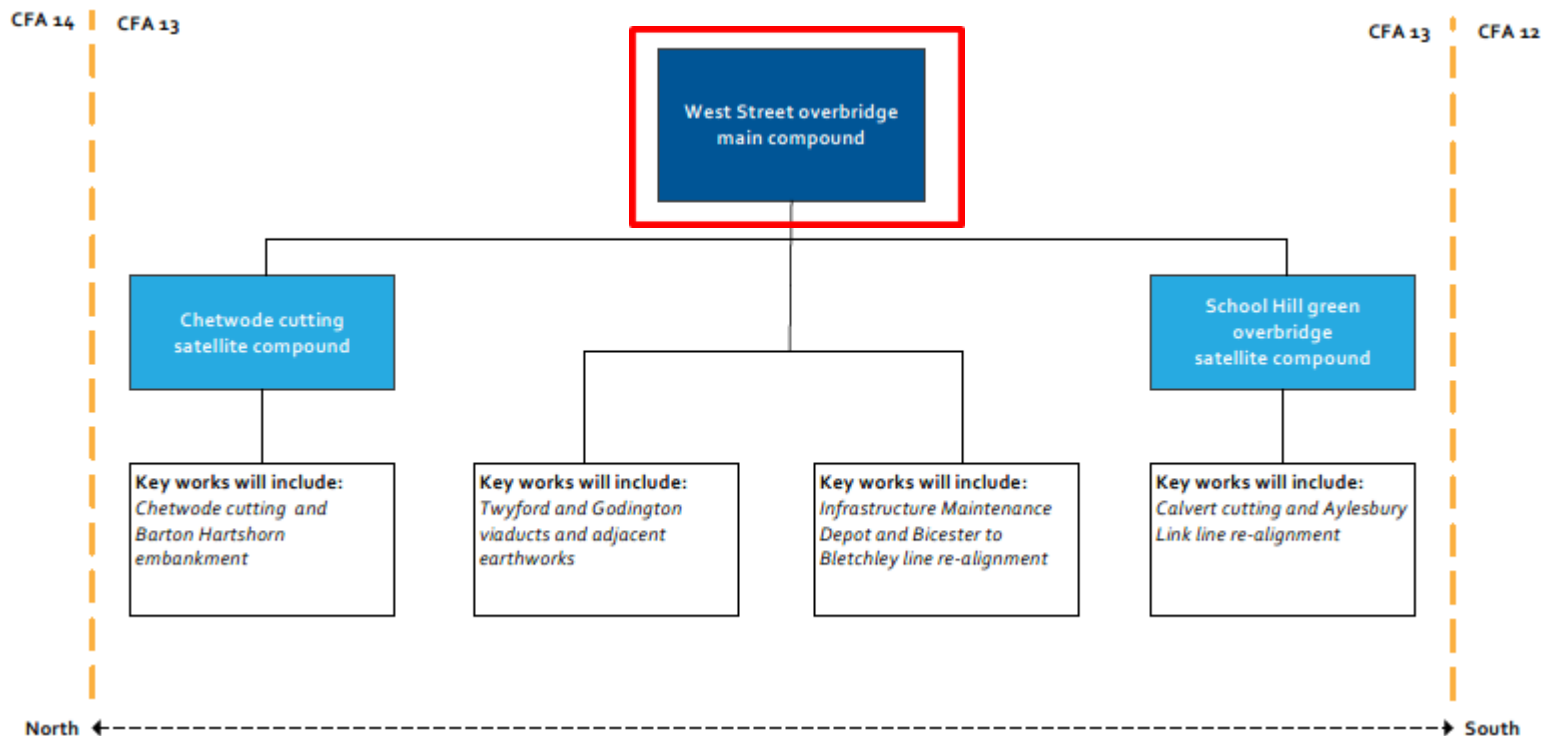
Satellite construction compounds



Construction compound(s) with space for workers' accommodation provided

Buckinghamshire Construction Compounds

CFA 13 – Civils works compounds

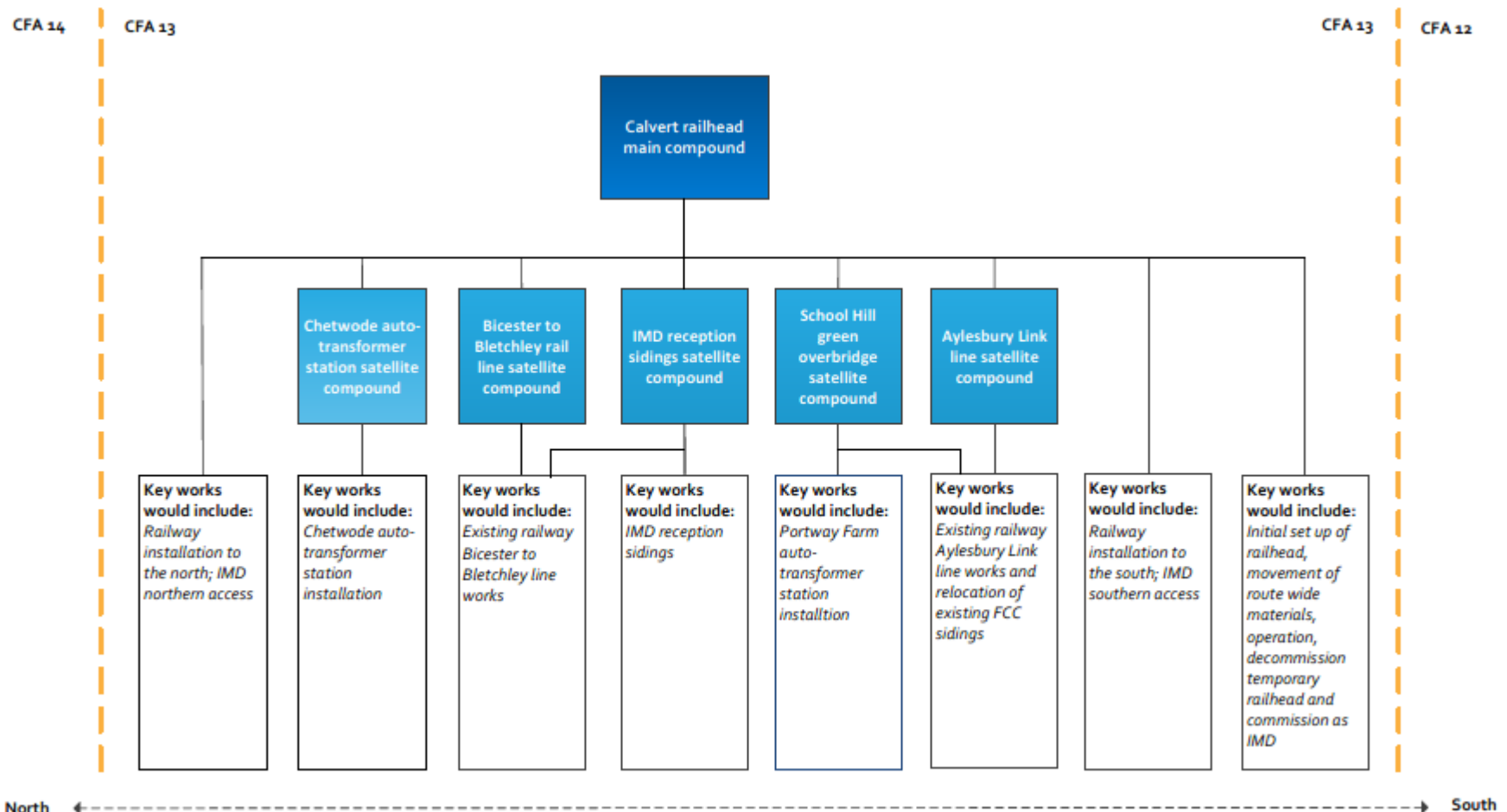


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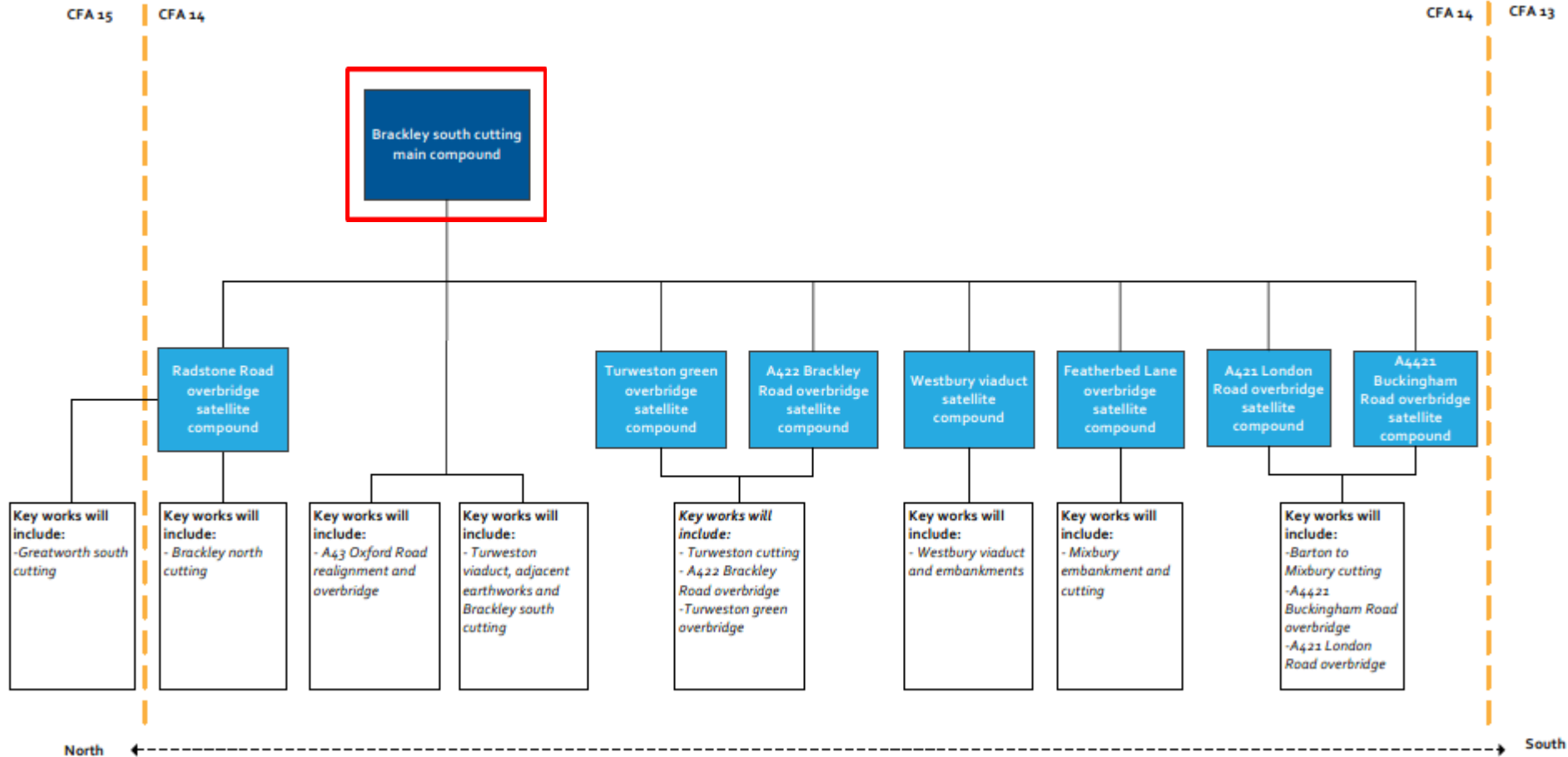
- Main construction compounds
- Satellite construction compounds
- Construction compound(s) with space for workers' accommodation provided

Buckinghamshire Construction Compounds

CFA 13 – Rail systems compounds



Construction Compounds – Buckinghamshire (CFA 14)



Key



Main construction compounds



Satellite construction compounds

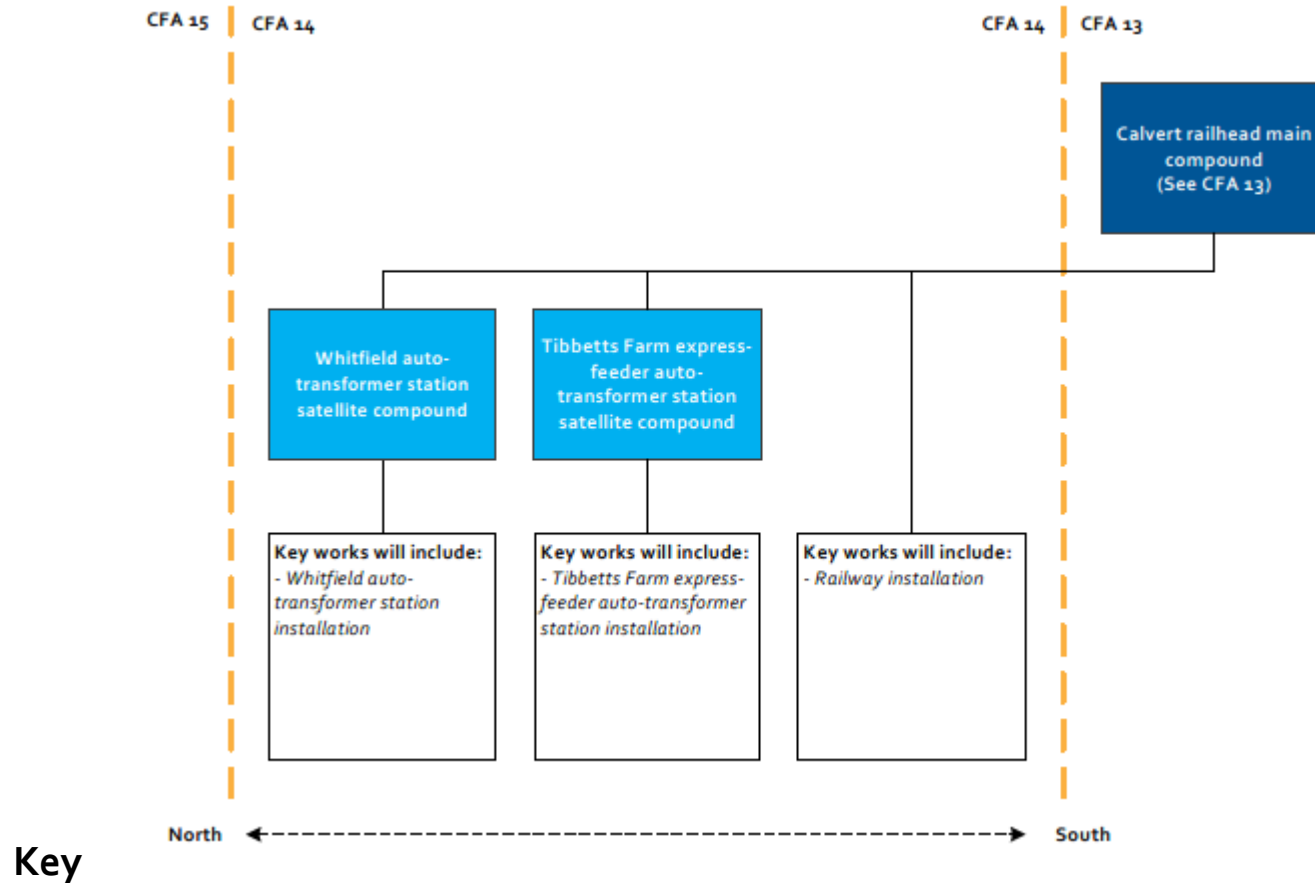


Construction compound(s) with space for workers' accommodation provided

P3751 (17)

Buckinghamshire Construction Compounds

CFA 14 – Rail systems compounds



■ Main construction compounds

■ Satellite construction compounds

□ Construction compound(s) with space for workers' accommodation provided